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RECEIVED

November 21, 2025

11/25/2025

VIA E-MAIL christina.ratcliffe@cityofsanrafael.org

**MARIN COUNTY
CLERK**

Christina Ratcliffe, AICP
Planning Consultant
City of San Rafael
1400 Fifth Ave.
San Rafael, CA 94901

Re: Challenged Conduct Notice (Gov. Code § 65589.5(h)(6)(D)) Regarding 700 Irwin Street (“**Property**”) Mixed-Income Housing Project (“**Project**”) and Further Response Regarding AB 2011 Review

Dear Ms. Ratcliffe:

Our firm has been retained by 700 Irwin Street Partners, LLC to take remedial action to address the City of San Rafael’s (“**City**”) ongoing violations of the Housing Accountability Act (“**HAA**”) and other applicable laws.¹ Accordingly, this letter serves as notice under Government Code § 65589.5(h)(6)(D) that the City has effectively disapproved the 700 Irwin St. Project by causing unnecessary delay and needlessly increasing the cost of the proposed Project.

The Applicant submitted a development application on April 10, 2025, following a preliminary SB 330 application filed November 6, 2024. Since then, the City has issued a string of letters that (1) conflate completeness requirements with objective standard consistency requirements, (2) confuse standards that apply to the State Density Bonus Law (“**SDBL**”) base project versus the actual bonus Project, (3) attempt to impose backdoor CEQA requirements even though AB 2011 and AB 130 exempt the Project from CEQA, and (4) repeatedly assert the Project’s ineligibility for AB 2011 without providing an opportunity for the Applicant to thoroughly respond to the City’s comments.

In addition to providing notice under Government Code § 65589.5(h)(6)(D), this letter also challenges the City’s review of the Project pursuant to AB 2011. It is apparent that the City has failed to properly review the Project under AB 2011 and instead has attempted to sidestep its responsibilities to do so by attempting to issue a “final determination” prior to properly considering the evidence submitted to it (or otherwise available to it under its own publications). Pursuant to the City’s email on October 31, 2025, Staff is currently reviewing the Project’s October 24, 2025, revised submittal for consistency with AB 2011, notwithstanding the City’s prior letters on July 9, 2025 (“**July 9 AB 2011 Letter**”), and July 31, 2025 (“**July 31 AB 2011**”).

¹ Our co-counsel, Reuben, Junius & Rose, LLP, will continue to represent 700 Irwin Street Partners, LLC (“**Applicant**”). Mr. Andrew Junius will continue to be the primary contact for the Applicant in connection with the 700 Irwin St. Project applications, and we expect a timely response to correspondence from his office.

Letter”), finding that the Project is not eligible for AB 2011. We are hopeful that this review will be substantive and not just another effort by the City to dodge its responsibility to properly review the Project under AB 2011.

To ensure that the City properly discharges its obligations to consider the Project under AB 2011, we provide the following information, including information responsive to prior incorrect positions taken by the City.

I. HOUSING ACCOUNTABILITY ACT CHALLENGED CONDUCT NOTICE

As you know, the HAA provides strict requirements for processing and approving housing development projects. Cities that violate the HAA by improperly disapproving qualifying housing projects may be subject to fines, fees, and other penalties.

The HAA has recently revised the concept of “disapproval” of a housing development project. The HAA now defines disapproval to mean, among other things, any instance in which a local agency “fails to cease a course of conduct undertaken for an improper purpose, such as to harass or to cause unnecessary delay or needless increases in the cost of the proposed housing development project, that effectively disapproves the proposed housing development without taking final administrative action . . .” (Gov. Code § 65589.5(h)(6)(D).)

Pursuant to AB 1893 (effective January 1, 2025), **this letter serves as notice** that the City has undertaken a course of conduct that effectively disapproves the proposed housing development without taking final administrative action, as explained in more detail below. The City has five working days of receiving this written notice to post the notice on the City’s website, provide the notice to any person who has made a written request for notices pursuant to Public Resources Code § 21167(f), and file the notice with the County Clerk. (Gov. Code § 65589.5(h)(6)(D)(ii).)

Further, the City has 90 days from this written notice to cease the challenged conduct described below, or else the City bears the burden to establish its course of conduct does not constitute a disapproval of the housing development project. (*Id.* subd. (h)(6)(D)(v).)

A. The City’s Conduct Demonstrates an Unwillingness to Approve the Project

It has been seven months since the Project application materials were filed with the City on April 10, 2025. As part of those filings, the application complied with all applicable requirements, and the Applicant worked in good faith with City Staff in processing and approving the Project in an expeditious good faith manner. The focus of the application and related submittals was on (a) the base project density (a critical step in determining the starting point for applying the SDBL), (b) the Project’s eligibility for AB 2011, detailing how both the site and the Project both comply with that state ministerial approval program, and (c) describing the Project’s zoning metrics and comparing those to the City’s recently adopted downtown form based zoning code.

Rather than engage in a cooperative dialogue with the Applicant as envisioned by state law, Staff has instead employed a course of conduct that focused on minutia and extraneous downstream requirements having nothing to do with the zoning entitlements and approvals required under state law.

The City's first response to the Applicant, on May 9, included many pages of comments and requests for information that had been previously provided, were not related to either the base density calculation or compliance with objective standards, was not required by either SDBL, AB 2011, or the 2023 Submittal Requirements checklist² that the Project is subject to (based on its November 6, 2024 SB 330 application), or that focused on post entitlement permit requirements that are irrelevant at this stage of entitlement review.

For example:

- The May 9 letter asked for “[a] list of requested waivers or reduction of development standards.” (*PLAN25-046 – Completeness Review for 700 Irwin* (May 9, 2025), pg. 5-6.) As explained in the Applicant’s June 17 response, the SDBL Project Letter submitted on April 10 detailed the requests for four waivers and no concessions, and included an explanation as to why the otherwise applicable standard would have the effect of physically precluding construction of the Project as proposed.
- The May 9 letter asks for a physical color and material board. (*PLAN25-046 – Completeness Review for 700 Irwin* (May 9, 2025), pg. 9.) This is not required by law and the request does nothing but burden and delay the Applicant.
- The May 9 letter calls for a revision to show a “include a line-of-site triangle for the driveway.” (*PLAN25-046 – Completeness Review for 700 Irwin* (May 9, 2025), pg. 7.) This requirement does not apply to projects subject to the Downtown Precise Plan (“DPP”).³
- The May 9 letter asks for a transportation analysis (*PLAN25-046 – Completeness Review for 700 Irwin* (May 9, 2025), pg. 9), despite the fact that the April 10 development application included a consultant-prepared transportation analysis.

The next series of letters from the City drive home its emerging strategy of diversion and focus on minutia rather than properly processing the application, while at the same time pretending that they were in fact processing the application as required by law. Case in point: on July 9, 2025, the City issued 40 pages of detailed comments and confused concerns regarding both the Project’s consistency with AB 2011 and local objective standards. In that letter, Staff continued to conflate and confuse the issue of base project compliance with local standards and the

² The Project filed an SB 330 application on November 6, 2024. Accordingly, the Project is subject to the submittal requirements listed in the March 2023 General Planning Permit Application Submittal Requirements form (“2023 Submittal Requirements”), rather than the Application Submittal Requirements form that was posted on November 18, 2024. (Cal. Govt. Code §§ 65589.5(o); 65941.1.)

³ SRMC Section 14.16.295(C) provides: “The provisions of this section are not applicable to properties within the downtown mixed use (DMU) district. For sight distance provisions and standards in the downtown mixed use district, see the Downtown San Rafael Precise Plan Form-Based Code which is adopted by separate ordinance and incorporated herein by reference.”

Project itself, and repeated many of the “concerns” in their May 9 letter, even though the applicant had already responded to those issues and requests in our letter of June 17.

For example, the 23 page consistency review chart included with the July 9 AB 2011 Letter requires the Project to revise the plans to show the number of parking spaces required by the DPP, even though the Project had requested a SDBL waiver from the parking requirements in all prior submittals. Additionally, that same July 9 consistency review chart includes several comments related to non-objective design standards:

- “Consider the design recommendations and explore other ways to build a tripartite façade articulation.”
- “Consider creating one or more corner elements that lend visual importance to the corners and enhancing the public realm.”

Five days after issuing the July 9 AB 2011 Letter, on July 14, the City grudgingly acknowledged that the project application was in fact complete, 27 days after the Applicant’s June 17 response was submitted, and just in time before the 30-day time period under the HAA expired. (Govt. Code § 65943(b).)

On July 17 the Applicant again provided further responses to the allegations of ineligibility put forth by the City regarding the application of AB 2011. (See discussion of AB 2011 eligibility below, in Section II). On July 29, the Applicant provided staff with another plan sheet explaining yet again, how the base project design complies with local objective standards. As late as their September 12 consistency review letter, the City continued to conflate and confuse the base Project with the actual Project that will use the SDBL and applicable waivers and concessions.

The Applicant provided a revision to the July 29 base project sheet on October 1. That revision provided minor additional details that the City asked for, but did not make any substantive changes to the base Project design. It was not until October 2 that the City finally agreed that the base project (the substantially same version of which had been in front of the City for six months) did in fact comply with local objective standards. We note that this confirmation of base project compliance came only after the Applicant told the City that they had hired our firm to explore options for initiating litigation to enforce the HAA and other applicable laws.

We also note that the City’s actions could potentially have broader implications. If the City does not proceed in good faith to implement the programs and policies in its Housing Element, it may risk decertification and other penalties. The 700 Irwin Street Property was specifically identified in the Housing Element’s suitable site inventory as a property that is well-suited for housing development (see Table 4.5 at p. 4-23), and the City must comply with Housing Element policies that call for “the creation of affordable housing opportunities in all parts of the city.” (See Policy H-4.1.)

A. CEQA Games

While the Project is eligible for processing under AB 2011 and should be ministerially approved, in mid-2025 a second approval path became available when Governor Newsom signed into law AB 130, which includes a statutory exemption from CEQA for housing projects that meet certain project and site criteria. (Public Resources Code § 21080.66.)

In an obvious attempt to delay the Project and drive up cost, the City has demanded that the Applicant respond to issues that are entirely unrelated to the AB 130 process. For example, the street width and urban perimeter issues that the City is using in an attempt to frustrate application of AB 2011 are not requirements for the AB 130 CEQA exemption. The Applicant immediately notified the City on July 17, that the Project qualifies for this new statutory exemption and requested that the City begin processing an AB 130 exemption.

Consistent with the ongoing conduct that frustrates the approval of the project, the City continued to throw up roadblocks in an apparent attempt to block the Applicant's use of AB 130. The express purpose of both AB 2011 and AB 130, is to streamline the processing of housing entitlements. The primary means of streamlining is to eliminate or dramatically reduce the amount of process necessary under CEQA. By providing a ministerial approval, AB 2011 requires no CEQA process at all. And under the AB 130 statutory exemption, CEQA clearance should be quick and clear given the short checklist set forth in new Public Resource Code § 21080.66, which cites to the same environmental site criteria that AB 2011 uses (both bills rely on the SB 423 criteria found in Govt. Code § 65913.4(a)(6)).

Instead of accepting the obvious, the City has stuck to the position staked out in its September 12 letter that a CEQA traffic study and noise and vibration study are required in order to comply with AB 130 and/or AB 2011. The City argues that a noise study is necessary to comply with the City's General Plan Policy related to maintaining acceptable noise levels. (See *700 Irwin Street (PLAN25-046) – 700 Irwin Street (PLAN25-046) – Consistency Review* (September 12, 2025), **Exhibit 1**.) With respect to the request for a transportation study, the City's September 12 letter confirms that this is a CEQA-related requirement.

The San Rafael Transportation Analysis Guidelines (February 2022) require a Transportation Impact Analysis and CEQA review for Tier 3 projects. (p. 6.) The Project is a Tier 3 Project, so you will be required to prepare a Transportation Impact Analysis.

(See *700 Irwin Street (PLAN25-046) – 700 Irwin Street (PLAN25-046) – Consistency Review* (September 12, 2025), Courtesy Comments.)

The noise study and transportation analysis requests ignore the fact that a Project eligible for AB 2011 and/or AB 130 is not required to complete any project-specific CEQA review. And the City cannot circumvent these state laws by requiring projects to comply with locally adopted requirements that are essentially CEQA procedures.

While the State has clearly created multiple pathways for this Project to move quickly through the housing approval process by avoiding CEQA, the City has taken the position that it will pick and choose what CEQA studies they will still demand, notwithstanding state law.

B. De Facto Disapproval

As detailed herein, Staff's review of this Project appears designed to deny it by delaying final action. By issuing letter after letter seeking to impose non-objective design standards, impose environmental review requirements that AB 2011 and AB 130 preclude, and excluding this downtown freeway-adjacent site from AB 2011 (which was designed to facilitate housing development on infill parcels on busy streets), the City has caused unnecessary delay that has dramatically and needlessly increased the cost of the Project.

II. AB 2011 SUPPLEMENTAL INFORMATION

To date, the City's incorrect position regarding the Project's eligibility for AB 2011 has focused on the two threshold issues: (1) the width of Irwin Street (related to Govt. Code § 65912.121(c), Commercial Corridors) and (2) Canal Adjacency (related to Govt. Code § 65912.121(e), 75% of Perimeter of Site Adjoining Urban Uses). As discussed below, other technical inconsistencies with AB 2011 raised by the City could be easily resolved once we address these two key questions.

Regardless, pursuant to the City's October 31, 2025, email, Staff is currently reviewing the Project for consistency with AB 2011 again, and we expect the City to reiterate its incorrect position that the Project is not eligible for AB 2011. So that the record is clear, we provide information showing without questions that the Project is eligible for streamlined processing under AB 2011 and ask that the City acknowledge that its prior positions were incorrect.

A. AB 2011 Eligibility

Both the HAA and AB 2011 require cities to **reasonably interpret the law to facilitate the production of badly needed housing**. To that end, the HAA provides as follows: "For purposes of this section, a housing development project or emergency shelter shall be deemed consistent, compliant, and in conformity with an applicable plan, program, policy, ordinance, standard, requirement, or other similar provision if there is substantial evidence that would allow a reasonable person to conclude that the housing development project or emergency shelter is consistent, compliant, or in conformity." (Gov. Code § 65589.5(f)(4).)

AB 2011 provides the same reasonableness standard: "For purposes of this section, a development is consistent with the objective planning standards if there is substantial evidence that would allow a reasonable person to conclude that the development is consistent with the objective planning standards." (Gov. Code § 65912.124(b)(1).)

This standard applies to the evaluation of whether or not the Project meets each of the following AB 2011 eligibility standards.

1. Govt. Code § 65912.121(c) – Commercial Corridors

The threshold requirement for a mixed-income project's eligibility for AB 2011 is that the site must abut a commercial corridor, defined in AB 2011 as a street measuring 70-150 feet wide, including sidewalks. (Gov. Code §§ 65912.101(b), (s).)

As detailed in the Applicant's July 17 Applicant Letter, the pedestrian easement on the east side of Irwin Street was dedicated to the City in order to connect Second Street to the San Rafael Canal (see Right of Way Exhibits and Grant of Perpetual Easement Doc. 88-22240, attached at **Exhibit 2**). As a permanent pedestrian easement, this pathway should count toward the width of Irwin Street as a qualifying commercial corridor for the purposes of AB 2011. The City has incorrectly referred to this public easement as a private pathway and asserted that a narrow strip of undevelopable land between the easement and the roadway disqualifies the pedestrian easement from counting as a sidewalk under AB 2011.

AB 2011 is clear that for the purposes of defining a “commercial corridor,” a street includes the sidewalk. (Gov. Code § 65912.101(s).) In a case like this, where an easement dedicates a right of way for permanent public access—and thereby prevents that right of way from being developed—it is reasonable to conclude that the easement should be included in the street width for the purposes of determining whether Irwin Street is a commercial corridor. The measurement from the eastern boundary of the easement to the west curb of Irwin ranges from 71 feet to 78.2 feet, well above the 70 feet required for a street to be considered a commercial corridor under AB 2011.

In its July 9 letter, the City took the position that the public pedestrian easement is “private property” and should not be counted in the width of the street. This position is factually incorrect, as shown by the documentation in Exhibit 2,⁴ and it is inconsistent with both the HAA and AB 2011’s standards for determining consistency with objective development standards. The Applicant has provided substantial evidence that would allow a reasonable person to conclude that the development is consistent with the objective planning standards. We therefore expect the City to confirm that the Project abuts a commercial corridor.

2. Govt. Code § 65912.121(e) – 75% Perimeter and Canal Adjacency

AB 2011 requires that at least 75% of the perimeter of an eligible site adjoin parcels that are developed with urban uses, and parcels that are only separated by a street, pedestrian path, or bicycle path shall be considered to be adjoined. (Gov. Code § 65912.121(e).) An “urban use” is “any current or former residential, commercial, public institutional, public park that is surrounded by other urban uses, parking lot or structure, transit or transportation passenger facility, or retail use, or any combination of those uses.” (Gov. Code § 65912.101(t).)

The Project site is occupied by a vacant office building that was constructed in the 1980s and is adjacent to the northbound off-ramp of US 101. The Property is not a perfect square, and the canal side of the Property equates to approximately 27% of the site—i.e., 73% of the site is surrounded by streets and other developed commercial uses.

As detailed in the July 17 Applicant Letter, the San Rafael Channel (or Canal) is a man-made channel that serves as part of the urban infrastructure of the City. There is a municipal stormwater pumping station on the south bank of the Canal opposite the Project site and a pedestrian boardwalk along the entire south frontage of the Property (see Canal photos attached at **Exhibit 3**, and site map attached at **Exhibit 4**).

The canal is specifically referenced in City materials and planning documents as an “urban” feature within the city limits. For example, a San Rafael Creek Federal Navigation Channel Factsheet prepared by the City and Marin County states that the “**San Rafael Channel is an urban waterfront.**” (San Rafael Creek Federal Navigation Channel Factsheet, attached at **Exhibit 5**.) Additionally, the San Rafael Canalfront Design Guidelines describe the Canal as follows:

The Canal is widely recognized as an under-utilized community resource and, in many ways, can be thought of as an open space - a public amenity for all the neighborhoods

⁴ See also Vehicle Code § 555, which confirms that sidewalks and/or pedestrian easements are the “portion of a highway” (i.e., roadways) designated “for pedestrian travel.”

of San Rafael. (San Rafael Canalfront Design Guidelines (December 2009), pg. 1, attached at **Exhibit 6**.)

The City's own design documents describe the Canal as an urban use and an open space, such that the canal could be treated as a park surrounded by other urban uses. AB 2011 aims to streamline multifamily housing developments on urban infill sites. The Property sits in the heart of Downtown San Rafael and is undeniably an urban infill site. It is surrounded by US 101 and other commercial uses. The properties directly across the canal from the Project site contain a stormwater pumping station and an office building. There is currently no public access to the canal at or adjacent to the Property, though the Project proposes to create canal access with a new publicly accessible boardwalk.

For all these reasons, considering the San Rafael Canal as an urban use in this case is consistent with the intent of AB 2011 and the City's efforts to avoid this obvious urban use is further evidence that it is not properly considering the Project under AB 2011 or the HAA.

3. Govt. Code § 65912.121(g) – FEMA Floodplain

A property within a FEMA special flood hazard area is not eligible for AB 2011 unless the proposed project on that property can meet one of the following conditions:

- 1) "The site has been subject to a Letter of Map Revision prepared by the Federal Emergency Management Agency and issued to the local jurisdiction;" or
- 2) "The site meets Federal Emergency Management Agency requirements necessary to meet minimum flood plain management criteria of the National Flood Insurance Program pursuant to Part 59 (commencing with Section 59.1) and Part 60 (commencing with Section 60.1) of Subchapter B of Chapter I of Title 44 of the Code of Federal Regulations." (Gov. Code § 65912.121(g); Gov. Code § 65913.4(a)(6)(G).)

As revised on October 24, 2025 (see SCB 700 Irwin Comment Response, attached at **Exhibit 7**), the Project proposes a smaller footprint at the basement and ground floor levels of the building, allowing the entire footprint of the Project to sit outside the surrounding FEMA special flood hazard area. With this revision, and subject to a Letter of Map Amendment issued by FEMA and supported by a revised grading and building elevation, the Project would no longer be subject to the floodplain management criteria. (See BKF 700 Irwin Flood Mitigation Memo, attached at **Exhibit 8**.)

Therefore, because the Project footprint has been redesigned to be located outside a special flood hazard area, and because the site is subject to a Letter of Map Amendment (i.e., Revision) prepared by FEMA, the Project is eligible for AB 2011. We expect the City to confirm the Project is eligible for AB 2011 pursuant to this criterion.

4. Govt. Code § 65912.122(d) – Distribution and type of affordable units

AB 2011 requires that an eligible project provide affordable units that "have the same bedroom and bathroom count ratio as the market rate units, be equitably distributed within the project, and have the same type or quality of appliances, fixtures, and finishes." (Gov. Code § 65912.122(d).)

The Project will provide comparable bedroom and bathroom counts between the market rate and affordable units (subject to SDBL waivers and/or concessions), the affordable units will be equitably distributed, and the affordable units will have the same quality of materials as the market rate units, as required by AB 2011 (and SRMC Section 14.16.030 and City Council Resolution 14890). Therefore, the Project complies with AB 2011's inclusionary housing requirements. Again, we expect the City to confirm the Project is eligible for AB 2011 pursuant to this criterion.

5. Govt. Code § 65912.123(b) – Maximum Allowable Residential Density

Maximum residential density under AB 2011 is based on the greater of either the applicable default density ratio under Government Code § 65583.2, for a metropolitan jurisdiction or Government Code § 65583.2, for a nonmetropolitan jurisdiction, or on the local density limit, if higher than the state law defaults, prior to the addition of a density bonus. At the Property, the maximum density is calculated by estimating the realistic development capacity of the site based on the applicable objective development standards from the City's DPP (the Downtown Form Based Code, attached at **Exhibit 9**), pursuant to the definition of "maximum allowable residential density" found in the State Density Bonus Law. (Gov. Code § 65912.123(b); § 65915(o)(6) [defining "base density" under the State Density Bonus Law and mandating an estimate based on "the realistic development capacity of the site based on the objective development standards applicable to the project."].)

On October 1, 2025, the applicant submitted a revised Density Bonus base project plan for the City's review ("**October 1 Base Density Plan**") (see 700 Irwin Revised Base Project A0.41A, attached at **Exhibit 10**.) On October 2, 2025, the City confirmed that revised base project plan is consistent with the City's development standards. (See email from Christina Ratcliffe, attached at **Exhibit 11**.)

The October 1 Base Density Plan proposes 100 dwelling units, with 15% of the units offered at Very Low Income and another 15% of the units offered at Moderate Income. Accordingly, the Project is eligible for a 100% density bonus, allowing the Project to construct up to 200 units. Therefore, the Project is consistent with maximum allowable residential density for the site.

6. Govt. Code § 65912.123(c) – Maximum Allowable Height

For the purposes of AB 2011, the applicable maximum height limit at the Property, prior to the application of a density bonus, is the 50-foot height limit established under the DPP. (See Downtown Form Based Code Table 2.2.020, showing a 50-foot base height limit for properties in the T5N 50/70 zone.) The October 1 Base Density Plan proposes an overall building height of 49 feet, 11 inches, and therefore complies with the height limit. The Project has requested a State Density Bonus Law waiver to exceed the 50-foot height limit, which the City is required to grant under state law (Gov. Code § 65915(b)).

7. Govt. Code § 65912.123(d) – Required Setback

The City's letters assert that the Project must incorporate a 10-foot ground floor setback, with setbacks equal to 7 feet times the floor number for each additional floor. It is not clear from the City's letters which of the four Property frontages the City intends to apply these setbacks to.

However, the City misconstrues the applicable setbacks under AB 2011. At the Property, AB 2011 does not require any setbacks for the portion of the Property fronting the Irwin Street commercial corridor. Instead, for the portion of the property that does not abut the commercial corridor, different setbacks apply to (A) property lines that abut a property that contains a residential use, or (B) along property lines that abut a property that does not contain a residential use. None of the property lines at 700 Irwin abut residential properties. For property lines that abut property that does not contain a residential use, the applicable setback requirement is 15 feet, though it can be decreased by the City (Gov. Code § 65912.123(d)) and waived pursuant to a SDBL waiver. (Cal. Govt. Code § 65912.124(f)(2).)

The October 1 Base Density Plan shows a 15 foot setback along the eastern property line, as required by § 65912.123(d)(3)(B). No AB 2011 setbacks are required along the Irwin Street commercial corridor, or along the Second Street or Canal frontages, because neither of those frontages abut properties that contain residential or commercial uses.

If the City agrees to approve the Project pursuant to AB 2011 as it is required to do, the Applicant will request a waiver from the 15-foot setback required by Government Code § 65912.123(d)(3)(B).

8. Govt. Code § 65912.123(j) – Other Objective Standards Not Specified in AB 2011.

AB 2011 requires that projects comply with a city's applicable objective standards. (Gov. Code § 65912.123(j).) In its July 9 AB 2011 Letter, the City determined that the Project is inconsistent with this requirement based on inconsistencies outlined by the City in the Consistency Review Comments chart attached to the July 9 AB 2011 letter. Following the City's October 2 determination of base project consistency, Staff sent another Project consistency review letter on September 12, 2025 (the "**September 12 Consistency Letter**"). In a meeting on October 10, Staff confirmed that the comments in the September 12 Consistency Letter are the only outstanding comments related to the Project's compliance with the applicable objective standards.

Our co-counsel responded to the September 12 Consistency Letter on October 25, explaining how the Project complies with the various requirements listed in that letter, or requesting a Density Bonus waiver or concession from those requirements. Based on that response, we expect that Staff will now find the Project to be consistent with the applicable development standards.

Very truly yours,



Ellis F. Raskin
Partner

Christina Ratcliffe, AICP
November 21, 2025
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cc:

Cristine Alilovich, City Manager (Cristine.Alilovich@cityofsanrafael.org)
Micah Hinkle, Director of Comm. & Econ. Dev. (Micah.Hinkle@cityofsanrafael.org)
Greg Minor, Assistant Director of Comm. & Econ. Dev. (Greg.Minor@cityofsanrafael.org)
Nira Doherty, Counsel for City of San Rafael (ndoherty@bwslaw.com)
700 Irwin Street Partners LLC

Submitted with Exhibits 1-11

EXHIBIT 1



September 12, 2025

700 Irwin Street Partners, LLC
c/o Andrew Junius

1 Bush Street, Suite 600 San
Francisco, CA 94104

Via Email: Via Email: ajunius@reubenlaw.com

Electronic Mail Delivery Only

RE: 700 Irwin Street (PLAN25-046) – 700 Irwin Street (PLAN25-046) – Consistency Review

Mr. Junius,

On November 6, 2024, on behalf of 700 Irwin Street Partners, LLC, you submitted a preliminary application pursuant to Government Code section 64941.1 for a project (the “Project”) located at 700 Irwin Street (APN 014-132-18) (“Project Site”) in the City of San Rafael (“City”). On April 10, 2025, you submitted a formal development application. The project application was deemed incomplete on May 9, 2025. On June 17, 2025, a resubmittal of the project application was received.

On July 9, 2025, the City informed you that pursuant to Government Code Section 65912.100 et seq. (“AB 2011”), the City evaluated the application material and determined that the Project Site does not satisfy the criteria specified under Government Code Section 65912.121. The City also determined that the Project itself does not meet the standards set forth in Government Code Sections 65912.122 and 65912.123. Therefore, the City concluded that the Project is not eligible for the streamlined ministerial review process described in Government Code Section 65912.124.

On July 14, 2025, the City deemed the June 17, 2025 application complete. This letter serves as the City’s consistency determination pursuant to Government Code section 65589.5(j)(2)(A). The Project is inconsistent with applicable objective standards as identified herein.

Consistency Review

This consistency review has been conducted based on application materials submitted to date. If the proposed Project, application materials for the Project, or proposal on the Project Site are modified, these changes may result in updates to the City’s consistency review determinations.

This consistency determination does not analyze the Project's consistency with post-entitlement permit requirements, including but not limited to standards and regulations applicable to any and all building permits (including but not limited to demolition, grading, foundation, and/or vertical infrastructure building permits). Prior to the issuance of any building permit, the Project must comply with all regulations that apply to the project and are imposed by the Sanitary District, Fire Department, Marin Municipal Water District, utility companies, and other local, state, and federal agencies.

PROJECT DESCRIPTION:

The Project would demolish the existing 4-story office building on the Project Site and construct a 17-story, 188-foot 9-inch high building in its place. The project would provide 200 dwelling units. As part of the application, the narrative stated that the Project would include 5,000 square feet of commercial space, but no commercial space is indicated on the plans. The plans also propose a 4,000 square foot civic area consisting of a residential/community room and 3,000 square feet of exterior landscaping on an adjacent parcel. Of the 200 dwelling units, 15 would be affordable to very low-income households and 15 would be affordable to moderate-income households.

CONSISTENCY REVIEW:

The City has evaluated the Project application materials submitted to the City April 10, 2025 and the June 17, 2025 for consistency with applicable City plans, programs, policies, ordinances, standards, and requirements, and other similar provisions in effect on November 6, 2024.

This letter specifies the ways in which the Project is inconsistent with development standards contained in the following portions of the San Rafael Municipal Code:

- Title 11 (Public Works)
- Title 14 (Zoning)
- Title 15 (Subdivisions)
- Title 18 (Protection of Flood Hazard Areas)

This letter also analyzes the Project's consistency with development standards and policies contained in Chapter 9 of the Downtown San Rafael Precise Plan, and the San Rafael General.

Should you have any questions about the information contained in this letter, please do not hesitate to contact me at Margaret.Kavanaugh-Lynch@cityofsanrafael.org.

Sincerely,

Margaret Kavanaugh-Lynch

Margaret Kavanaugh-Lynch

Planning Manager

Attachments

A. SRMC Consistency Review Comments

- i. Title 11 (Public Works)
- ii. Title 14 (Zoning) Consistency Review Comments
 - Table A-1: Chapter 14.16 — Site and Use Regulations
 - Table A-2: Chapter 14.18 — Parking Standards
 - Table A-3: Chapter 14.25 — Environmental and Design Review Permits
- iii. Title 15 (Subdivisions)
- iv. Title 18 (Protection of Flood Hazard Areas)

B. Downtown San Rafael Precise Plan, Chapter 9 Downtown Form Based Code Consistency Review Comments

- i. Table B-1: 2.3.050 T5 Neighborhood Development Standards
- ii. Table B-2: Division 3.1 Site Standards
- iii. Table B-3: Division 3.2 Massing and Façade Articulation Standards
- iv. Table B-4: Division 3.3 Frontage Standards

C. General Plan Consistency Review

- i. Table C-1: Noise Element Policy Analysis

D. Courtesy Comments

CC: Chloe Angelis

Reuben, Junius & Rose, LLP

Via email: cangelis@reubenlaw.com

SRMC TITLE 11 (PUBLIC WORKS) CONSISTENCY REVIEW COMMENTS

<p>11.04.030.010.A - Encroachment permit required</p> <p>It is unlawful for any person, utility or special district to encroach or to make or to cause to be made any encroachment without first having obtained the required encroachment permit or license from the director as prescribed in this chapter. The requirements herein for an encroachment permit or license shall be in addition to any other permit that may be required under this code for the work proposed.</p>		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
<p>The project is <u>inconsistent</u> with this standard. Upon issuance of a qualifying encroachment permit, the project would likely be consistent with this standard.</p>	<p>The architectural plans illustrate portions of the building overhanging the north property line into the Second Street public right-of-way (ROW).</p> <p>None of the submittals to date include an application for an encroachment permit or license that would allow the Project to encroach into the Second Street public ROW.</p>	

SRMC TITLE 14 (ZONING) CONSISTENCY REVIEW COMMENTS

<p>Table A-1: Chapter 14.16 — Site and Use Regulations</p>		
<p>14.16.030 and City Council Resolution 14890 - Location</p> <p>Affordable housing units shall be dispersed throughout the residential development project. Units may be clustered within the residential project when the city determines that such clustering furthers affordable housing opportunities and where such clustering is consistent with Federal and State fair housing laws. (Section C of Resolution 14890)</p>		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
<p>The project is <u>inconsistent</u></p>	<p>All the below market rate units are located on</p>	<p>Modify the plans to demonstrate the</p>

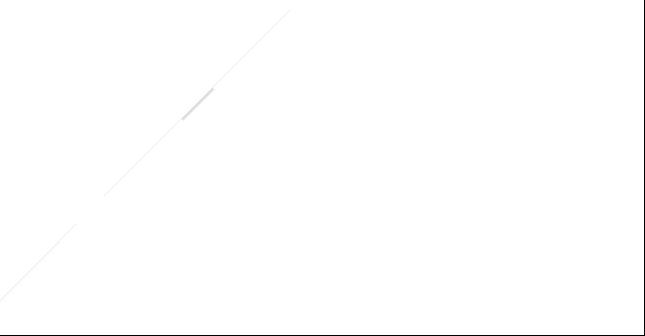

with this standard.	floors 5-11. None of the below-market units are on floors 12-17.	affordable housing units are dispersed throughout the Project.
14.16.030 and City Council Resolution 14890 - Size		
The affordable housing units shall be comparable in size to the market-rate units within the residential development project, as measured by net habitable square footage. For purposes of this provision, “comparable size” means having habitable square footage proportionate to the range of unit sizes provided for market-rate units in the project. (Section C(1) of Resolution 14890).		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this standard.	<p>The project as a whole provides units in the following sizes:</p> <p>Studio: 470 to 560 sf 1-bedroom units: 510 to 925 sf 2-bedroom units: 1035 to 1120 sf 3- bedroom units:1330 sf Penthouse units:1515 to1600 sf</p> <p>Affordable units at 120% AMI provide units in the following sizes: Studio: 525 to 560 sf 1 Bedroom: 640 to 770 sf 2 Bedroom: 930 (only one du)</p> <p>Affordable units at 50% AMI provide units in the following sizes: Studio: 525 to 560 sf 1 Bedroom 670 to 925 sf 2 Bedroom 840 to 1035 sf</p>	Modify the plans to demonstrate the size of affordable housing units is comparable to the market rate units, as measured by net habitable square footage.

<p>14.16.030 and City Council Resolution 14890 - Type</p> <p>The affordable housing units shall possess a distribution of bedroom counts proportionate to that of the market-rate units within the residential development project. (Section C(1) of Resolution 14890).</p>		
<p>Consistency Review</p>	<p>Consideration Factors and Notes</p>	<p>Requested / Recommended Action</p>
<p>The project is <u>inconsistent</u> with this standard.</p>	<p>None of the below-market rate units is a 3-bedroom or a penthouse unit.</p>	<p>Modify the plans to provide affordable units at each bedroom count proportionate to that of market-rate units within the Project, including 3-bedroom units and penthouses.</p>
<p>14.16.030 and City Council Resolution 14890 - Design and materials</p> <p>The exterior and interior design of the affordable housing units shall be of the same design, materials, amenities, quality of finish, and appearance of the other developed units. (Section C(2) of Resolution 14890)</p>		
<p>Consistency Review</p>	<p>Consideration Factors and Notes</p>	<p>Requested / Recommended Action</p>
<p>The project is <u>inconsistent</u> with this standard.</p>	<p>Ten of the market rate 1-bedroom units have a terrace, 42 of the market rate 2- bedroom units have a terrace, all four of the 3-bedroom units have terraces (non-penthouse), and 4 of the eight penthouse units.</p> <p>Only two of the 2-bedroom low-income units have a terrace, three of the moderate-income 2-bedroom units have a terrace.</p>	<p>Modify the plans to provide balconies on affordable units in the same proportion as balconies are provided on market rate units.</p>
<p>14.16.030 and City Council Resolution 14890 - Access</p> <p>The affordable housing units shall have access to the same amenities and common spaces as the market rate units. (Section C(2) of Resolution 14890)</p>		
<p>Consistency Review</p>	<p>Consideration Factors and Notes</p>	<p>Requested / Recommended Action</p>
<p>The Applicant has not submitted sufficient</p>	<p>The common amenities and spaces appear to be accessible to all units based on the plans. However,</p>	<p>Modify the narrative to confirm that affordable units have the same access to</p>

<p>information to enable the City to analyze this standard; therefore the Project is not consistent with this standard.</p>	<p>the application narrative does not affirmatively state that this is the case.</p>	<p>amenities and common spaces as the market rate units.</p>
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<p>14.16.295 - Sight Distance Fencing and improvements or vegetation be located within the established vision triangle (e.g., fifteen feet from the curb return at any intersection or driveway) shall not exceed a height of three feet (3') as measured above the adjacent street pavement. The vision triangle shall be kept free of any visual obstruction between a height of three feet (3') to eight feet (8') above the street grade elevation.</p>		
<p>Consistency Review</p>	<p>Consideration Factors and Notes</p>	<p>Requested / Recommended Action</p>
<p>The Applicant has not submitted sufficient information to enable the City to analyze this standard; therefore the Project is not consistent with this standard.</p>	<p>It is unclear if the bioretention / planting areas adjacent to the driveway are elevated and conflict with the vision triangle.</p>	<p>Show the vision triangle on the drawings to confirm the driveway and corners and demonstrate compliance with this requirement.</p>

Table A-2: Chapter 14.18 — Parking Standards		
14.18.090.E.4 - Bicycle Parking		
The facilities shall provide at least an eighteen inch (18”) clearance from the centerline of adjacent bicycles on the left and right, and at least ten inches (10”) to walls or other obstructions.		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this standard. A waiver pursuant to the SDBL has been sought to exceed bicycle parking requirements.	Bicycle racks are abutting the wall, which does not meet the 10-inch clearance requirement.	
14.18.100.A & B - Parking Space Dimensions		
Standard size parking spaces shall be nine feet (9') by nineteen feet (19') in dimension, except that in downtown, the standard size parking space shall be eight and one-half feet (8.5') by eighteen feet (18') in dimension. Compact parking spaces shall be eight feet (8') by sixteen feet (16') in dimension.		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The Applicant has not submitted sufficient information to enable the City to analyze this standard; therefore the Project is not consistent with this standard. A waiver pursuant to the SDBL has been sought to exceed parking controls.	While there are parking stalls called out as standard and compact on the plans, no dimensions are given.	
14.18.130.A - Parking Facility Dimensions and Design		
The following shows the minimum dimensions for aisle widths and parking spaces for parking facilities. [Please see Zoning Code for all diagrams] Where the configuration and/or layout angle for proposed parking differs from those shown, the dimensions shall be		

<p>prorated and adjusted accordingly, as approved by the city traffic engineer.</p>		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
<p>The project is <u>inconsistent</u> with this Section. A waiver pursuant to the SDBL has been sought to exceed parking controls.</p>	<p>The Project does not comply with the minimum dimensions for aisle widths and parking spaces. Instead of providing parking in the format required by the diagrams included in SRMC Section 14.18.130, the Project proposes to park vehicles in tandem and triple tandem configurations and proposes aisles that are narrower than any of the aisles on the permissible parking configuration diagrams.</p>	
<p>Table A-3: Chapter 14.25 — Environmental and Design Review Permits</p>		
<p>14.25.050.B.2 - Review Criteria</p>		
<p>Development proposed within the downtown mixed use district shall comply with the design criteria and provisions set forth in the Downtown San Rafael Precise Plan Form-Based Code.</p>		
<p>The project is <u>inconsistent</u> with this Section.</p>	<p>As discussed below in Tables B-1 through B-4, the project is inconsistent with the Downtown San Rafael Precise Plan Form-Based Code in various respects.</p>	

SRMC TITLE 15 (SUBDIVISIONS) CONSISTENCY REVIEW COMMENTS

15.06.080 – Sidewalks and Walkways		
Sidewalks along streets intended for residential use shall be a minimum of four feet (4') in width, while sidewalks along streets intended for commercial or industrial use shall be a minimum of six feet (6') in width. All sidewalks and walkways shall be designed to comply with the requirements of the Americans with Disability Act (ADA).		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this Section.	Sheet C2.01 shows tree wells and other proposed improvements along the sidewalk that reduce the sidewalk width to less than 4 feet. The plans will need to be revised to include a minimum 4-foot wide sidewalk, exclusive of the width of any curb in accordance with R302.2 Public Right-of-Way Accessibility Guidelines (PROWAG) requirements.	Revise the Project such that sidewalks are no less than 4 feet wide.

SRMC TITLE 18 (PROTECTION OF FLOOD HAZARD AREAS) CONSISTENCY TABLE

18.50.010.C - Elevation and floodproofing (See Chapter 18.20 definitions for "basement," "lowest floor," "new construction," "substantial damage" and "substantial improvement")		
Residential construction, new or substantial improvement, shall have the lowest floor, including basement, in an AE or AH [FEMA] zone, elevated to a height equal to or exceeding one (1) foot above the base flood elevation plus predicted thirty (30) years settlement; said base flood elevation shall be determined by one of the methods on Section 18.40.030 B of this ordinance.		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this standard.	<p>The narrative for the submitted plans state that the Project would include 5,000 square feet of commercial space, but no commercial space is indicated on the plans. Accordingly, the Project as proposed is a 100% residential development.</p> <p>The subject property is located within a FEMA Special Flood Hazard Area (SFHA) zone AE with a Base Flood Elevation (BFE) of 10 NAVD 88. The Level B1 shown on the drawings is not permitted as it is below the BFE. See also the definition of "lowest floor", which states that "For residential structures, all subgrade enclosed areas are prohibited as they are considered to be basements (see "Basement" definition). This prohibition includes below-grade garages and storage areas.</p>	Revise the plans to show level B1 elevated above the BFE or revise the project description and plans such that the Project is not a 100% residential development.

DOWNTOWN SAN RAFAEL PRECISE PLAN, CHAPTER 9 DTFBC CONSISTENCY REVIEW COMMENTS

Table B-1: 2.3.050 T5 Neighborhood Development Standards		
C. Building Placement (Setback)		
Per Table C. Building Placement (Setback) on page 267 of the Downtown San Rafael Precise Plan, the minimum setback (distance from right-of-way “ROW”/lot line) is zero (0) feet.		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this standard.	The plans show that portions of the structure are constructed over and across the property line adjacent to the Canal. Portions of the structure also extend across the property line and into the ROW on Second Street.	Revise the plans to observe setbacks and not construct over the property lines.
C. Building Placement (Civic Space)		
Per Table C. Building Placement (Civic Space) on page 267 of the Downtown San Rafael Precise Plan, projects on lots of 15,000 – 30,000 square feet must provide a minimum of 200 square feet of civic space.		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this standard.	The site is 22,550 square feet and therefore the project is required to provide a minimum of 200 square feet of civic space. While the project narrative states that civic space will be provided, the plans do not depict the required civic space.	Clearly identify the size and nature of the civic space on the site plans.
E. Building Form and Maximum Envelope (Overall Building Height)		
Per Table E. Building Form and Maximum Envelope (Overall Building Height) on page 268 of the Downtown San Rafael Precise Plan, buildings in the T5N 50/70 zone must be no taller than 50 feet (base height).		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this standard. A waiver	The project exceeds 50 feet in height.	

pursuant to the SDBL has been sought to exceed height standards.		
E. Building Form and Maximum Envelope (Stepbacks)		
Per Table E. Building Form and Maximum Envelope (Stepback) on page 268 of the Downtown San Rafael Precise Plan, buildings in the T5N 50/70 zone must be stepped back by a minimum of 10 feet at heights above 45 feet.		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this Section.	The project does not include any stepbacks.	Revise the design to provide the required stepbacks.
H. Parking. Required Spaces (min.) for New Buildings		
Per Table H. Parking on page 271 of the Downtown San Rafael Precise Plan, buildings containing residential uses must provide 1 bicycle space per studio or 1-bedroom unit, 2 bicycle spaces per 2-bedroom unit, and 3 bicycle spaces per 3-bedroom unit.		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this standard. A waiver pursuant to the SDBL has been sought to exceed parking controls..	The number of bicycle parking spaces provided is 60, when 271 are required.	

Table B-2: Division 3.1 Site Standards		
<p>3.1.020 - Screening</p> <p>Per Table 3.1.020 on page 289 of the Downtown San Rafael Precise Plan, the maximum height for freestanding walls on sites zoned T5N 50/70 is 3 feet in the front, 3 feet along side streets, 9 feet along the side, and 9 feet in the rear.</p>		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
<p>The Applicant has not submitted sufficient information to enable the City to analyze this standard; therefore the Project is not consistent with this standard.</p>	<p>The site plan does not contain enough information to determine whether the Project is consistent with this standard.</p>	<p>Identify height of freestanding walls on the plans (Sheet A3.03).</p>
<p>3.1.040 - Parking and Loading</p> <p>Per Table 3.1.040 on page 293 of the Downtown San Rafael Precise Plan, residential projects of 101 or more units must provide 2 carshare parking spaces.</p>		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
<p>The project is <u>inconsistent</u> with this standard. A waiver pursuant to the SDBL has been sought to exceed parking controls.</p>	<p>The Project does not provide any carshare spaces.</p>	

Table B-3: Division 3.2 Massing and Façade Articulation Standards

3.2.050 - Corner Elements

Per standard 3.2.050 on pages 302-303 of the Downtown San Rafael Precise Plan, new façades shall be designed to include at least one corner element. Three types of corner elements are defined by reference to diagrams: a square corner element, an octagonal corner element, and a chamfer corner element. Corner elements shall incorporate at least 1 entire story within the composition and may extend from the ground floor through the top of the building. Corner elements may be recessed or projected from the main façade by up to 4 feet.

Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project is <u>inconsistent</u> with this standard.	The project does not include any of the corner elements specified in the Downtown Precise Plan.	Revise the Project to include a corner element.

Table B-4: Division 3.3. Frontage Standards		
3.3.010 – Frontage Standards		
Per standard 3.3.010 on page 308 of the Downtown San Rafael Precise Plan, each building shall include at least one frontage type along each street frontage or along a civic space. Frontage types are defined in pages 310-320 of the Downtown San Rafael Precise Plan.		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The project appears to be partially <u>inconsistent</u> with this section. Additional information is needed in order to determine consistency with this standard.	<p>In the T5N 50/70 Open zone, permitted frontage types include Porch Projecting (3.3.030), Porch Engaged (3.3.040), Dooryard (3.3.050), Stoop (3.3.060), Forecourt (3.3.070), Shopfront (3.3.090), Terrace (3.3.100), and Gallery (3.3.110).</p> <p>As currently proposed, it appears that the project incorporates the Porch Engaged frontage type on the Second Street side of the building. It is unclear what frontage type is intended for Irwin Street or the Canal front.</p>	Provide additional details on the frontage types proposed. Note that each frontage type provides objective standards, such as finish levels above the sidewalk, depth of entries, ground floor glazing, etc.

GENERAL PLAN CONSISTENCY REVIEW COMMENTS

iTable C-1: Noise Element Policy Analysis		
Policy N-1.2 - Maintaining Acceptable Noise Levels		
<ul style="list-style-type: none"> a) New development shall not increase noise levels by more than 3 dB Ldn in a residential area, or by more than 5 dB Ldn in a non-residential area. b) New development shall not cause noise levels to increase above the “normally acceptable” levels shown in Table 9-2. c) For larger projects, the noise levels in (a) and (b) should include any noise that would be generated by additional traffic associated with the new development. d) Projects that exceed the thresholds above may be permitted if an acoustical study determines that there are mitigating circumstances (such as higher existing noise levels) and nearby uses will not be adversely affected. 		
Consistency Review	Consideration Factors and Notes	Requested / Recommended Action
The Applicant has not submitted sufficient information to enable the City to analyze this standard; therefore the Project is not consistent with this standard.	The City cannot evaluate consistency with noise thresholds because no noise and vibration analysis has been submitted.	Submit a Noise and Vibration Analysis that demonstrates that the Project is consistent with this standard.

Courtesy Comments

This section contains courtesy comments intended to provide additional information relevant to the Project.

1. The City of San Rafael holds a Public Pedestrian Access Easement over a portion of the west side of the Project Site, recorded as DN 1988-22240. It is shown graphically on Assessor Map Book 14, page 13, County of Marin California and on the document submitted by the applicant created by BKS Engineers, titled ROW Exhibit of Irwin Street. The Project appears to conflict with the easement, and the City intends to enforce its property rights guaranteed by the easement. Please ensure the Project does not conflict with the easement.
2. In accordance with section 11.04.040.010 of the municipal code, an encroachment permit is required from the Department of Public Works prior to conducting any work within the public right of way (ROW) or any time the ROW is restricted. You will be required to apply for an encroachment permit if construction of the Project will restrict or be within the ROW. In accordance with section 11.04.040.030.B.6, such application will be reviewed for accessibility.
3. The San Rafael Transportation Analysis Guidelines (February 2022)¹ require a Transportation Impact Analysis and CEQA review for Tier 3 projects. (p. 6.) The Project is a Tier 3 Project, so you will be required to prepare a Transportation Impact Analysis.
4. Section 9.30.151 of the San Rafael Municipal Code requires applicants to develop, submit and implement a Stormwater Control Plan (SCP) when required by the Phase II Stormwater Permit Provision E.12, or where required by the nature and extent of a proposed project and where deemed appropriate by the agency. Phase II Stormwater Permit Provision E.12(c)(ii) requires the City to regulate all projects that create and/or replace 5,000 square feet or more of impervious surface (Regulated Projects). The City regulates such projects by requiring submission of a Stormwater Control Plan. The Project creates more than 5,000 square feet of total impervious area, so it is subject to the requirements in Section 9.30.151. Per Section 9.30.151.A, the SCP shall follow the appropriate SCP template, based on project type, in the most recent version of the BASMAA Post Construction Manual. The appropriate SCP template for this project is found in Appendix D of the BASMAA Post-Construction Manual. The stormwater management plan exhibit shown on sheet C3.01 does not follow this template.

¹ Available at <https://www.cityofsanrafael.org/documents/sanrafael-ta-guidelines-feb-2022/>

You will be required to provide a complete stormwater control plan with narrative report, prepared per the template format provided as Appendix D in the BASMAA Post-Construction Manual in accordance with section 9.30.151A. The storm water control plan shall account for all private improvements in and outside of the property including undisturbed areas within the property line. Be sure to provide a cross-section detail of the treatment facilities demonstrating compliance with BASMAA standards. More information and templates are available from the MCSTOPPP website. See tools and guidance, and post-construction requirements at: <https://mcstoppp.org/2020/03/new-and-redevelopment/>

EXHIBIT 2

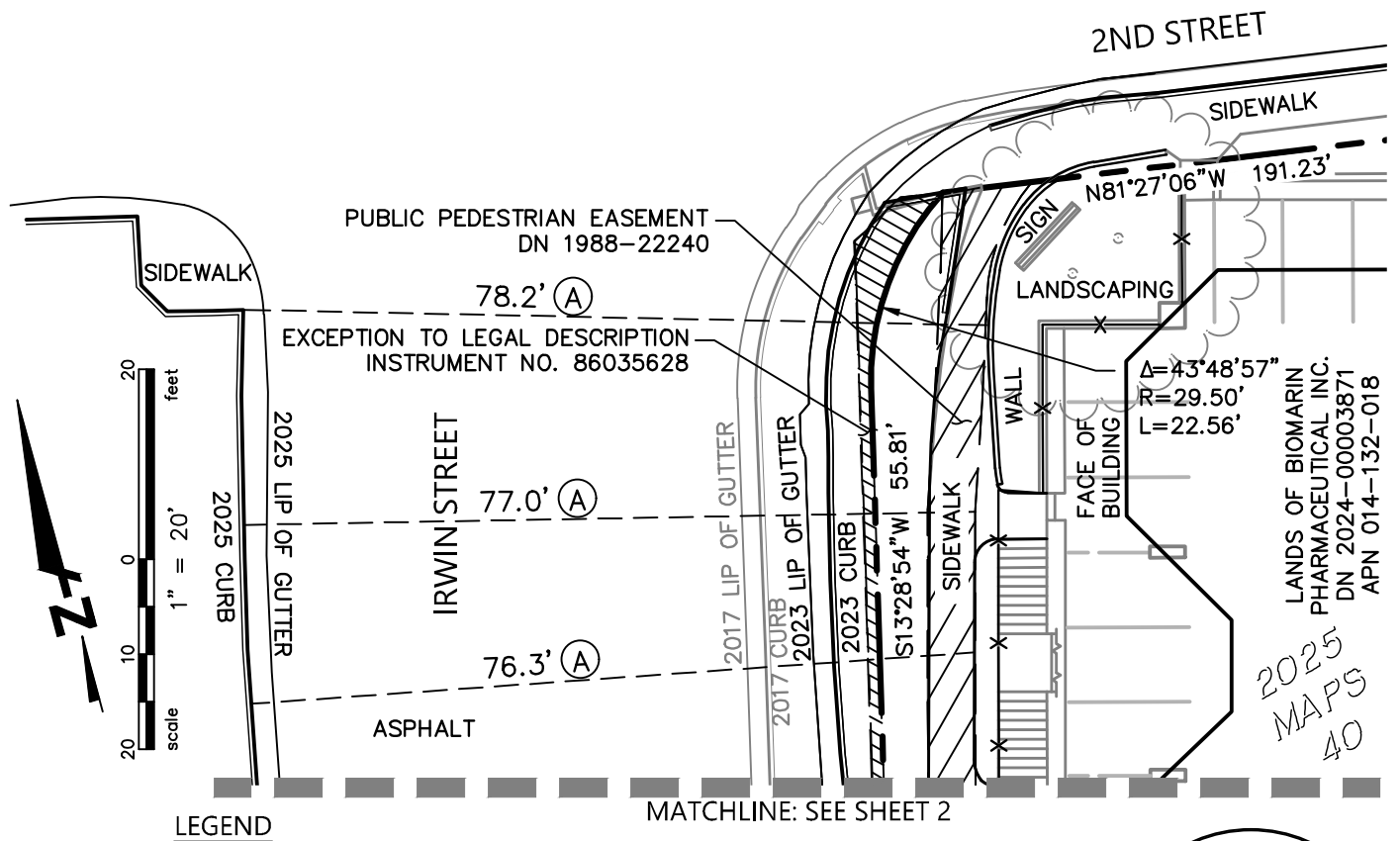
TOPOGRAPHIC INFORMATION SHOWN HEREON IS BASED UPON FIELD SURVEYS PERFORMED BY BKF ENGINEERS ON FEBRUARY 16, 2017, DECEMBER 13 & 14, 2023, & FEBRUARY 22, 2025.

THIS EXHIBIT DEPICTS CHANGES TO IMPROVEMENTS ALONG THE EASTERLY CURB LINE OF IRWIN STREET AS WELL AS THE SIDEWALK WITHIN THE PUBLIC PEDESTRIAN EASEMENT RECORDED UNDER DN 1988-0022240 THAT TOOK PLACE BETWEEN FIELD SURVEYS PERFORMED IN 2017 & 2023.

EASEMENT TO FACE OF CURB MEASUREMENTS SHOWN HEREON ARE MEASURED FROM THE EASTERLY LINE OF THE PUBLIC PEDESTRIAN EASEMENT RECORDED UNDER DN 1988-0022240 PERPENDICULAR TO THE FACE OF CURB ON THE WESTERLY SIDE OF IRWIN STREET AND HIGHWAY 101 OFF RAMP.

SUBJECT PROPERTY BOUNDARIES ARE SHOWN ON THAT RECORD OF SURVEY RECORDED IN BOOK 2025 OF MAPS, PAGE 40, MARIN COUNTY RECORDS.

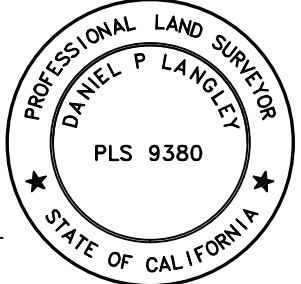
- (A) INDICATES MEASUREMENT FROM THE EASTERLY LINE OF THAT PUBLIC PEDESTRIAN EASEMENT RECORDED UNDER DOCUMENT NUMBER 1988-22240, MARIN COUNTY RECORDS, AND MEASURED PERPENDICULAR TO THE FRONT FACE OF CURB ON THE WESTERLY SIDE OF IRWIN STREET.
- (B) INDICATES MEASUREMENT FROM THE SOUTHWEST CORNER OF THE LANDS BIOMARIN PHARMACEUTICAL INC., BY GRANT DEED RECORDED UNDER DOCUMENT NUMBER 2024-0003871, MARIN COUNTY RECORDS, AND MEASURED PERPENDICULAR TO THE INSIDE FACE OF WALL ON THE WEST SIDE OF THE IRWIN STREET/HIGHWAY 101 OFF RAMP.



- PROPERTY LINE
- [Hatched Box] PUBLIC PEDESTRIAN EASEMENT
- [Diagonal Lines Box] EXCEPTION TO LEGAL DESCRIPTION
- [Dotted Box] RIGHT OF WAY GRANT
- APN ASSESSOR'S PARCEL NUMBER
- DN DOCUMENT NUMBER

PRELIMINARY
NOT FOR CONSTRUCTION
DATE: 11/19/2025

DANIEL P LANGLEY PLS 9380



BKF ENGINEERS
200 4th STREET
SUITE 300
SANTA ROSA, CA 95401
(707) 583-8500
www.bkf.com

Subject RIGHT OF WAY EXHIBIT OF 700 IRWIN ST
SAN RAFAEL, CALIFORNIA
Job No. 20232412-11
By DAC Date NOV 2025 Chkd. DPL
SHEET 1 OF 2

TOPOGRAPHIC INFORMATION SHOWN HEREON IS BASED UPON FIELD SURVEYS PERFORMED BY BKF ENGINEERS ON FEBRUARY 16, 2017, DECEMBER 13 & 14, 2023, & FEBRUARY 22, 2025.

THIS EXHIBIT DEPICTS CHANGES TO IMPROVEMENTS ALONG THE EASTERLY CURB LINE OF IRWIN STREET AS WELL AS THE SIDEWALK WITHIN THE PUBLIC PEDESTRIAN EASEMENT RECORDED UNDER DN 1988-0022240 THAT TOOK PLACE BETWEEN FIELD SURVEYS PERFORMED IN 2017 & 2023.

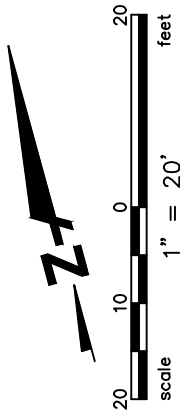
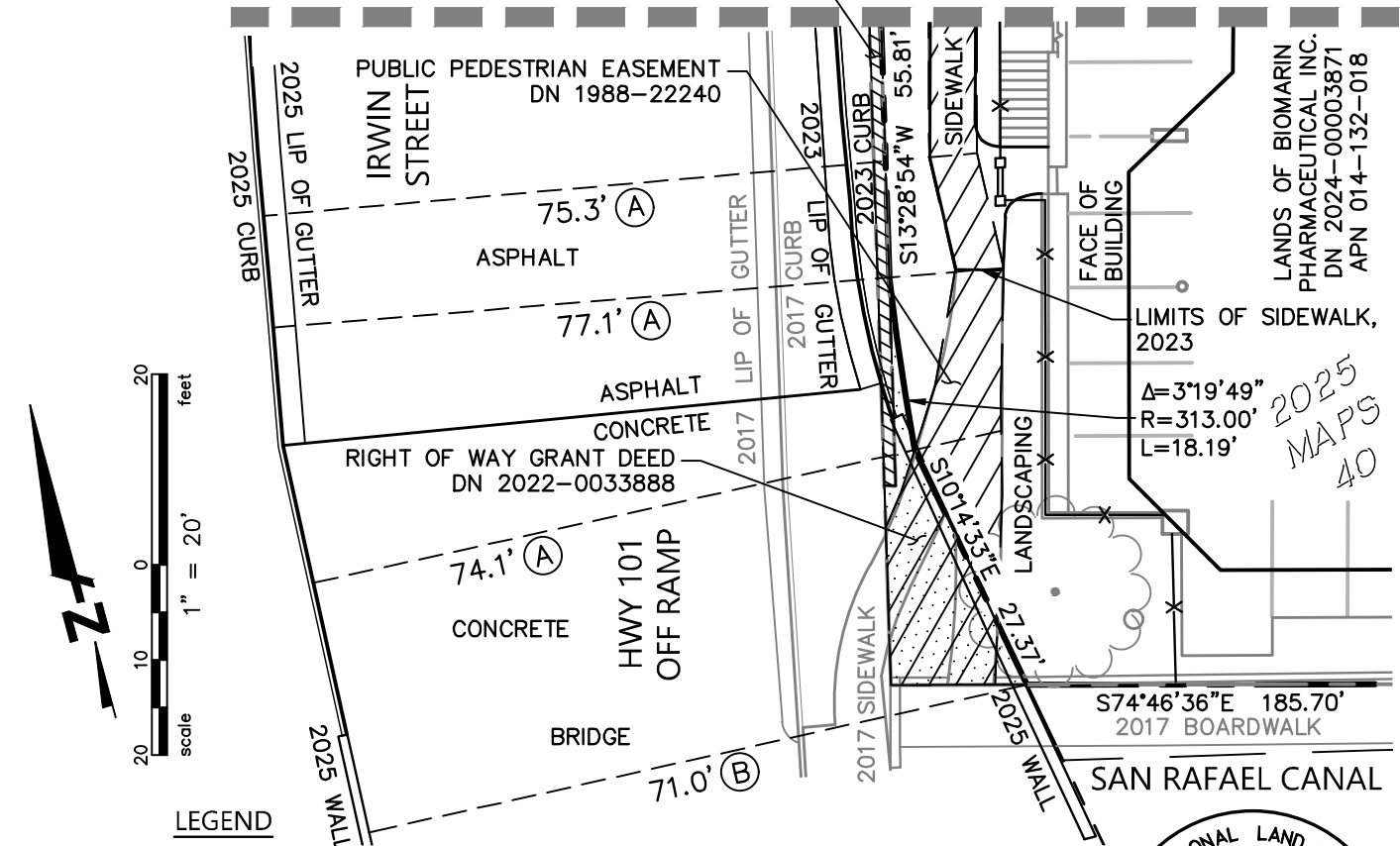
EASEMENT TO FACE OF CURB MEASUREMENTS SHOWN HEREON ARE MEASURED FROM THE EASTERLY LINE OF THE PUBLIC PEDESTRIAN EASEMENT RECORDED UNDER DN 1988-0022240 PERPENDICULAR TO THE FACE OF CURB ON THE WESTERLY SIDE OF IRWIN STREET AND HIGHWAY 101 OFF RAMP.

SUBJECT PROPERTY BOUNDARIES ARE SHOWN ON THAT RECORD OF SURVEY RECORDED IN BOOK 2025 OF MAPS, PAGE 40, MARIN COUNTY RECORDS.

- (A) INDICATES MEASUREMENT FROM THE EASTERLY LINE OF THAT PUBLIC PEDESTRIAN EASEMENT RECORDED UNDER DOCUMENT NUMBER 1998-22240, MARIN COUNTY RECORDS, AND MEASURED PERPENDICULAR TO THE FRONT FACE OF CURB ON THE WESTERLY SIDE OF IRWIN STREET.
- (B) INDICATES MEASUREMENT FROM THE SOUTHWEST CORNER OF THE LANDS BIOMARIN PHARMACEUTICAL INC., BY GRANT DEED RECORDED UNDER DOCUMENT NUMBER 2024-0003871, MARIN COUNTY RECORDS, AND MEASURED PERPENDICULAR TO THE INSIDE FACE OF WALL ON THE WEST SIDE OF THE IRWIN STREET/HIGHWAY 101 OFF RAMP.

EXCEPTION TO LEGAL DESCRIPTION INSTRUMENT NO. 86035628

MATCHLINE: SEE SHEET 1

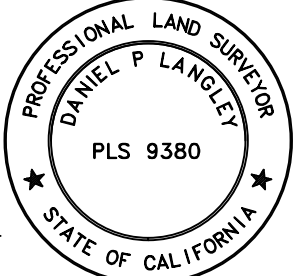


LEGEND

- PROPERTY LINE
- PUBLIC PEDESTRIAN EASEMENT
- EXCEPTION TO LEGAL DESCRIPTION
- RIGHT OF WAY GRANT
- APN ASSESSOR'S PARCEL NUMBER
- DN DOCUMENT NUMBER

PRELIMINARY
NOT FOR CONSTRUCTION

DATE: 11/19/2025



DANIEL P LANGLEY PLS 9380

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Subject RIGHT OF WAY EXHIBIT OF 700 IRWIN ST
SAN RAFAEL, CALIFORNIA
Job No. 20232412-11
By DAC Date NOV 2025 Chkd. DPL
SHEET 2 OF 2

176448-PW

88 22240

RECORDED AT REQUEST OF

AGENCY SHOWN

1988 APR 29 AM 8:00

OFFICIAL RECORDS
MARIN COUNTY CALIFORNIA
JAMES J. DAL BONRecorded at the request of:Office of the City Attorney
City of San Rafael
Mary Casey, Asst. City Attorney
P. O. Box 60
San Rafael, CA 94915-0060After recordation, return to:Office of the City Attorney
City of San Rafael
Mary Casey, Asst. City Attorney
P. O. Box 60
San Rafael, CA 94915-0060**No Fee GC8103**A.P. No. 14-132-12GRANT OF PERPETUAL EASEMENT

IRWIN PLAZA ASSOCIATES, a California General Partnership, does hereby grant to the CITY OF SAN RAFAEL, acting as Trustee pursuant to Chapter 83 of the Statutes of 1923, as amended, with respect to their sovereign interests by virtue of the grant in trust as amended; and the STATE OF CALIFORNIA, acting by and through the State Lands Commission as to its supervisory and reversionary interest reserved to it under Chapter 83, Statutes of 1923, as amended (hereinafter referred to as "Grantees"), a Public Pedestrian Easement as described on Exhibit "A" attached hereto and incorporated herein, in perpetuity, pursuant to the "AGREEMENT FOR THE SETTLEMENT OF A TITLE DISPUTE WITHIN THE CITY OF SAN RAFAEL, COUNTY OF MARIN AMONG THE CITY OF SAN RAFAEL, DAVOOD KOHANZADEH, AND THE CALIFORNIA STATE LANDS COMMISSION", dated June 25, 1986 and recorded on July 10, 1986 in the Official Records, Marin County, California, as Recorder's Serial No. 86-035627.

This Grant of Easement is made to Grantees pursuant to Paragraph 23 of said AGREEMENT and, specifically, is a relocation of said Public Pedestrian Easement as provided for in Paragraph 7 of the "TERMS" of said AGREEMENT appearing on page 10 thereof.

88 22240

As a result of said AGREEMENT and this Grant, the easement granted herein is the sole easement required to be granted pursuant to said AGREEMENT.

IRWIN PLAZA ASSOCIATES, a California General Partnership

By: Davood Kohanzadeh aka Davood Kohan, General Partner

By: *Davood Kohan*

By: NORTH COAST HOLDING COMPANY, a California General Partnership, General Partner

By: *[Signature]*
General Partner

By: *[Signature]*
General Partner

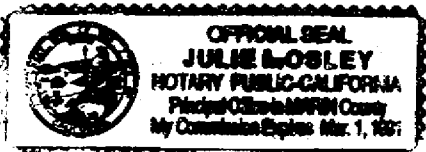
By: *[Signature]*
General Partner

[Signature]
General Partner
[Signature]
General Partner
[Signature]
General Partner
STATE OF CALIFORNIA
COUNTY OF MARIN

)
) ss
)

On March 31, 1988 before me, the undersigned, a Notary Public in and for said State, personally appeared DAVOOD KOHANZADEH aka DAVOOD KOHAN, personally known to me or proved to me on the basis of satisfactory evidence to be the person who executed the within instrument as ~~the~~ General Partner of ~~the~~ ~~partnership of~~ IRWIN PLAZA ASSOCIATES, the partnership that executed the within instrument, and acknowledged to me that he executed the same on behalf of IRWIN PLAZA ASSOCIATES, a partnership, and that said last named partnership executed the same.

[Signature]
NOTARY PUBLIC



88 22240

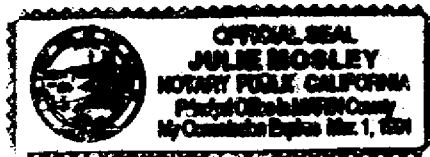
STATE OF CALIFORNIA :

: ss.

COUNTY OF Marin :

On this 31 day of March in the year 1988. before me Julie Mosley, a Notary Public in and for said county and state, personally appeared Charles W. Bennett, Eugene DiBasilio, Donald A. Leaf, P. Roy Vallarino, Paul Hickman, and G. Allen Pickrel, personally known to me to be all of the partners of North Coast Holding Company a partnership; said partnership being known to me to be one of the partners of Irwin Plaza Associates, the partnership that executed the within instrument and acknowledged to me that they executed the same as partners of the partnership first above named; that said partnership executed the same as a partner of Irwin Plaza Associates, and that said last name partnership executed the same.

Julie Mosley
Notary Public



88 22240

EXHIBIT A

DESCRIPTION

All that certain real property situate in the City of San Rafael, County of Marin, State of California, described as follows:

An easement for pedestrian ingress, egress and access purposes more particularly described as follows:

Beginning at the Southwesterly corner of the parcel of land described in the Deed to Davood Kohanzadeh, recorded October 25, 1984 as Instrument No. 84050079, Marin County Records; thence along the Easterly line of Irwin Street North $13^{\circ} 28' 54''$ East 16.55 feet; thence leaving said Easterly line North $38^{\circ} 47' 49''$ East 4.06 feet; thence North $33^{\circ} 07' 53''$ East 11.76 feet; thence North $25^{\circ} 07' 52''$ East 12.42 feet; thence North $02^{\circ} 05' 56''$ East 11.96 feet; thence north $15^{\circ} 14' 45''$ East 30.34 feet; thence North $19^{\circ} 03' 46''$ East 11.93 feet; thence North $17^{\circ} 51' 50''$ East 16.25 feet; thence North $15^{\circ} 58' 22''$ East 5.34 feet to the Southerly line of Second Street; thence along the Southerly line, South $81^{\circ} 27' 06''$ East 9.56 feet; thence leaving said Southerly line along a curve Southwesterly whose center bears South $34^{\circ} 03' 28''$ East 20.00 feet, through a central angle of $38^{\circ} 04' 42''$, an arc distance of 13.29 feet; thence South $17^{\circ} 51' 50''$ West 10.91 feet; thence South $19^{\circ} 03' 46''$ West 11.75 feet; thence South $15^{\circ} 14' 45''$ West 29.60 feet; thence South $02^{\circ} 05' 56''$ West 12.40 feet; thence South $16^{\circ} 39' 55''$ West 13.50 feet; thence South $14^{\circ} 38' 55''$ West 10.25 feet; thence South $17^{\circ} 30' 00''$ West 20.01 feet to the Northerly line of San Rafael Canal; thence along the Northerly line of San Rafael Canal, north $74^{\circ} 46' 36''$ West 10.94 feet to the point of beginning.

88 22240

CERTIFICATE OF ACCEPTANCE

The City of San Rafael hereby accepts the attached
Grant of Perpetual Easement over Assessor's Parcel
#14-132-12 in San Rafael, California

By: *Lawrence E. Mulryan*
LAWRENCE E. MULRYAN
Mayor
CITY OF SAN RAFAEL

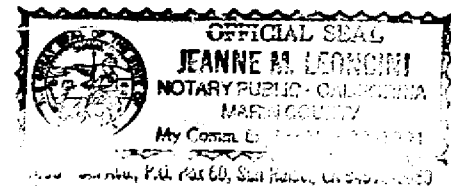
Date: *April 8, 1988*

STATE OF CALIFORNIA)
COUNTY OF MARIN) ss.

On this 8th day of APRIL, 1988, before me, JEANNE M. LEONCINI, a
Notary Public in and for the said County and State, residing therein, duly
commissioned and sworn, personally appeared LAWRENCE E. MULRYAN,
personally known to me (~~or proved to me on the basis of satisfactory~~
evidence) to be the Mayor of the City of San Rafael that executed the within
document and acknowledged to me that said City of San Rafael did execute the
same.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal
the day and year in this certificate first above written.

Jeanne M. Leoncini
JEANNE M. LEONCINI, NOTARY PUBLIC in and for
said County and State.



88 22240

RECORDED AT THE REQUEST OF,
AND WHEN RECORDED MAIL TO:

State of California
State Lands Commission
1807 13th Street
Sacramento, CA 95814

Attn.: Blake D. Stevenson
Telephone: 916-322-2277

STATE OF CALIFORNIA - OFFICIAL BUSINESS
Document entitled to free recordation
pursuant to Government Code Section 27383
NO TAX DUE

SPACE ABOVE THIS LINE FOR RECORDER'S USE

**CERTIFICATE OF ACCEPTANCE AND
CONSENT TO RECORDING**

(Govt. Code 27281)

(Sovereign Interest)

SLC No.: W23817
AD 53
Dictation
and typing: Theresa Cairel

This is to certify that the State of California, acting by and through the State Lands Commission, an Agency of the State of California, hereby accepts the right, title and interest in real property conveyed by, and consents to the recordation of, the attached deed or grant, dated March 31, 1988, from Irwin Plaza Associates, a California General Partnership

to the State of California.

The said interest in real property is accepted by the State of California in its sovereign capacity in trust for the people thereof, as real property of the legal character of tidelands and submerged lands.

This acceptance and consent to recording is executed by and on behalf of the State of California by the State Lands Commission, acting pursuant to law, as approved and authorized by its Minute Item No. 35 of its regular public meeting on June 26, 1986, by its duly authorized undersigned officer.

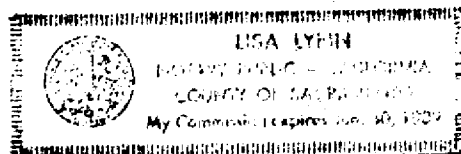
Dated: 4/5/88

State Lands Commission
By: James F. Trout

STATE OF CALIFORNIA)
COUNTY OF SACRAMENTO) ss.

On this 5th day of April, 1988, before me, the undersigned, a Notary Public in and for the State of California, with principal office in the County of Sacramento, personally appeared James F. Trout, known to me to be the Assistant Executive Officer of the STATE LANDS COMMISSION, STATE OF CALIFORNIA, the Commission that executed the within instrument, known to me to be the person who executed the within instrument on behalf of the Commission therein named, and acknowledged to me that such Commission executed the within instrument pursuant to a resolution of its Commissioners.

WITNESS my hand and official seal.



Lisa Lynn
LISA LYNN
Notary Public in and for said
County and State

EXHIBIT 3

Boardwalk Looking South Across Channel



Canal Alliance Building (South Side of Channel)



Municipal Building (Across Project Site, South Side of Channel)



South Side of Project Site and Existing Boardwalk



EXHIBIT 4

700 Irwin Street Aerial
Google Maps – November 11, 2025

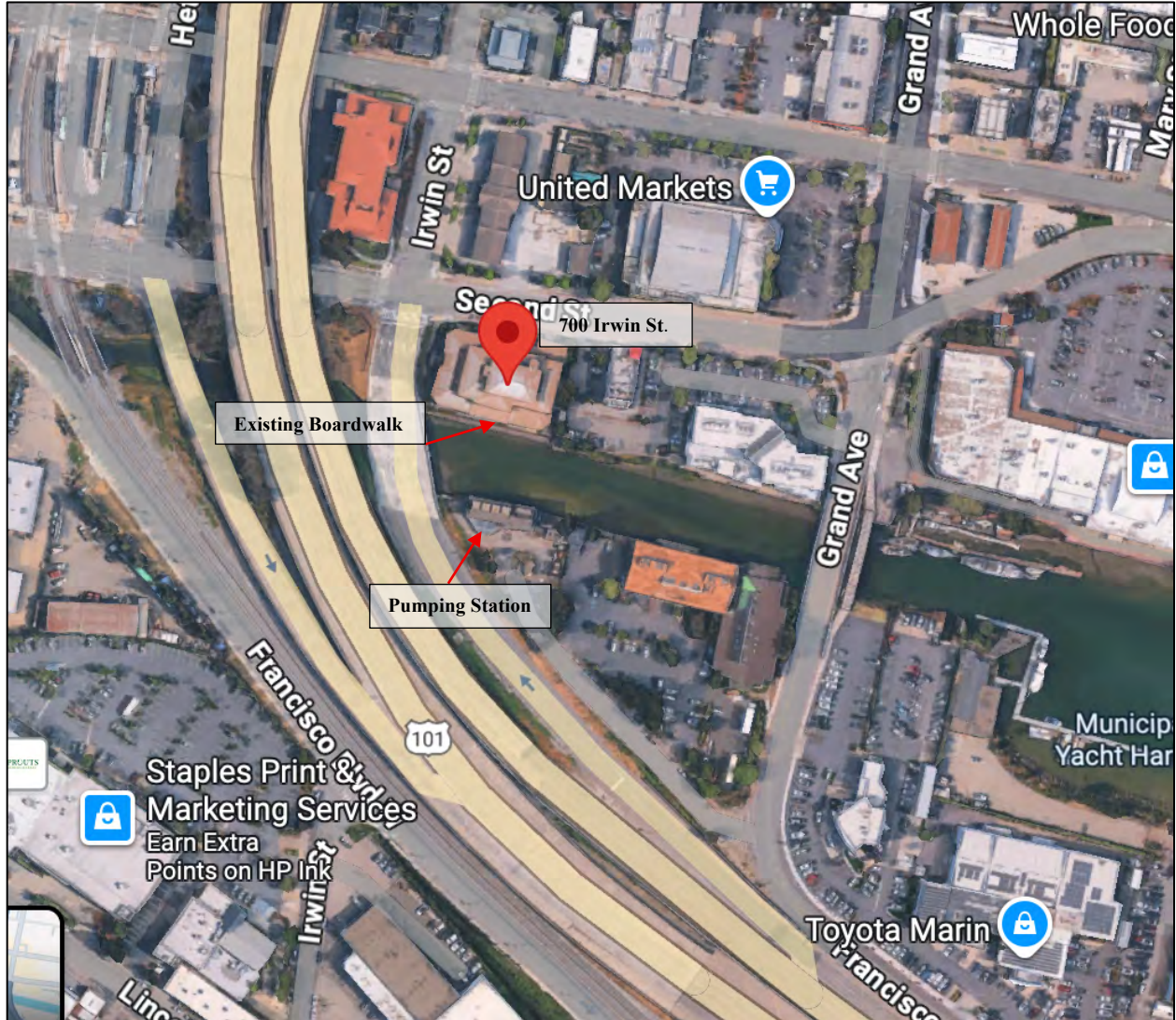


EXHIBIT 5



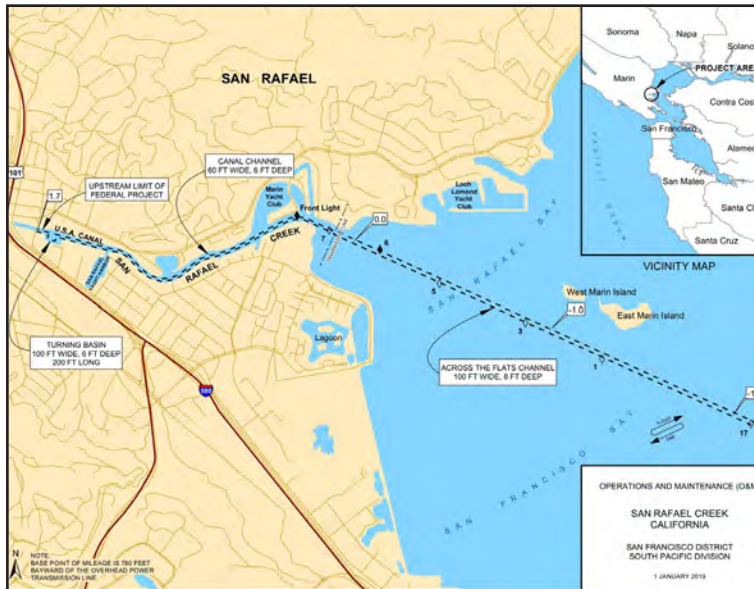
San Rafael Creek Federal Navigation Channel Factsheet



A source of significant commercial, public safety and recreational activity, the San Rafael Creek Federal Navigation Channel runs through the heart of Marin County to the San Francisco Bay.

1918 Rivers and Harbors Act federal authorization provided for maintenance dredging every 6 to 8 years

2002 Last full dredging of the San Rafael Federal Navigation Channel



<p>T Federally authorized depth</p> <p><i>Inner Channel</i> 6 ft</p> <p><i>Outer Channel</i> 8 ft</p>	<p>T Current conditions</p> <p>2.5 ft</p> <p>4.8 ft</p>
---	--



An Urgent Need.

Presidentially-Declared Disasters in winter 2017 and 2019 for severe winter storms, flooding, and mudslides drove significant volumes of sediment from the surrounding hillsides into the Channel, further shoaling the channel and rendering large portions of the Channel non-navigable in low tide.

\$1,018,000 Engineering and Environmental compliance (*secured in FY 2020 work plan*)

\$13,000,000 Dredging operations (*still needed*)

What's at stake for San Rafael

San Rafael Channel is an urban waterfront.

12,000 Residents live within 0.5 miles of the Channel

2,000 Boats moor in the Channel's five harbors, including 135 live aboards



Critical Public Safety Operations

3 Police and Fire rescue boats operate daily responding to **1,380** water incidents in the last 15 years



Firefighting defense

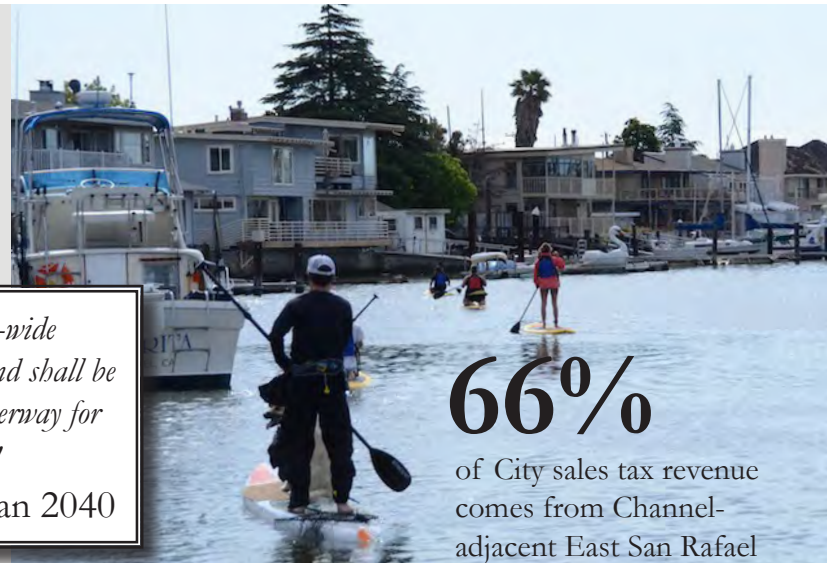
Sediment buildup has eliminated the use of the Channel's turning basin as a water pick up in fighting hillside fires—increasing critical response times by more than 20 minutes.

A Regional Economic Engine

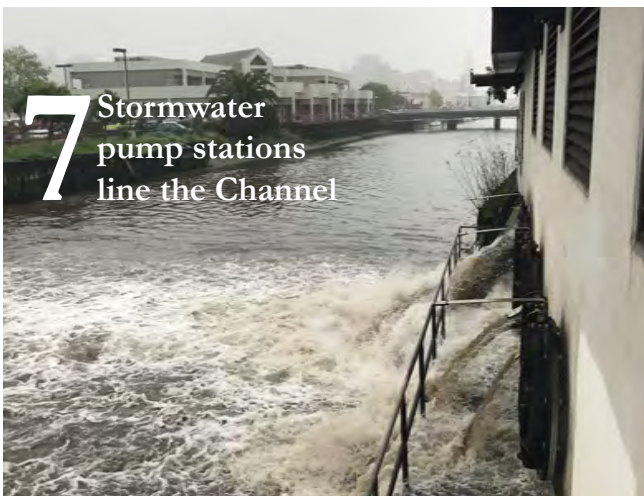
134 businesses within 400ft of the Channel grossed **\$191m** in taxable sales in 2019



"The Channel is a community-wide asset for marine related uses and shall be maintained as a navigable waterway for commercial and other activity."
- San Rafael General Plan 2040




66% of City sales tax revenue comes from Channel-adjacent East San Rafael



7 Stormwater pump stations line the Channel

Real Flood Risk and Damage

The seven stations pump stormwater directly into the Channel, protecting the County's most heavily populated FEMA floodzone.

 **28** FEMA repetitive loss properties already exist within the floodzone

The FEMA approved Marin County Local Hazard Mitigation Plan identifies dredging as a high priority project to protect against flood damage. The plan also cites data and research which expects winter storm events to increase in severity and frequency.

EXHIBIT 6



San Rafael Canalfront

DESIGN GUIDELINES

DECEMBER 2009

CANALFRONT ADVISORY COMMITTEE

Maite Duran, co-chair
Jacqueline Schmidt, co-chair
Bruce Abbott
Don Blayney
Jeanette M. Broering, Pickleweed Park
Advisory Board member
Joanne Gordon
Russell Hamel
Anne Laird-Blanton
Maribeth Lang, Planning Commissioner
Richard M. Lockman
Don Magdanz, Bicycle & Pedestrian
Committee member
Sue McCullough
Cyr Miller, City Councilmember
Catherine Orman
Carol Patterson, Redevelopment Citizen
Advisory Committee member
Fernando Quezada
Steve Raghianti
Michael Watenpaugh
Craig Yates, Park & Recreation
Commissioner

PROJECT VOLUNTEERS

David Israel
Phil Abey
Michele Rodriguez

CITY COUNCIL

Al Boro, Mayor
Barbara Heller
Cyr Miller
Greg Brockbank
Damon Connolly

CITY STAFF

Ken Nordhoff, City Manager
Bob Brown, Director, Community Development
Linda M. Jackson, Principal Planner
Suvra Chakrabarti, Traffic Engineer II
Anne Derrick, Administrative Assistant III
Chris Gray, Fire Chief
Paul Jensen, Planning Manager
Lee Johnson, Building Inspector
Katie Korzun, Economic Development Coordinator
Richard Landis, Administrative Supervisor, Public
Works
Nader Mansourian, Assistant Director, Public Works
Carlene McCart, Director, Community Services
Carol Simonson, Planning Intern

DESIGN CONSULTANTS

Manuela King, Royston Hanamoto
Alley & Abey (RHAA)
Lisa Orr, RHAA
John Martin, RHAA
Dana Merker, Patri Merker Architects
Brian Capelli, Patri Merker Architects
John Ware, Endres Ware

ENVIRONMENTAL CONSULTANTS

Bob Berman, Nichols Berman
Environmental Planners
Jeffrey Peters, Questa Engineering Corp.

COMMUNITY OUTREACH CONSULTANTS

Hannikah Diaz, Canal Welcome Center
Douglas Mundo, Canal Welcome Center

PARTNER AGENCIES

This project has been funded in part by grants from the Association of Bay Aovernments and from the California Department of Transportation.

Maureen Gaffney, Association of Bay Area
Governments
Dianne Steinhauer, Transportation
Authority of Marin
Robert Talley, California Department of
Transportation
Laura Thompson, Association of Bay Area
Governments
Bill Whitney, Transportation Authority of
Marin



INTRODUCTION

The Canal is widely recognized as an under-utilized community resource and, in many ways, can be thought of as an open space - a public amenity for all the neighborhoods of San Rafael. It also provides a much sought after literal connection to the San Rafael and San Francisco Bays and the San Francisco Bay Trail (a project of the Association of Bay Area Governments to develop a trail around the perimeter of San Francisco and San Pablo Bays).

The *San Rafael Canalfront Conceptual Design Plan* envisions providing and improving access to and along the water's edge for all of San Rafael's residents and visitors. This includes providing clear and open views toward the open space of the Canal, which will strengthen the area's unique waterfront identity.

The following design guidelines underscore these goals and identify options for achieving them and provide an architectural and landscape architectural framework for future development or redevelopment along the waterfront in the Canal Neighborhood.

Written primarily for property owners, developers, design professionals, City staff and elected/appointed decision makers in San Rafael the guidelines are not mandatory rules. They will, however, be referenced during the City's design review process for any new projects or developments in conjunction with the City's zoning ordinance and municipal code. These guidelines supplement the San Rafael Design Guidelines, providing additional direction to help with the design of new commercial, residential and mixed-use buildings. Together, these guidelines are an invitation for innovation in redevelopment along the canalfront.

Throughout these design guidelines, special attention is paid to both the commercial and recreational marine influence in the area. The Canal has a unique maritime character and natural beauty admired by many residents and visitors. These guidelines endeavor to protect this character and beauty as well as to enhance the many and varied uses of the Canal. Some key issues include ensuring bicycle and pedestrian safety, promoting sustainable design solutions in new public and private development projects, and providing public access to the canalfront.

The current character of the canalfront area varies greatly. Commercial businesses, yacht harbors, high density housing and single family homes line the Canal, yet many residents and visitors to the area do not know that the Canal exists. In addition, some buildings in the area have deteriorated over time or are reaching the end of their expected life span. Others have been developed, renovated and re-purposed without clear direction as to how to engage with and acknowledge the canalfront. This document attempts to acknowledge what is working in the area but, most importantly, what could be improved in the future.

As is common in San Rafael, the community has articulated a preference for an eclectic and diverse architectural character for the area, and a desire to support creative design ideas that might evolve over time. At the same time, preserving the history and memory of the Canal's past is also important. These guidelines attempt to address all of the above by setting general parameters for future projects that highlight key issues such as community identity, safety and public access to the Canal.

This document is organized into two sections. The first section discusses design guidelines that apply to the entire area, while the second section presents guidelines specific to particular sub-areas. A map is provided to clarify the locations referenced in these sections.



Along the Canal, San Rafael



View of the Canal by boat, San Rafael



AREA-WIDE (AW) DESIGN GUIDELINES

Area-wide guidelines apply to the entire area defined by the San Rafael Canalfront Conceptual Design Plan. They promote a cohesive, holistic vision for the area that will improve the quality of life for local residents and visitors alike. Although each sub-area has unique attributes and considerations, a relatively uniform attitude about some aspects of future development improvements is warranted, and will be beneficial in establishing a common identity for projects related to the Canal.

AW: Water's Edge - Paseo, Boardwalk or Pathway

- Create and/or maintain a minimum 10' clear pathway along the water's edge whenever possible. A 12' wide shared pathway for both pedestrians and bicycles is strongly encouraged.
- All pathways should endeavor to meet the design standards of the San Francisco Bay Trail, and be fully accessible per the California Building Code and the Americans with Disabilities Act.
- The pathway may be a free-standing boardwalk, a cantilevered walkway, or an on-grade pathway to be determined based on the existing context and with the approval of the City.
- Permeable paving is strongly encouraged for paved areas at the water's edge where possible, in accordance with widely accepted sustainable design practices such as those defined by the U.S. Green Building Council (USGBC). The durability of all materials in a marine environment must be carefully considered.
- Provide public access to maritime uses of the Canal in the form of kayak launches, boat docks, public boat tie ups, etc. as is appropriate per the *San Rafael Canalfront Conceptual Design Plan*.

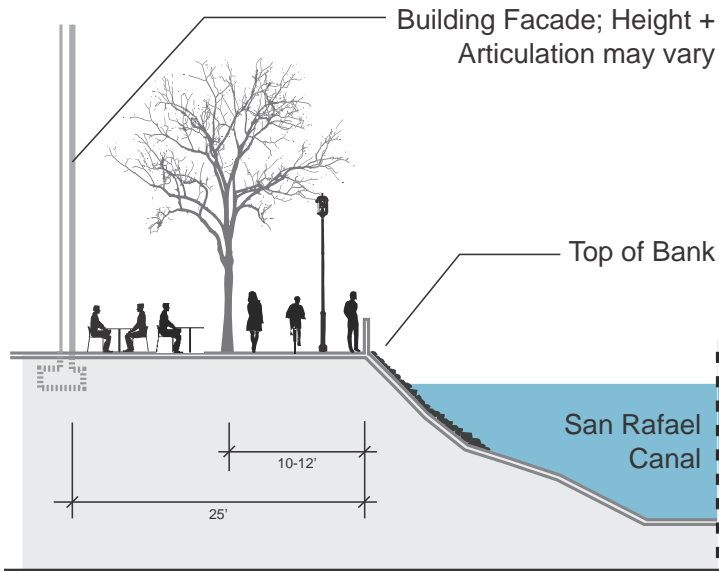


Examples of waterfront access

- Provide public amenities of common design/type such as seating, lighting, litter and recycling receptacles, bicycle racks, etc. at an appropriate interval along the water's edge. See **Site Furniture** section for more detail.
- Whenever possible and appropriate, improvements along the water's edge should carefully consider and support the natural environment including wildlife habitat, native vegetation and water quality issues.
- New public amenities along the water's edge should be designed with maritime uses, both commercial and recreational, in mind. For example, where appropriate, new development should accommodate and encourage recreational boat traffic (canoes, paddle boats, sail boats, etc.) along the Canal.
- Public boat docks and launches for small, non-motorized personal watercraft such as kayaks, canoes and sail boats are strongly encouraged.

AW: Building Scale and Mass

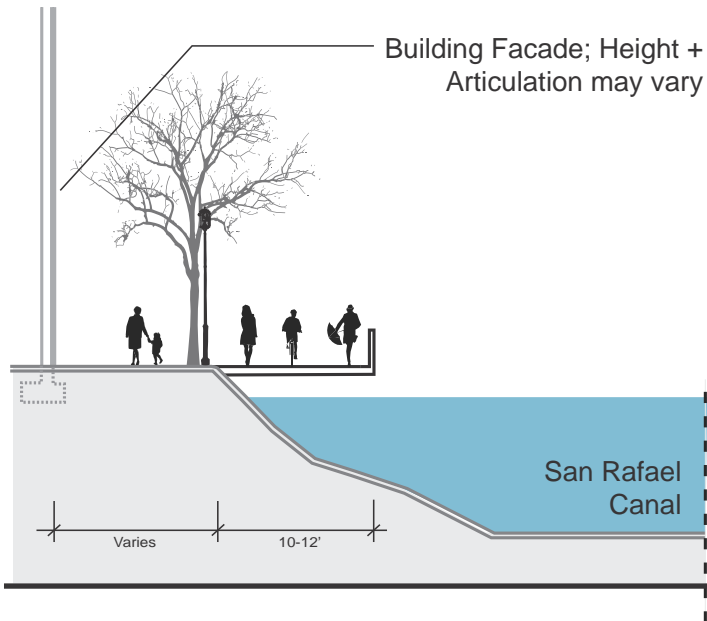
- Buildings should be complementary with the surrounding area and should not adversely affect the visual quality of the canalfront. For example, taller buildings can be made compatible through transitions between existing and new buildings by maintaining horizontal proportions of ceiling heights, windows, doors, and/or materials at lower floor levels.



CANALFRONT PASEO

OPTION 1:

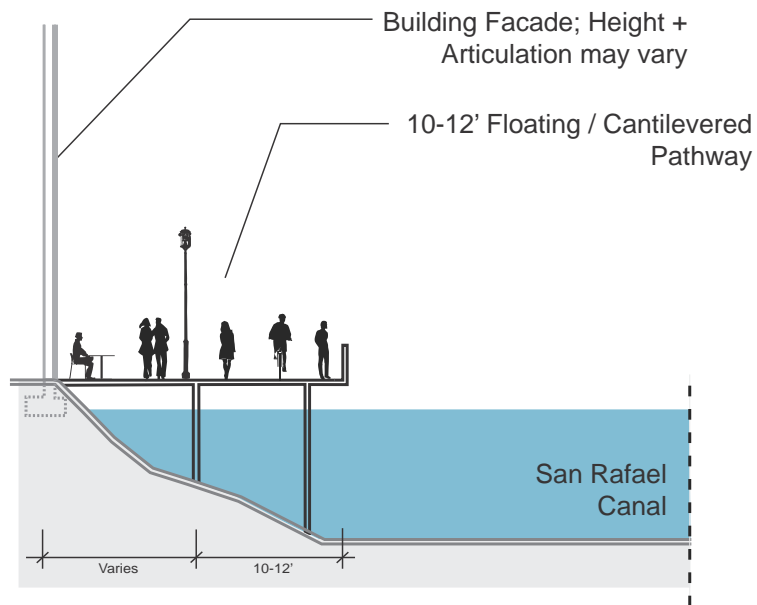
All New Development



CANALFRONT PASEO

OPTION 2:

Possible Modification to Existing Development



CANALFRONT PASEO

OPTION 3:

Possible Modification to Existing Development



Kayak launch



Small boats

AW: Building Character and Articulation

- Building design should maximize visual and physical access to the canalfront.
- Buildings should provide visual interest and variety.
- When a building lot has both street and Canal frontage, the building design should take advantage of its Canal setting and provide equal facade treatment, windows, and other design features on both sides.
- See also the **Area Guidelines** for specific recommendations.

AW: Architectural Style & Materials

- Building designs should be innovative and provide visual interest and variety to support the unique identity of the area.
- Durability of architectural materials in a marine environment should be a key consideration for all new or renovated buildings. Materials and architectural detailing that is not well suited to the canalfront environment should be avoided.
- Typical industrial developments utilizing box-like concrete tilt-up buildings with minimal architectural interest are discouraged along the canalfront.
- Facades of high quality, architectural-grade steel, masonry, corrugated metal, concrete, wood, glass or other appropriate materials relieved with reveals, moldings, and punched openings are encouraged.
- Pre-fabricated metal buildings should not be permitted unless an exception is recommended by the Design Review Board based on exceptional building and site design.



Architectural variation in style and materials



Architectural variation and interest

AW: Lighting

- Building accent lighting and uplighting along the canalfront (street and Canal sides) should be used to highlight the facility's entrances and architectural features.
- Building lighting should be functional, support a unified concept for each building/property and should not spill light onto adjacent properties or natural areas (unless such spillover is desirable to illuminate a public area).
- Surface-mounted lighting on buildings and sources of indirect lighting should be concealed to mitigate glare and reduce light pollution.
- Site lighting should be designed to provide safe conditions for pedestrians, bicyclists, motorists, and to discourage vandalism.
- Exterior site lighting near wetland areas should be sited carefully to reduce glare into sensitive natural areas.
- Where appropriate site lighting may be designed to encourage use of public spaces at night to promote vitality along the canalfront.
- Utilize light fixtures that are of appropriate scale, material and color. A combination of pedestrian pole lights, bollards and wall lights may be utilized in pedestrian areas, for example.

AW: Signage

- Provide consistent wayfinding and signage elements to show the walkway along the water's edge, public access points, and locations of nearby public amenities such as the Transit Center and Pickleweed Community Center.
- Interpretive signage regarding the natural and cultural history of the area along the canalfront is strongly encouraged when possible and appropriate. Design, materials, locations etc. of all interpretive signs along the canalfront will be reviewed and approved by the City.
- Signage for buildings along the canalfront (street and Canal sides) should be used to identify entrances and architectural features.
- Signs should be high quality and have a durable finish suitable for the marine climate.
- Signs with individual letters with architecturally integrated backgrounds are preferred; "canned" or framed signs are not recommended.
- External illumination of signs is preferred.
- Sign types, materials, sizes and locations should be appropriate and specific to the project and its site.
- Use universal symbols in place of text wherever possible.
- Provide signage marking the Bay Trail's alignment as per ABAG's standards.



Signage examples

AW: Views

- Enhance views toward and over the Canal wherever possible. Although the water itself may not be visible from some vantage points, views of the Canal and the canalfront should be ever-present from developed areas and should be the design focus of new or renovated buildings and sites.
- On narrow lots, view preservation may not be feasible but every effort should be made to obtain views wherever possible.
- Careful placement of architectural and landscape architectural elements is strongly encouraged. For example, buildings should be sited in clusters or tiered to provide view corridors.
- Locate architectural and landscape architectural improvements (both public and private) such as buildings, structures, walls and planting areas, etc. to create clear view corridors from public streets, canalfront parks, and plazas toward the Canal and the water's edge.
- Private development and/or improvements are encouraged to provide and frame views toward the Canal from both indoor and outdoor spaces wherever possible in order to promote awareness and strengthen the identity of the area.
- Consideration of views to Mt. Tamalpais and nearby ridge lines is strongly encouraged. Where possible and appropriate, new development and improvements should take care to frame views of ridges and mountains.



View of the Canal



Lively streetscape

AW: Public Open Spaces

- Providing outdoor spaces, such as plazas and courtyards, is encouraged as part of any new public and private development in the area, particularly along the canalfront. Due to high vehicular traffic volumes in this area, consider including interior courtyards or canalfront patios as part of any new retail development including restaurants. Outdoor spaces should include access to power, water, and other utilities as may be necessary to support public events.
- Consider adjacencies when planning public access and open spaces. For example, restaurants and retail establishments sited with proximity to parks, plazas and courtyards are strongly encouraged. These types of land use synergies will encourage vitality in public spaces.
- Public and recreational uses of City-owned or controlled outdoor spaces, particularly those that encourage connectivity within the canalfront area or to adjacent City neighborhoods, is encouraged. One example is the area under Highway 101 where improvements to pedestrian and bicycle access would be beneficial for safety reasons as well as to strengthen the connection between the canalfront and downtown San Rafael.
- Provisions for public access to and along the water's edge is strongly encouraged whenever possible, as is connecting to other City trails in the area such as along Mahon Creek.
- The inclusion of family-friendly and multi-generational amenities in public parks and open spaces in the Canal Neighborhood is strongly encouraged.



Waterfront access and public art

AW: Landscape Design

- Continuity in landscape design elements such as light fixtures, railings, and site furniture that may occur along the canalfront is desirable.
- Hardscape and planting materials should be chosen for durability, ease of maintenance, site appropriateness and aesthetic considerations in conjunction with City staff and user groups. Universal accessibility (accessible to all people of all abilities) is encouraged.
- Landscape designs for individual site developments may vary in character, materials and design but visual and physical access to the Canal should be key considerations in all cases.
- All landscape design should minimize negative impacts to natural areas and wetlands.
- The use of plant materials that have naturalized to our climate zone and waterfront in natural areas is encouraged.

AW: Public Art

- The inclusion of public art elements into public or private developments that celebrate the City's cultural heritage and/or natural environment is strongly encouraged.

AW: Sustainability

- The use of sustainable landscape and building design practices are encouraged. Following Marin Municipal Water District standards for low water use may be required; adhering to the U.S. Green Building Council's Leadership in Energy & Environmental Design (LEED) standards is strongly encouraged.
- New development, both public and private, should protect and enhance the natural environment of the Canal.
- Site and building design along the canalfront should use rainwater infiltration and retention strategies to reduce run-off into the Canal and improve water quality.
- The use of locally produced and/or recycled materials for building and landscape projects is strongly recommended.
- New buildings along the canalfront should consider implementing alternative energy generation strategies such as installing photo voltaic collectors on roofs.
- New buildings should also reduce energy consumption as much as possible by utilizing highly efficient materials and designs. Utilizing green roof technology is an example of this.
- Pervious paving materials for the Canalfront Paseo are strongly encouraged.
- New development should provide ample bicycle parking in convenient locations along the canalfront.



Wetland area near the Canal



Class II bike lanes

AW: Streetscapes & Sidewalks

- Ensure that streetscapes are designed for universal access (accessible to all people of all abilities) and are in compliance with all local, state and federal accessibility guidelines.
- Where designated as Bay Trail, sidewalks should be a minimum of 10' wide wherever possible. Widen existing narrow sidewalks where possible.
- The use of natural gray concrete paving for standard pedestrian sidewalks is preferred in order to maintain a consistent look over time. Adhere to current City standards where applicable; all sidewalks should be designed to facilitate easy maintenance and replacement by the City.
- Consider using alternate paving types and permeable paving materials as accents in streetscape designs at key areas such as intersections and crosswalks to improve aesthetics and wayfinding. Examples include permeable asphalt or concrete, colored or textured concrete, and concrete unit pavers.
- Crosswalks should be designed to clearly identify the right of way to pedestrians and minimize crossing distances where possible.
- Obstructions to pedestrian and bicycle travel (e.g. utilities, signs, overgrown landscaping) should be removed or relocated where feasible.

- The undergrounding of utilities is strongly encouraged.
- Provide screening (an art program or planting are examples) of utility installations, utility boxes, traffic signal boxes, pad mounted transformers, pump stations and other similar utility facilities where feasible.
- New drainage facilities and utilities should be designed and located to minimize visibility and interference with pedestrian, bicycle, and vehicular traffic.
- When sidewalks are repaved or upgraded, improvements should also include new planting areas and street trees wherever possible.
- Select tree species based on site and soil conditions, desired size, aesthetic considerations and City standards when present. The use of low-maintenance, drought tolerant, climate appropriate, disease and pest-resistant canopy trees is encouraged. Consideration should be given to existing street trees in the vicinity, and match if appropriate.
- Use tree grates and guards that are consistent with existing streetscape standards in the vicinity. If no precedent exists, tree grates should be heavy duty, low maintenance and readily available.
- Provide groundcover and shrubs where appropriate to create a pedestrian friendly and aesthetically pleasing environment. Plants should be low water use, low maintenance, and highly durable.
- Identify and consider utilization of any available street right-of-way in the area. In particular, capturing additional width for sidewalks and/or bike lanes is strongly encouraged wherever possible.

AW: Site Furniture

- Provide durable, high quality street and site furniture for the canalfront that is aesthetically consistent throughout and enhances the unique identity of the area. This includes seating, bicycle parking, trash/recycling receptacles, newspaper racks (if possible), and lighting.
- Once standard furnishings have been selected and established in cooperation with the City, street furniture in the area which does not comply should be removed and replaced.
- Locate secure bicycle racks in all parks and plazas and near other public gathering areas as is appropriate.

AW: Parking

- Conceal on-site parking and locate away from the street edge and the water's edge to minimize visual effects on the streetscape and canalfront where appropriate and possible. Locating surface parking lots behind buildings and toward the interior of blocks is encouraged.
- Ensure that all parking areas are designed so that pedestrians are protected from conflicts with entering and exiting vehicles. Minimize the width and number of curb cuts.
- Use canopy trees in parking lots to create shade wherever possible. An exception to this may be allowed along the Highway 101 frontage where visibility for businesses from the freeway might be preferred.
- Where a surface parking lot abuts a street edge, create a visual buffer with planting to minimize the impact of parked cars and to provide interest for pedestrians and cyclists.
- If a parking garage is proposed, it should be designed to include retail uses at the street level to encourage a lively pedestrian experience. Blank walls should be avoided; lush planting and architectural articulation is encouraged. Curb cuts at sidewalks should be minimized as much as possible in order to limit vehicular and pedestrian conflicts.
- Transit-oriented development that would reduce on-street parking demand in the area is encouraged. Ideas for consideration include providing shared cars for residents, transit shuttles, and van pool parking spaces.



Example of an operable crossing

AW: Crossing(s)

- Any proposed crossing must be fully accessible, be a minimum of 12' wide to accommodate bicycle and pedestrian traffic, and meet the clearance requirements of the U.S. Coast Guard. A crossing should be well lit for night use and safety. Any crossing is strongly encouraged to have a unique and iconic design to create a gateway for both the Canal and Montecito neighborhoods.



Examples of operable crossings



AREA 1: TRANSIT CENTER AND VICINITY

The Transit Center and Vicinity area includes Second and Third Streets from Lincoln Avenue to Irwin Street. Unlike other areas included in the *San Rafael Canalfront Conceptual Design Plan*, this area has no direct access to the Canal itself. It is part of the study area because of the critical link it provides from the Canalfront area to the Transit Center and serves as a gateway to the city from Highway 101.

Pedestrian sidewalks and bicycle paths are at less than ideal widths in this area due to myriad site constraints, namely the narrow right-of-way between the columns under the Highway 101. Even so, there are many opportunities to improve the pedestrian and bicycle experience under the freeway with beautiful lighting and/or the creation of a public art program.

The following guidelines refer to the Transit Center and Vicinity only and are to be followed in addition to the Area-wide Guidelines.

A1: Public Open Spaces

- Improvements to pedestrian and bicycle access under Highway 101 should be explored for safety reasons as well as to strengthen the connection between the Canal and Montecito neighborhoods and downtown San Rafael.
- An accessible and welcoming park-like setting would be ideal in the open space under the freeway.

A1: Lighting

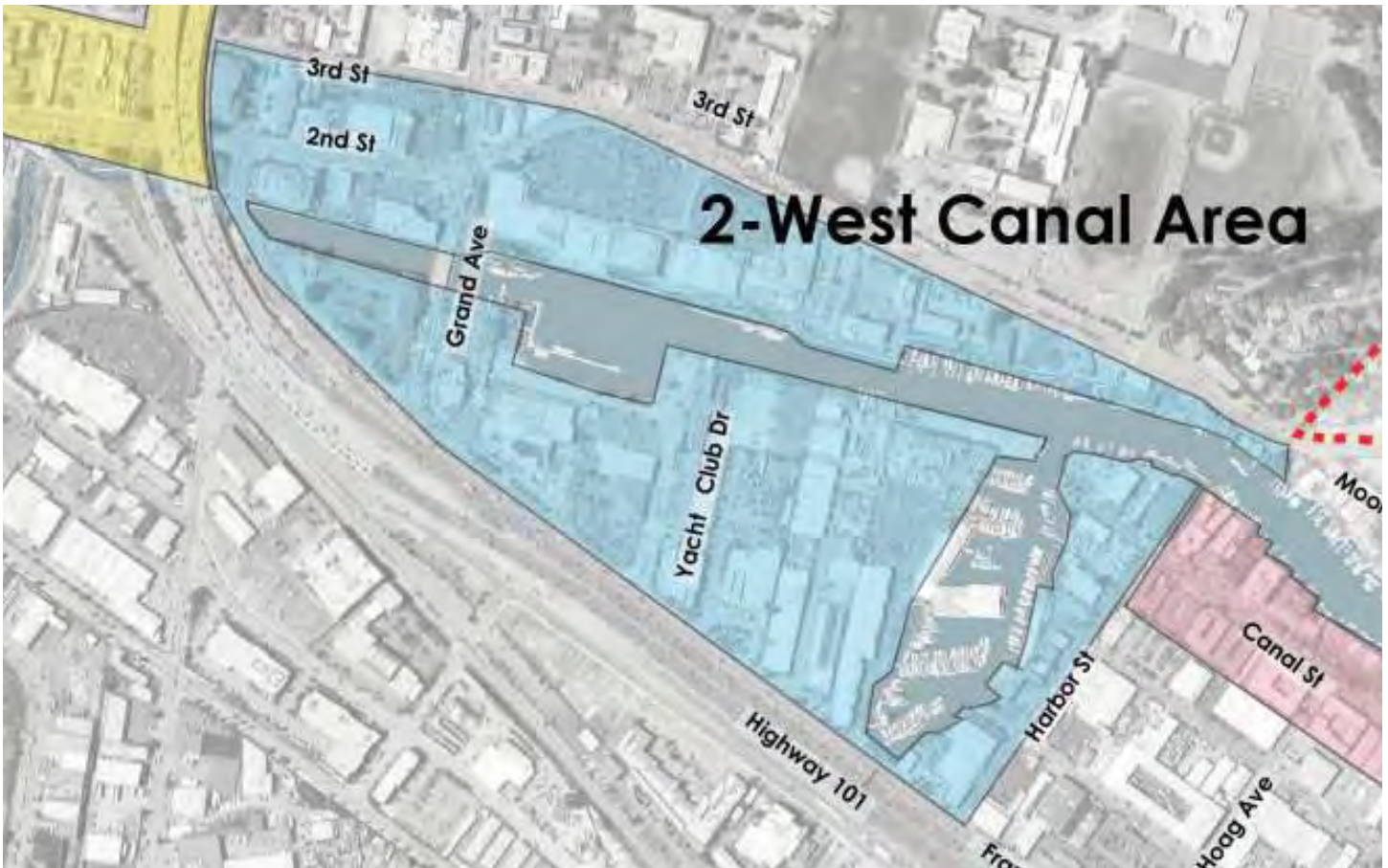
- Improve lighting under the Highway 101 viaduct. Consider providing a public art element that incorporates lighting for this area.

A1: Public Art

- Encourage public art in the area underneath Highway 101 that celebrates the City's cultural heritage and/or natural environment to provide visual interest.



Art, lighting, and activities under the highway



AREA 2: WEST CANAL

The West Canal Area, Area 2, is comprised of lands north and south of the waterway at the western end of the Canal. It includes the land from the northern edge of Third Street and from Highway 101 east to the City limit line and is home to retail shops, offices, and businesses.

Area 2 also encompasses the land from the southern edge of Francisco Boulevard East and Highway 101 northbound. On the east end of the area is Harbor Street and on the west is the northbound off-ramp from Hwy. 101. It is populated by retail establishments, auto dealerships, offices, businesses and light industrial uses including those along Bay Street (a private street) and at the Harbor Center shopping center.

The *San Rafael Canalfront Conceptual Design Plan* envisions a pedestrian and bicycle pathway along the water's edge in this area, a Canalfront Paseo (which means 'walkway' in Spanish) enlivened with restaurants, housing, businesses and cafes. Currently there is a walkway behind the popular Montecito Shopping Center that should be widened and improved as part of this effort. The area's businesses are encouraged to face both the street and the canalfront wherever possible.



Waterfront walkway

Area 2 is fortunate to have public lands as well. In particular, the City has the rare opportunity to re-imagine Beach Park and the land currently occupied by the San Rafael Yacht Club (owned by the City) as part of the Canalfront Conceptual Design Plan. The options for improvement are tremendous and will provide substantial benefit to City residents.

The following guidelines apply to the West Canal area only and are to be followed in addition to the Area-wide Guidelines.

A2: Water's Edge - Paseo, Boardwalk or Pathway

- Expand and improve the existing walkway along the Canal that begins behind Montecito Shopping Center and follows the Canal eastward to the city limit line.
- Create and/or maintain a 12' clear shared pathway along the water's edge for pedestrians and bicycles wherever possible. This pathway may be a boardwalk, a cantilevered walkway, or an on-grade pathway to be determined based on the existing context. Varied conditions are allowed and encouraged. All options should endeavor to meet the San Francisco Bay Trail standards, connect to adjacent segments, and be fully accessible.
- Additional width to accommodate site amenities such as cafe tables and chairs is encouraged.

A2: Marine Related Amenities

- A publicly accessible boat dock at or near the Montecito Shopping Center is encouraged so that boaters could access the businesses and restaurants.

- A concession for boat rentals (canoes, kayaks, peddle boats, etc.) is also encouraged in this location.

A2: Setbacks

- A 25' setback from the top of bank is required. (Top of bank is defined as the point at which there is a break in slope, toward the water, at the Canal's edge.) The intent of this setback is to provide space for a walkway, the Canalfront Paseo, and to protect the natural environment along the Canal. This may be modified where site conditions, such as at a narrow or odd-shaped lot, may warrant.
- An 8 - 10' setback from front (street side) lot line for landscaping, sidewalk, and pedestrian amenities is encouraged
- Connection/s between street and canalfront should be incorporated where appropriate.

A2: Building Scale and Mass

- New buildings should be massed to avoid creating a narrow canyon-like space along the Canal lacking natural light.
- Development projects that increase density for multi-use, retail or commercial properties are encouraged as they will help to bring life and vitality to the area.
- Storefronts and building heights should be varied, along with roof lines and parapets, to create unique individual/business identities.
- Street frontage parking at buildings facing the street should be limited and occur only where necessary due to site or project constraints. To enhance the urban context, parking should be concealed as much as possible. Alternatively, parking could occur in combination with landscaping or green space alongside the building as a break or buffer between development projects. See **Area-wide: Parking** for more information.

A2: Building Character, Articulation and Color

- Provide visual interest and variety in architectural character and building colors.
- Retail storefronts should face the Canal where possible. If a business has two facades, one on the Canal side and one facing the street or parking, they should be designed to be of equal importance visually.
- Businesses offering outdoor seating along the canalfront are strongly encouraged.



Variation in architectural materials and massing



Restaurant with outdoor dining

- Use of colors that are complementary to the waterfront is strongly encouraged. Combined with the natural coloration of materials such as wood or metal, color can provide a visually diverse edge to the Canal. Color may be used to identify a particular building function such as a main or secondary entrance door, structural elements of the building, vertical circulation, a special architectural feature, or to break up and articulate longer facades.
- Single coloration of an entire building is not encouraged, except in the context of a multiple building development.
- Using color to articulate building massing, step-backs, and floor level changes is encouraged.



Streetfront view of mixed-use development and architectural variation



Building materials and color

A2: Architectural Style & Materials

- Incorporate traditional waterfront architectural motifs with more modern types by using a variety of materials to provide visual interest. To assist in the desired variety and individuality of each building and use, a variety of roof forms are encouraged and should be complementary to the building façade.
- Employing materials such as metal siding/roofing, wood and glass found on the marine and commercial properties on the south side of Canal may be appropriate on the north side in order to create visual continuity on both sides of the Canal.
- Similarly, attention should be paid to the character of the adjacent residential neighborhoods; any new development should acknowledge its context and transition appropriately.
- Cement plaster is not encouraged as an entire building concept or a treatment, however a combination of materials including plaster incorporated in a balanced application would be acceptable.
- The use of varied materials, including those consistent with the existing buildings in the area, is encouraged.
- The texture and scale of siding should be combined with various sizes, scale, and direction of pattern to add visual interest to an otherwise simple utility building type.
- Smaller buildings should employ a siding with texture and scale appropriate to a smaller scale structure.

- Other materials such as wood, concrete or plaster could be woven into a project or development to provide some variation while maintaining a visual link to the other areas along the Canal and enhancing the eclectic nature of the area in general.

A2: Lighting

- Surface mounted fixtures on both sides of the Canal should complement each other and reinforce the relationship and connection across the Canal.

A2: Signage

- Signs should be complementary on both sides of the Canal and consistent with the scale of storefronts. Three-dimensional signs or signs that allude to a three-dimensional affect would be allowed if compatible with the use and articulated in a way that would not be overwhelming in their context.
- Use materials that enhance exterior building finishes, while providing clear identification of the use.
- Metal, glass, acrylic halo or backlit signs should provide a comfortable level of identity and should be secondary to architectural lighting.
- Canvas or metal awnings with integrated signage, if employed, are encouraged at the Canal and street sides. Use of awnings should be consistent along an entire building or group of buildings in a development to create a rhythm (minimum of 3-5 tandem storefronts).
- Neon signs employed in an appropriate manner for the use would be allowed subject to Design Review Board approval.



Waterfront public space

A2: Public Open Spaces

- Providing outdoor spaces along the water, such as plazas, courtyards and patios, is encouraged. Public access should be oriented toward the Canal whenever possible.
- The improvement of **Beach Park** is envisioned as part of the *San Rafael Canalfront Conceptual Design Plan*. Together with the adjacent City-owned land that fronts the Turning Basin, Beach Park has the potential to be a key public gateway to the Canalfront.
 - The unique setting of the park will draw local residents as well as visitors from other City neighborhoods, and the amenities should be designed with this in mind.
 - Conceptually the park design should acknowledge the cultural history and/or natural environment unique to the Canal. Interpretation of such themes is encouraged.
 - The park itself should “face” and safely engage the water’s edge. This edge should not be considered the “back” of the park.
 - A boat launch is strongly encouraged. The inclusion of additional amenities such as a public boat dock and/or ramp should be studied.
 - The facilities currently used by San Rafael High School’s sailing club should be maintained for that use if appropriate and feasible.
 - Connecting and utilizing all of the City-owned property to create one park parcel is encouraged.
 - Picnic areas and other family-friendly activities such as a safe play area for young children are encouraged.
 - Appropriate themes for play areas and activities may relate to water, the Canal and the bay; native flora and fauna; boats and ships, etc.
 - The park has hosted beach volleyball and bocce ball in the past; the community should be surveyed to see if these uses are still pertinent and desirable.
 - Amenities such as permanent rest rooms and parking are encouraged.
 - Careful attention to lighting design and safety should be paid.
- Encourage retail businesses, restaurants and/or other private concessions to locate near Beach Park. This will draw people to the canalfront and support the creation of lively and active public spaces in the area.
- Providing outdoor spaces, such as plazas and courtyards, is encouraged as part of any new public and private development in the area. Public access should be oriented toward the Canal whenever possible.



Waterfront walkway



Waterfront park

- The renovation of the mini-park at the end of **Canal Street** is envisioned as part of the *San Rafael Canalfront Conceptual Design Plan*.
 - Although small, this public space has a unique setting at the mouth of San Rafael Yacht Harbor that should be highlighted as much as possible.
 - This mini-park would function best as a primarily paved, multi-use, flexible gathering area.
 - Public art, related to the culturally diverse history of the neighborhood, is encouraged.
 - Picnic tables, seating for small groups and game tables could be appropriate.
 - The view toward the Canal should be open and free from obstructions as much as possible.
 - The water's edge may include a boat launch area, but should be made safe for visitors.
 - Night time lighting, if desired, should be carefully designed to emphasize safety.

- The City-owned property at the end of **Mill Street** should become a public mini-park where local residents and people who work in the area could come for fresh air or to eat their lunch.
 - Ideally, visual access to the water's edge along the San Rafael Yacht Harbor should be included.
 - The mini-park should be primarily paved and very simply designed with basic site amenities such as benches, trash and recycling receptacles, bicycle racks, etc.
 - Shade could be provided by a trellis, arbor, structure or trees.
 - Parking should be removed to allow pedestrians and cyclists to inhabit the space.

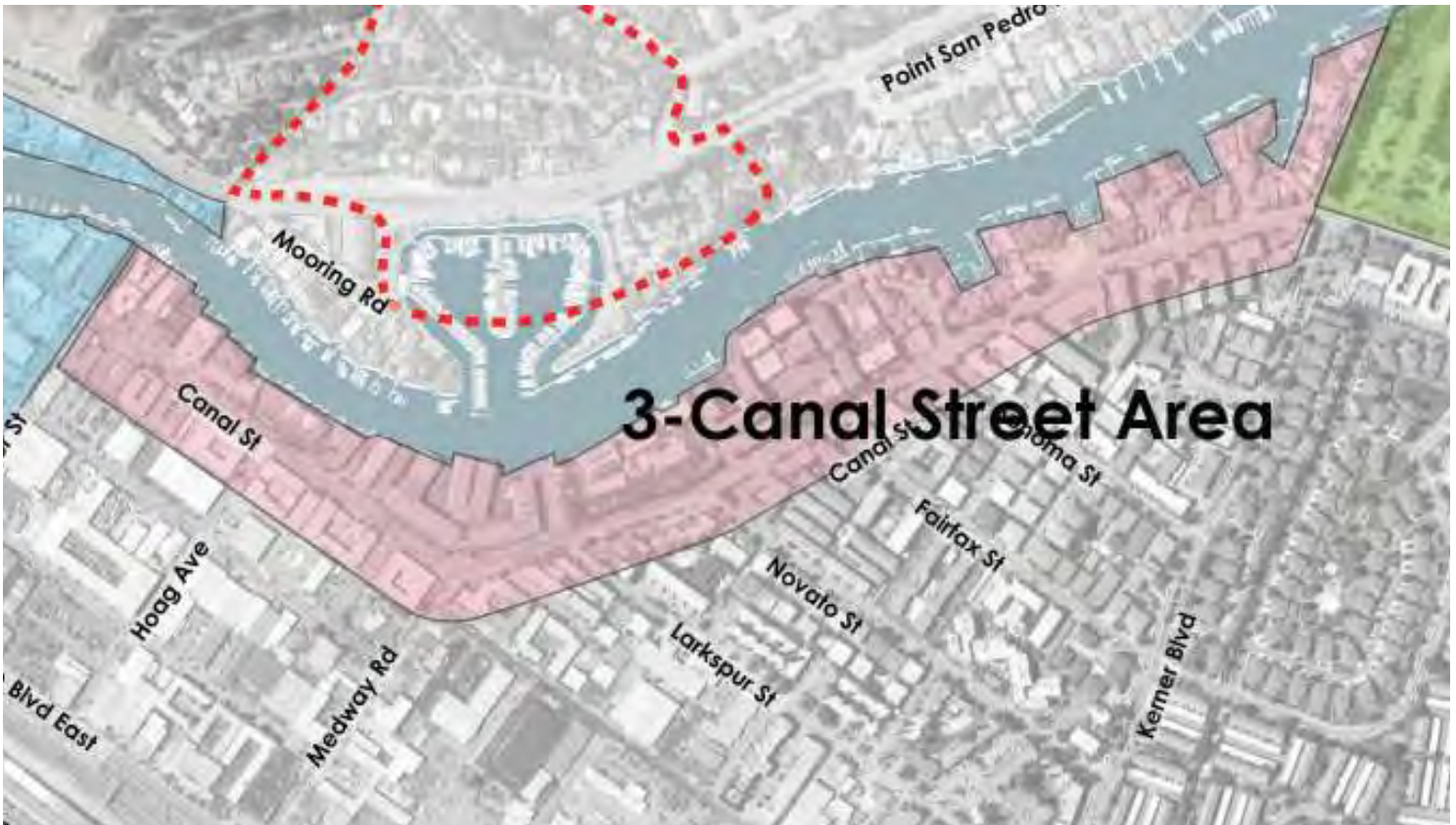
- The City-owned property at the end of **Front Street** should become a public mini-park and water access point for boaters and kayakers.
 - Visual and physical access for boats and kayaks to the water's edge should be maintained and improved for functionality and safety. The design of the mini-park should largely be informed by this use.
 - The mini-park should be primarily paved and very simply designed with basic site amenities such as benches, trash and recycling receptacles, bicycle racks, etc.
 - Shade could be provided by a trellis, arbor, structure or trees if appropriate.
 - The design should also encourage coordinated uses with the adjacent restaurant(s) as much as possible.



Mini-park



Water play



AREA 3: CANAL STREET AREA

The Canal Street Area is composed of predominantly high density residential buildings along Canal Street. The buildings vary in size; the largest are four-stories tall. There are many redevelopment opportunities in this area.

Views toward the Canal and the water's edge from Canal Street are nearly non-existent. The opportunities for re-imagining Canal Street in the future are many. Pedestrian and bicycle access is in need of improvement and access to the Canal's edge should be provided.

The following guidelines are to be followed for Canal Street only in addition to the Area-wide guidelines above.

A3: Water's Edge - Paseo, Boardwalk or Pathway

- The creation of a Canalfront Paseo is envisioned by the *San Rafael Canalfront Conceptual Design Plan*. Creating a walkway along the south side of the Canal, between the residential buildings along Canal Street and the water's edge, is strongly encouraged.

- Create and/or maintain a 10 -12' wide minimum shared-use pathway along the water's edge for pedestrians and bicycles wherever possible. A 12' wide pathway is ideal and encouraged where possible. This pathway may be a boardwalk, a cantilevered walkway, or on-grade to be determined based on the existing context. All options should endeavor to meet the Bay Trail standards and be fully accessible.
- Per the *San Rafael Canalfront Conceptual Design Plan*, a minimum of two, but preferably three, public access points from Canal Street to the water's edge are encouraged. Ideally, the access points will be located at each end of Canal Street and one centrally located in between (such as near the intersection with Medway Road). Each access pathway should be a minimum of 8' wide, with an ideal width of 12'. It should be clearly identified as public access, well lit at night and kept free of obstacles that would inhibit a clear view and/or passageway to the Canal.

A3: Marine Related Amenities

- Public access to the water should be planned and provided at key points along the Canalfront and accessed easily from Canal Street. Public boat docks and boat launches are encouraged where appropriate and possible. A minimum of one of each is suggested for this area.

A3: Setbacks

- A 25' setback from the top of bank is required. The intent of this setback is to provide space for a walkway, the Canalfront Paseo, and to protect the natural environment along the Canal. This may be modified where site conditions, such as at a narrow or odd-shaped lot, may warrant.
- An 8 - 10' setback from front (street side) lot line for landscaping, sidewalk, and pedestrian amenities is encouraged
- Connection/s between street and canalfront should be incorporated where appropriate.

A3: Building Scale and Mass

- Maintain a maximum 36' height along the Canal waterfront. The intent of this is to provide as much housing as is possible (to enhance the economic viability of redevelopment), while also protecting sunlight and access to the Canal.



Example of a floating and cantilevered walkway

A3: Building Character, Articulation & Color

- Visual Interest and variety are important and encouraged for this area of the canalfront.
- Private decks and patios are encouraged facing both the street and the canalfront to activate the Canal area.
- Street frontage parking at buildings facing the street should be limited and occur only where necessary due to site or project constraints. Under building parking with little exposure is preferred, to enhance the urban context. Alternatively, parking could occur in combination with landscaping or green space along side the building as a break or buffer between development projects. See **Area-wide: Parking** for more information.

A3: Architectural Style & Materials

- Promote a waterfront and marine related stylistic expression. Prevent one-sided buildings articulated on the Canalfront only.
- Decks and patios at the street front and canalfront are encouraged to take advantage of the southern exposure on Canal Street and of views to the north side of the Canal.
- Balcony railings should screen and let light through both to mitigate the visual clutter on individual balconies and to lighten massing of building and balconies.

- Well-defined building or unit entries may be recessed, projected or framed with strong architectural features. Incorporate lighting as appropriate for clear delineation, as well as for safety.
- Flat or gabled roofs are a prevalent pattern of the area however, alternative roof forms with a scale consistent with canalfront patterns, are also encouraged to provide diversity and project a sense of identity.
- Use of natural materials is encouraged, i.e., wood – horizontal, vertical, lap, and/or shingle siding, stone or masonry at the building base. Metal siding and/or roofing may be used if shown to be complementary and enhancing the residential fabric of area.



High-density residential building with “front porches”



High-density residential along waterfront



AREA 4: PICKLEWEED PARK AREA

Area 4 includes Pickleweed Park and Community Center at the eastern end of Canal Street. Renovated in 2006, the community center has been a tremendous success in the neighborhood.

Pickleweed's unique waterfront setting and neighboring wetlands are wonderful assets to the community. A launch for small non-motorized watercraft and accessible pathway along the water's edge is strongly encouraged.

The following guidelines are to be followed in Pickleweed Park only and in addition to the Area-wide guidelines above.

A4: Water's Edge - Paseo, Boardwalk or Pathway

- Create and/or maintain a 12' clear shared pathway, in the same location as the current unimproved trail, for pedestrians and bicycles. This pathway will likely be an on-grade pathway and should be suitable for maintenance vehicle access. It should have a natural appearance, meet the Bay Trail standards, and be fully accessible.

- A launch area for small non-motorized personal watercraft along this pathway is strongly encouraged.

A4: Public Open Spaces

- Any proposed changes or improvements to Pickleweed Park should align with the park's adopted master plan under the guidance of the City's Community Services Department.



Small boat launch



Natural areas

EXHIBIT 7

700 IRWIN STREET

SAN RAFAEL, CA 94901

Owner

700 Irwin Street Partners, LLC

Architect

SCB

Structural Engineer

DCI Engineers

Geotechnical Engineer

Rockridge

Civil Engineer

BKF

Landscape Architect

SWA

PROPOSED BONUS PROJECT:

UNIT MIX SUMMARY:

PROPOSED PROJECT DWELLING UNITS					
	ST	1B	2B	3B	TOTAL
L5	5	9			14
L6	4	9	1		14
L7	4	9	5		18
L8	4	9	5		18
L9	4	9	5		18
L10	4	9	5		18
L11	4	9	5		18
L12	4	9	5		18
	33	72	31	0	136
	24%	53%	23%	0%	
L13	2	6	5	1	14
L14	2	6	5	1	14
L15	2	6	5	1	14
L16	2	6	5	1	14
L17			4	4	8
	41	96	55	8	200
	21%	48%	28%	4%	

50%AMI UNITS					
	ST	1B	2B	3B	TOTAL
L5-11	4	8	3	0	15

120%AMI UNITS					
	ST	1B	2B	3B	TOTAL
L5-11	4	8	3	0	15

MARKET RATE UNITS					
	ST	1B	2B	3B	TOTAL
L5-17	33	80	49	8	170
	19%	47%	29%	5%	

LEGEND:
 ST Studio Unit
 1B 1 Bedroom Unit
 2B 2 Bedrooms Unit
 3B 3 Bedrooms Unit

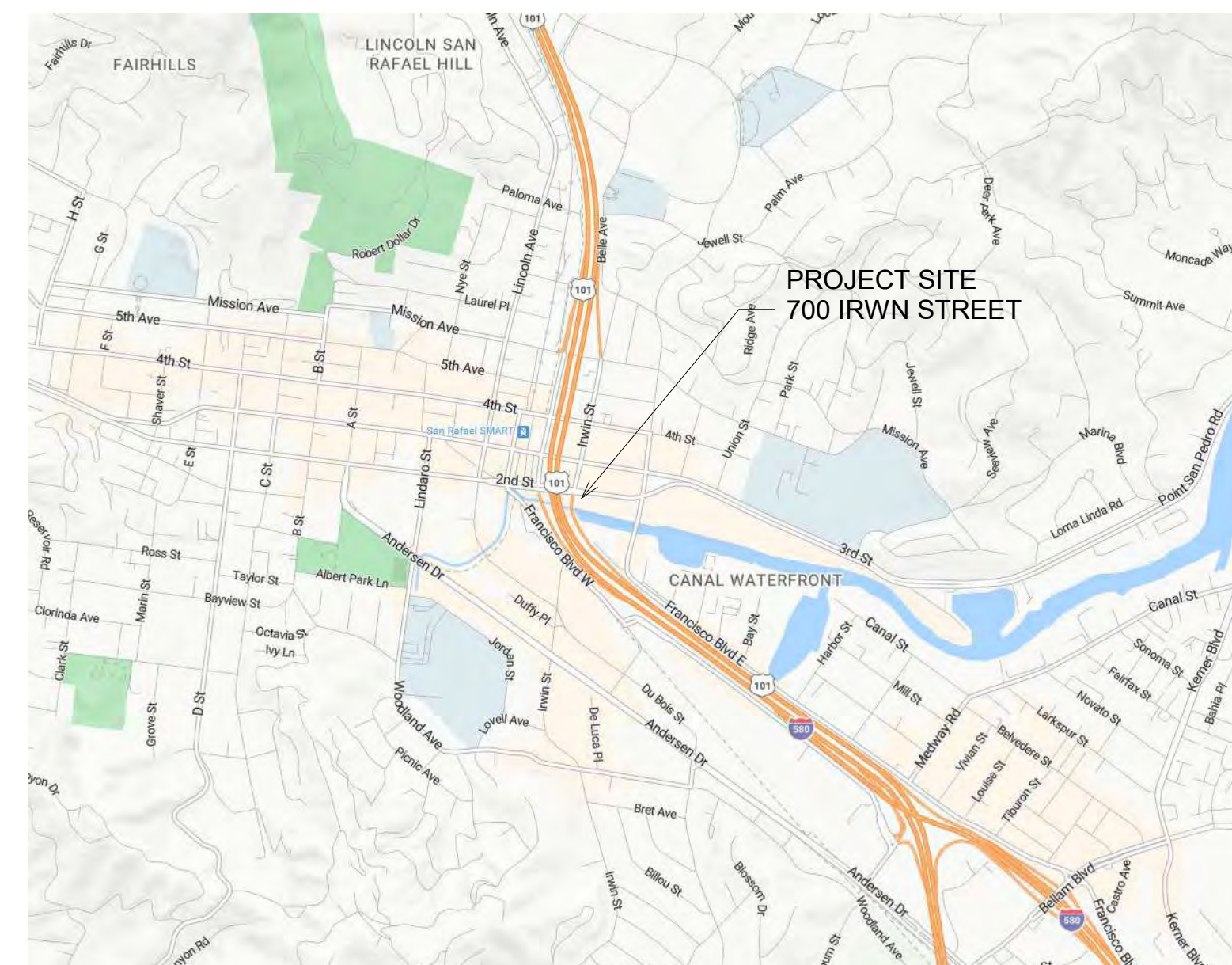
PARKING SPACES SUMMARY:

TOTAL PARKING PROVIDED **226**
 MINIMUM PARKING REQUIRED PER SRMC 14.18.040 **311 TOTAL***
 *40 TOTAL GUEST SPACES AND 16 SPACES ASSUMED FOR COMMUNITY ROOM

MECHANICAL PARKING SYSTEM
 LEVEL 03 MECHANICAL PARKING 86
 LEVEL 04 MECHANICAL PARKING 85
 LEVEL 05 MECHANICAL PARKING 50
 TOTAL **221** (1.1:1 PER RESIDENTIAL UNIT PLUS 2 DEDICATED FOR CAR SHARE)

NON-MECHANICAL PARKING
 ACCESSIBLE 2
 FULL SIZE 3
 TOTAL **5**

LOCATION MAP:



PROJECT DESCRIPTION:

THE PROPOSED PROJECT IS A MIXED-USE HIGH RISE MULTI-FAMILY RESIDENTIAL BUILDING ON AN URBAN INFILL SITE IN DOWNTOWN SAN RAFAEL, CA. THE SITE FRONTS ONTO 2ND STREET AND IS ADJACENT TO SAN RAFAEL CANAL AND THE IRWIN STREET OFF-RAMP FROM US-101.

THE PROJECT WILL INCLUDE THE DEMOLITION OF AN EXISTING 4 STORY OFFICE BUILDING AND CONSTRUCTION OF A 17 STORY RESIDENTIAL STRUCTURE. THE PROJECT WILL BE 180 FT IN HEIGHT AND INCLUDE 200 DWELLING UNITS, 221 PARKING SPACES IN AN AUTOMATED MECHANICAL PARKING SYSTEM PLUS 5 CONVENTIONAL SPACES, AND 5,000 SQFT OF ENCLOSED COMMERCIAL FUNCTION SPACE.

IN ADDITION TO THE ENCLOSED COMMERCIAL SPACE THE PROJECT INCLUDES AN EXTERIOR BOARDWALK THAT ALLOWS PUBLIC PEDESTRIAN CIRCULATION ACCESS FROM THE INTERSECTION OF 2ND AND IRWIN TO AND ALONG THE CANAL, EVENTUALLY LINKING INTO THE FUTURE BOARDWALK SYSTEM ENVISIONED IN THE CITY'S CANALFRONT PLAN. PORTIONS OF BOTH THE NEW BOARDWALK AND ENCLOSED COMMERCIAL FUNCTION SPACE WILL BE CANTILEVERED BEYOND THE SOUTHERN PROPERTY LINE AND EXTEND OVER THE CANAL. THE CANTILEVERED AREAS WILL NOT RELY ON BEARING STRUCTURE OUTSIDE OF THE PROPERTY LINE.

THE PROJECT IS UTILIZING THE STATE DENSITY BONUS TO ALLOW WAIVERS AND CONCESSIONS FROM THE PLANNING CODE IN ORDER TO ACHIEVE THE PROPOSED PROJECT.

PROJECT DATA:

GENERAL ZONING INFORMATION

ASSESSORS BLOCK: 014-132-18
 TRACK BLOCK: 110100
 PROJECT ADDRESS: 700 IRWIN STREET, SAN RAFAEL, CA 94901
 OVERLAY PLAN: DOWNTOWN PRECISE PLAN (DTPP)
 GENERAL PLAN DESIGNATION: DOWNTOWN MIXED USE
 ZONING USE DISTRICT: T5N 50/70
 LOT AREA (GROSS): 25,550 SQUARE FEET
 ADJACENT LAND USES: COMMERCIAL; PUBLIC
 EXISTING BUILDING AREA: 25,545 SQUARE FEET; 4-STORY OFFICE BUILDING
 FLOOD ZONE: YES; ZONE X
 FIRE ZONE: NO
 STREAM CONSERVATION AREA: YES

BASE PROJECT COMPLIANT WITH PLANNING CODE T5N ZONING CONTROLS (MORE ANALYSIS PROVIDED ON SHEET A0.41)

HEIGHT: 5 STORIES / 50 FT IN HEIGHT
 SETBACK: 0 MIN; 15' MAX
 TOTAL GROSS AREA (PLANNING DEPARTMENT AREA DEFINITION): 139,300 SQUARE FEET
 RESIDENTIAL GROSS AREA (PLANNING DEPARTMENT AREA DEFINITION): 100,330 SQUARE FEET
 RESIDENTIAL DWELLING UNITS: 100 DU
 PARKING SPACES: 83 STALLS
 BICYCLE PARKING SPACES: 124

STATE DENSITY BONUS PROPOSED PROJECT

HEIGHT: 17 STORIES / 180 FT IN HEIGHT
 CONSTRUCTION TYPE: TYPE IV-B OVER TYPE I-A
 LOT AREA: 25,550 SQUARE FEET
 LANDSCAPE AT GRADE: 1,800 SQUARE FEET
 IMPERVIOUS SURFACE: 19,700 SQUARE FEET
 COMMON OPEN SPACE: 10,360 SQUARE FEET
 PRIVATE USEABLE OPEN SPACE: 5,440 SQUARE FEET
 LOT COVERAGE: 23,000 SQUARE FEET / 90% COVERAGE
 TOTAL GROSS AREA (PLANNING DEPARTMENT AREA DEFINITION): 201,700 SQUARE FEET
 RESIDENTIAL GROSS AREA: 195,900 SQUARE FEET
 COMMERCIAL GROSS AREA: 5,000 SQUARE FEET
 RESIDENTIAL DWELLING UNITS: 200 DU
 PARKING SPACES: 226 STALLS (INCLUDING 2 DEDICATED FOR CAR SHARE)
 BICYCLE PARKING SPACES: 60 (TYPE I)

CONTACT INFORMATION:

OWNER
 700 IRWIN STREET PARTNERS, LLC
 One Bush St, Suite 600
 San Francisco, CA 94104
 (415) 567-9000

ARCHITECT
 SCB
 255 California Street, 3rd Floor
 San Francisco, CA 94111
 (415) 216-2450
 www.scb.com

STRUCTURAL ENGINEER
 DCI ENGINEERS
 135 Main Street, Suite 1800
 San Francisco, CA 94105
 (415) 781-1505
 www.dci-engineers.com

GEOTECHNICAL ENGINEER
 ROCKRIDGE
 1350 Ocean Avenue
 Emeryville, CA 94608
 (510) 420-5738
 www.rockridgegeo.com

CIVIL ENGINEER
 BKF
 810 Fifth Ave., Suite 210
 San Rafael, CA 94901
 (415) 930-7960
 www.bkf.com

LANDSCAPE ARCHITECT
 SWA
 530 Bush Street, 6th Floor
 San Francisco, CA 94108
 (415) 836-8770
 www.swagroup.com

DRAWING INDEX	
SHEET NUMBER	SHEET DESCRIPTION
GENERAL	
A0.00	COVER
A0.31	DRAWING INDEX & PROJECT DATA
A0.33	FLOOD ZONE ANALYSIS
A0.41A	STATE DENSITY BONUS - BASE PROJECT
A0.41B	STATE DENSITY BONUS - ANALYSIS
ARCHITECTURE	
A2.0B1	LEVEL B1 - FLOOR PLAN
A2.01	LEVEL 01 - FLOOR PLAN
A3.01	BUILDING NORTH ELEVATIONS
A3.04	BUILDING WEST ELEVATIONS

NO.	DATE	DESCRIPTION
10	10.24.2025	REVISED PLANNING PERMIT APPLICATION
2	04.10.2025	PLANNING PERMIT SUBMITTAL



700 IRWIN STREET

SAN RAFAEL, CA 94901

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DRAWING INDEX & PROJECT DATA

Drawn By: Author
 Checked By: Checker
 Project Number: 2024022

Sheet Number: **A0.31**

DENSITY BONUS CALCULATIONS - BASE PROJECT:

- a. The base density has been calculated using the objective controls set for in Ch. 9 Downtown Precise Plan (specifically Section 2.3.050 T5 Neighborhood (TSN 40/60 and TSN 50/70)). Per the Base Project Floor Area Analysis shown below, the base Project includes 100 residential dwelling units.
- b. The Base Project includes 15 Very Low Income Units (15%), and 15 Moderate Income Units (15%)
- c. The project density bonus request is 100% (100 bonus units) for a total of 200 Units.
- d. The parking ratio for the "base project" is shown in a separate table below.

BASE PROJECT - ZONING INFORMATION

ZONING TSN 50/70
 LOT AREA (GROSS) 25,550 SQUARE FEET

TSN 50/70

	BASE ZONING	PROPOSED	COMPLIANCE
HEIGHT	50'	49'11"	COMPLIES
SETBACK - FRONT	0' MIN; 15' MAX	0'	COMPLIES
SETBACK - SIDE STREET	0' MIN; 15' MAX	0'	COMPLIES
SETBACK - SIDE	0' MIN; 15' MAX	VARIES AT EASEMENT	COMPLIES
SETBACK - REAR	0' MIN; 15' MAX	0'	COMPLIES
STEPBACKS - FRONT	0' MIN; 15' MAX	NONE W/ MANSARD ROOF	COMPLIES
STEPBACKS - SIDE STREET	0' MIN; 15' MAX	NONE W/ MANSARD ROOF	COMPLIES
STEPBACKS - REAR	0' MIN; 15' MAX	NONE W/ MANSARD ROOF	COMPLIES

BASE PROJECT - FLOOR AREA TABLE

	FLOOR AREA (SQUARE FEET)				GROSS AREA
	RESIDENTIAL	COMMERCIAL	MISC NON-RES	PARKING	
LEVEL 5	21,490				21,490
LEVEL 4	22,695				22,695
LEVEL 3	22,695				22,695
LEVEL 2	22,695				22,695
LEVEL 1 (GROUND)	13,725	5,000	900	2,000	21,625
BASEMENT 1				22,200	23,575
BASEMENT 2				11,100	11,100
TOTAL	103,300	5,000	2,275	35,300	145,875

+ 100% Bonus Residential Floor Area = 206,600 SF

BASE PROJECT - UNIT MIX SUMMARY

OVERALL	DWELLING UNITS				TOTAL
	ST	1B	2B		
LEVEL 5	8	9	6		23
LEVEL 4	8	8	6		22
LEVEL 3	8	8	6		22
LEVEL 2	8	8	6		22
LEVEL 1	4	7	0		11
TOTAL	36	40	24		100

UNIT DISTRIBUTION	ST	1B	2B	TOTAL
LEVEL 50%AMI UNITS	6	6	3	15
LEVEL 120%AMI UNITS	5	6	4	15
LEVEL MARKET-RATE UNITS	25	28	17	70

*Legend: ST: Studio 1B: 1 Bedroom Unit 2B: 2 Bedrooms Unit 50%AMI UNITS: Very Low Income Units 120%AMI UNITS: Moderate Income Units

BASE PROJECT - VEHICULAR PARKING

The project is not located within the Downtown Parking District therefore per Downtown Precise Plan 2.3.050.H residential parking requirements are:

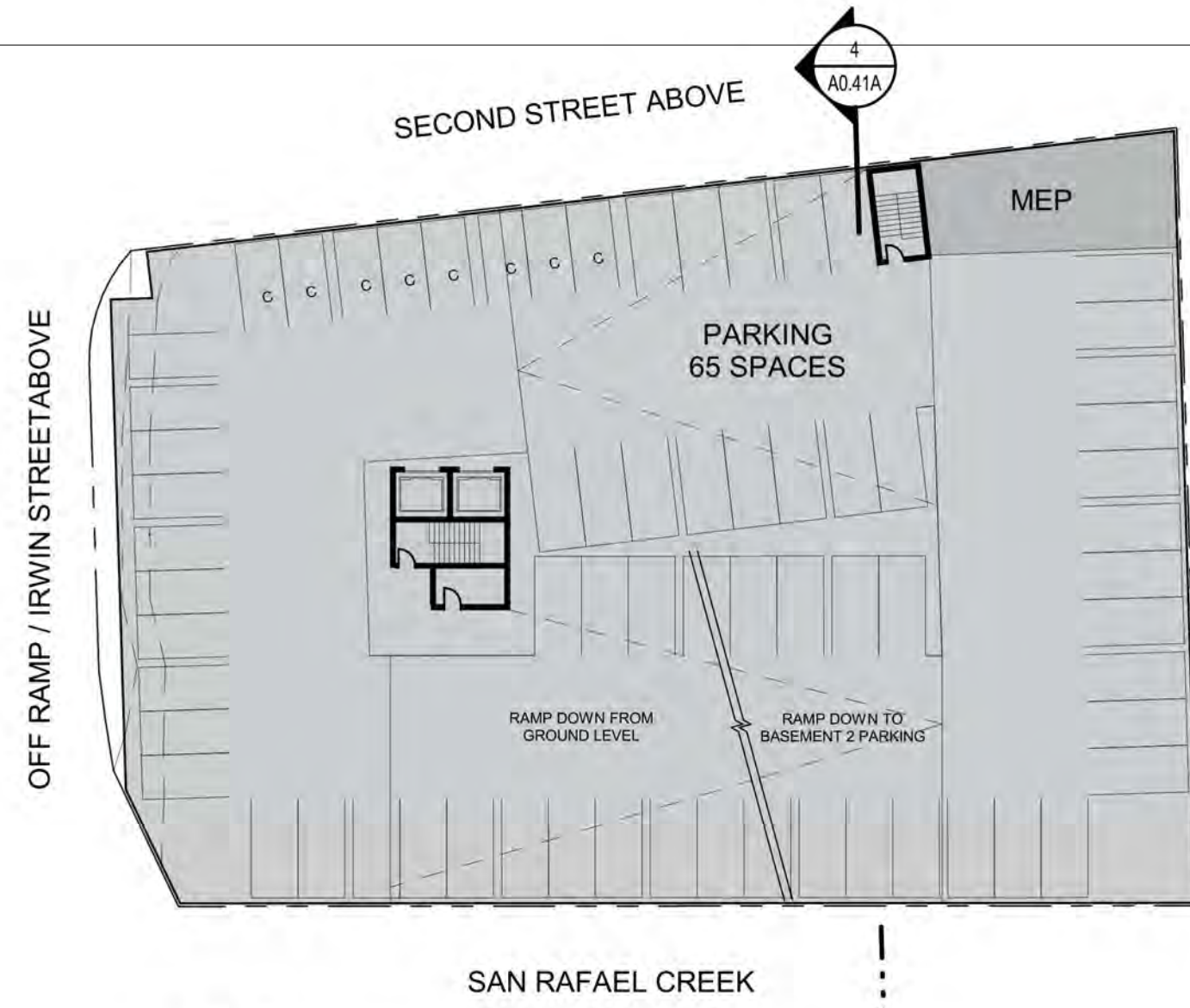
	UNITS	RATIO	TOTAL
CAR SHARE SPACE			2
STUDIO	36	0.75	27
1 BEDROOM UNITS	40	0.75	30
2 BEDROOM UNITS	24	1	24
COMMERCIAL	5,000 SF	2.75 PER 1000 SF OVER 2,500 SF	14
TOTAL REQUIRED			97
PROVIDED			97

* Two car share spaces included in Base Project per 3.1.040.A

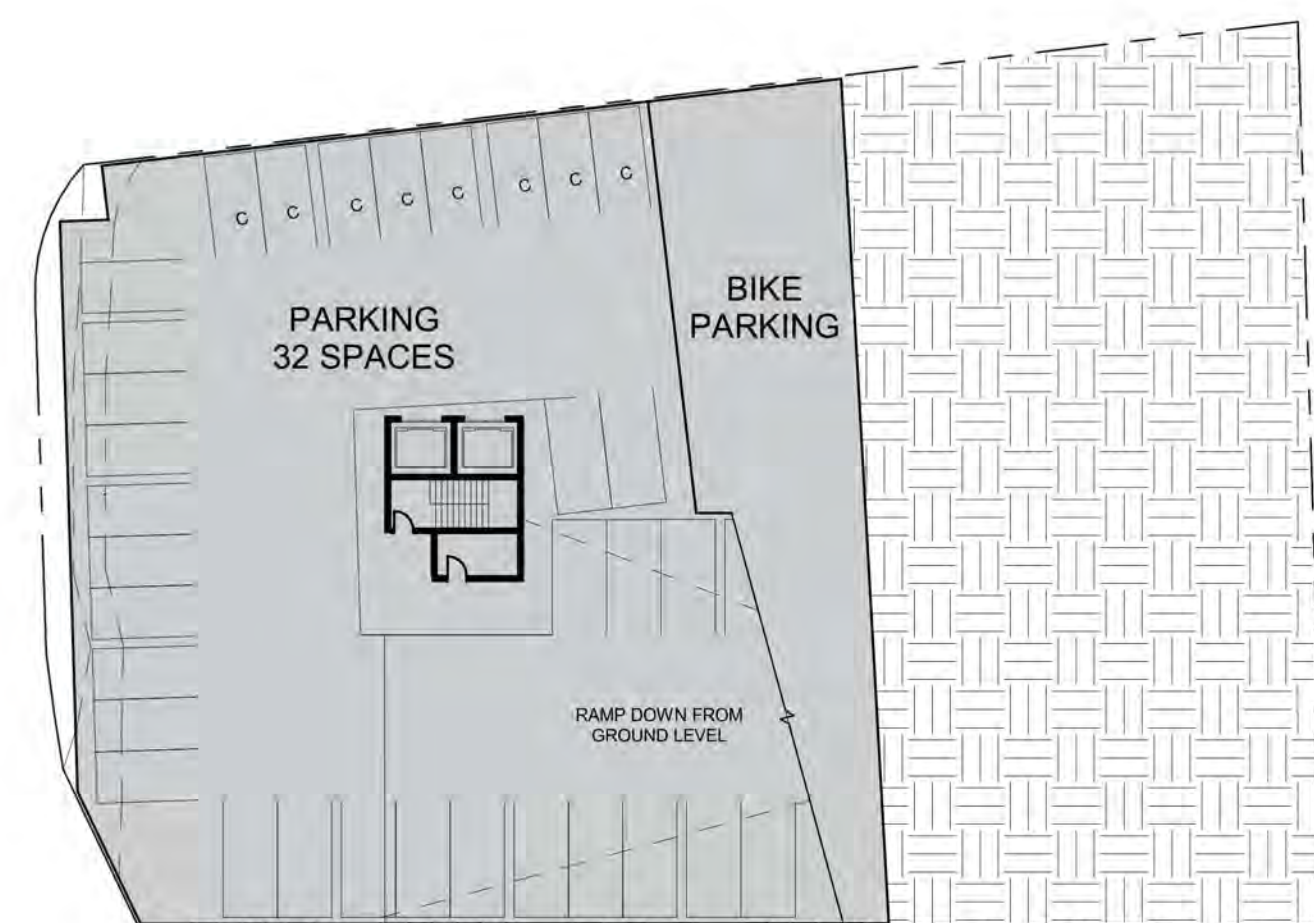
BASE PROJECT - BIKE PARKING

2.3.050.H Bicycle Spaces for residential use requirements are:

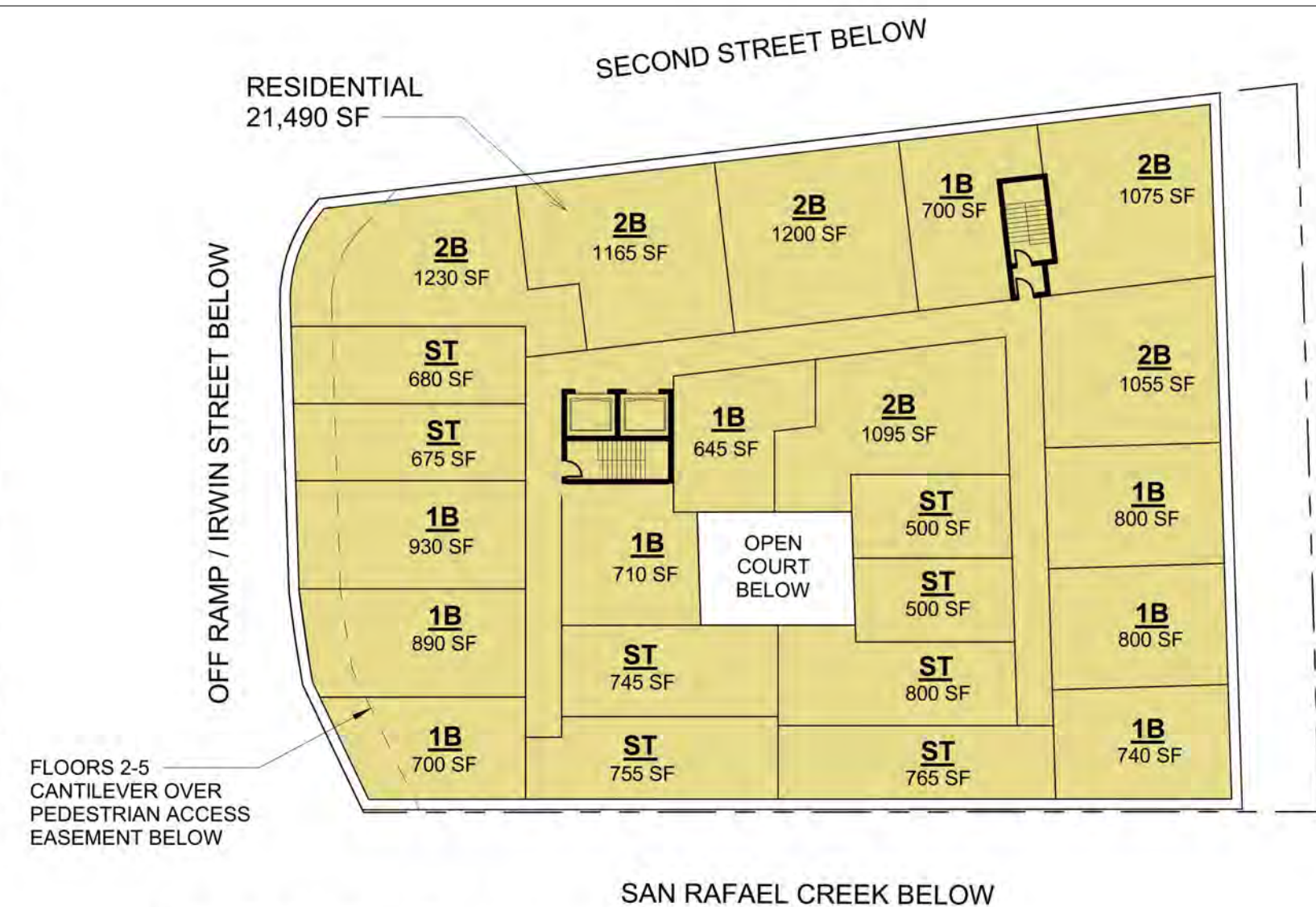
	UNITS	RATIO	TOTAL
STUDIO	36	1	36
1 BEDROOM UNITS	40	1	40
2 BEDROOM UNITS	24	2	48
TOTAL REQUIRED			124
PROVIDED			124



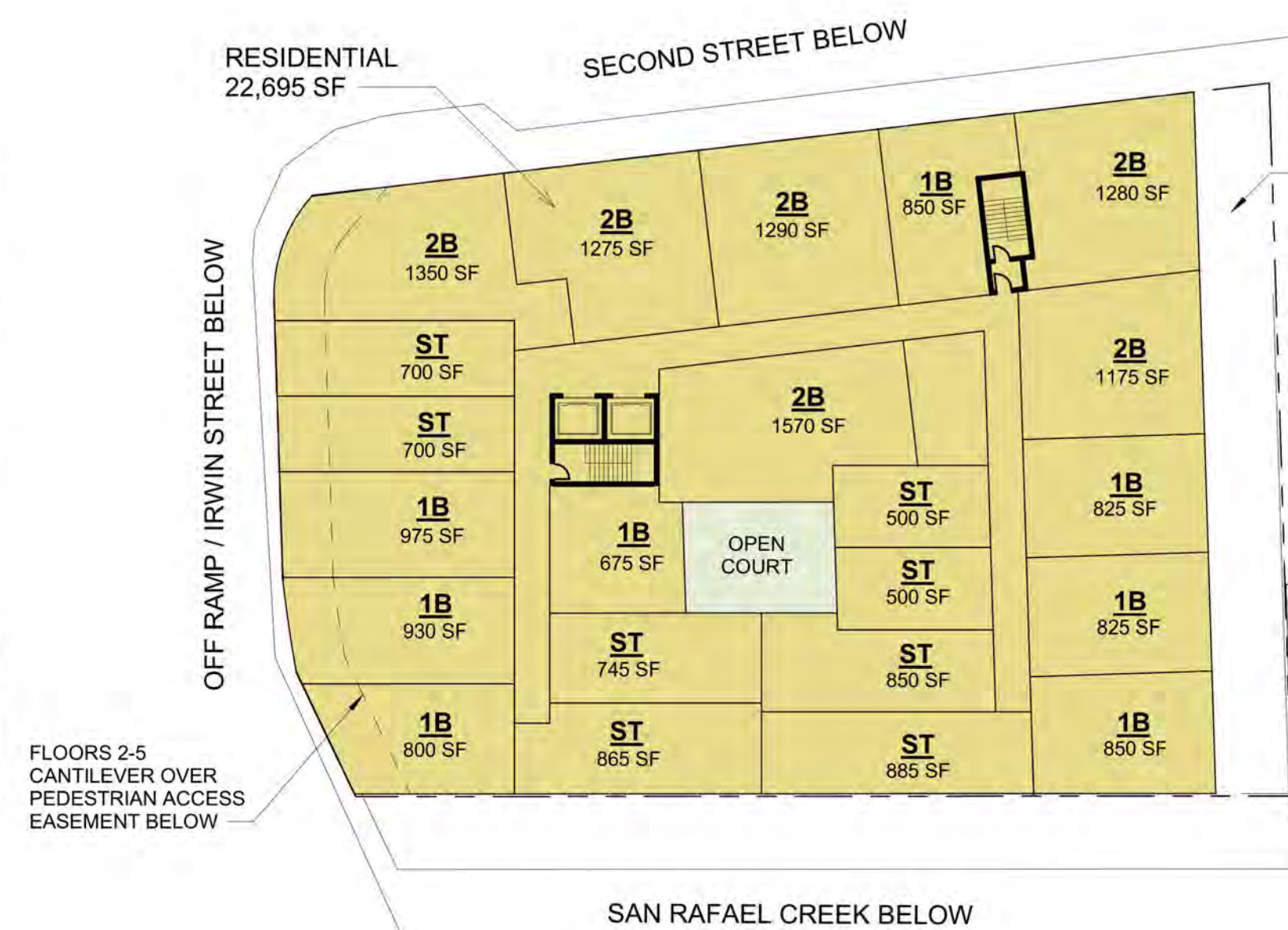
5 LEVEL B1
1" = 30'-0"



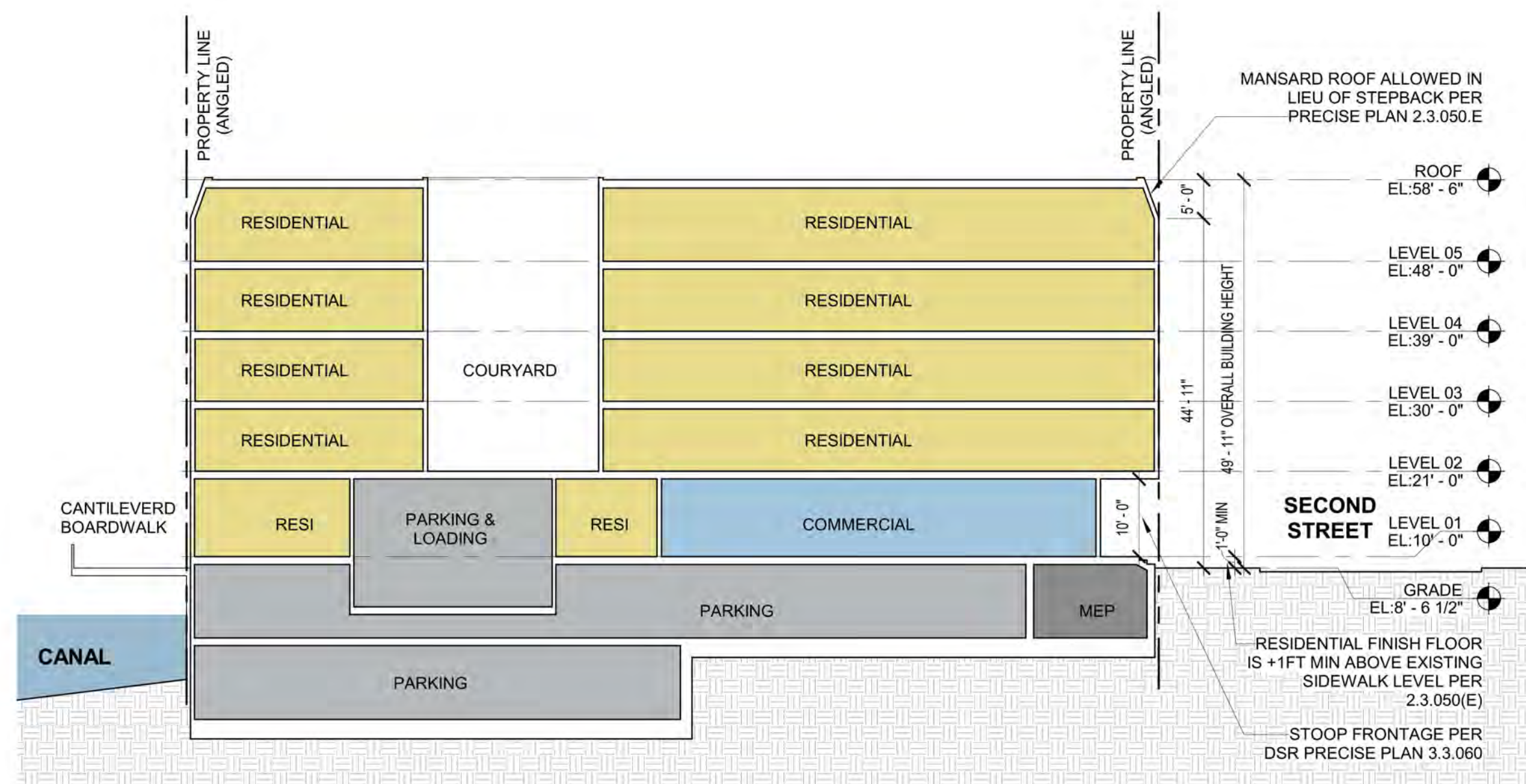
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1" = 30'-0"



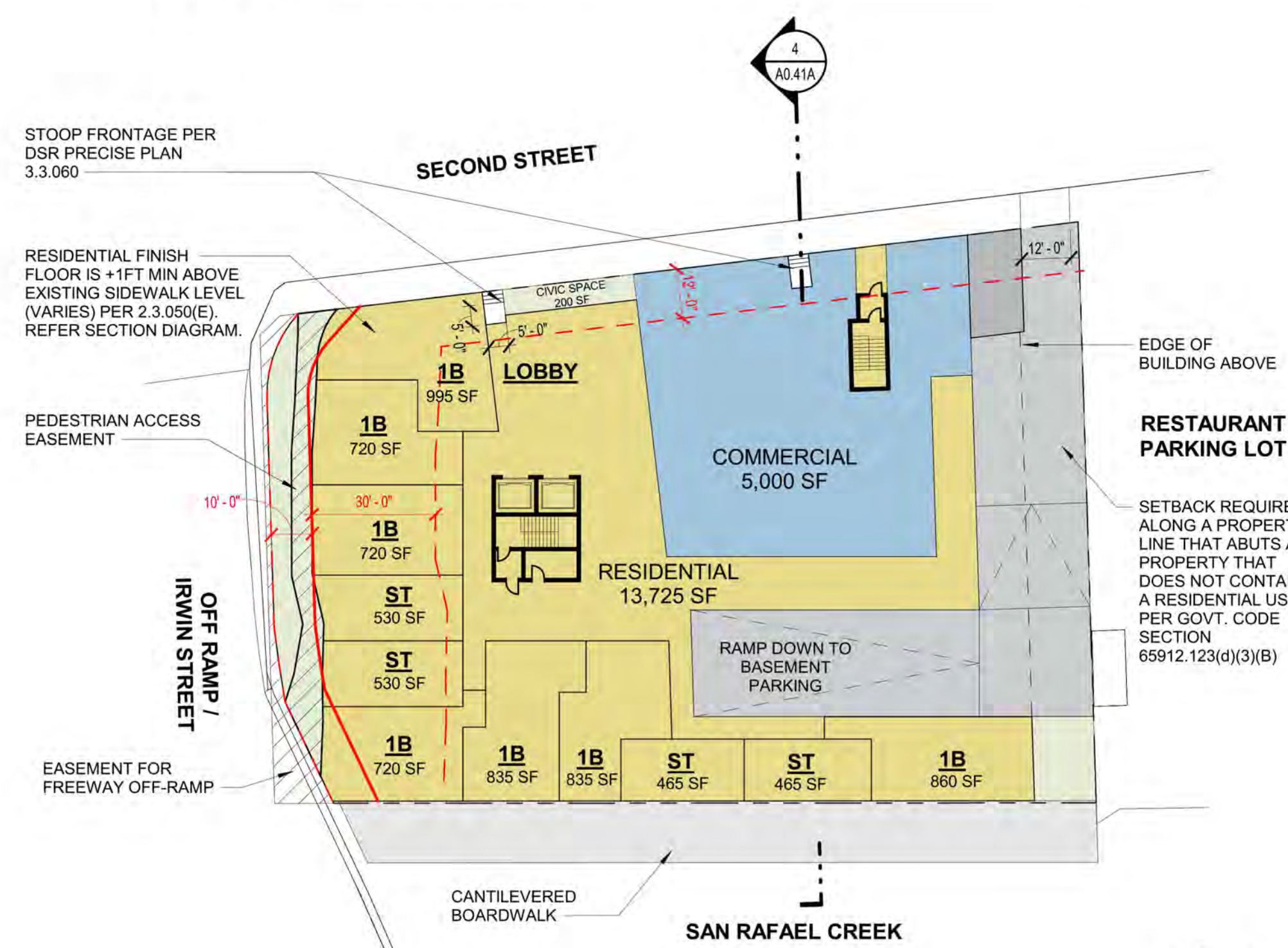
3 BASE PROJECT LEVEL 5
1" = 30'-0"



2 BASE PROJECT TYPICAL LEVELS 2-4
1" = 30'-0"



4 BASE PROJECT BUILDING SECTION
1/16" = 1'-0"



1 BASE PROJECT GROUND FLOOR
1" = 30'-0"

NO.	DATE	DESCRIPTION
1	10.24.2025	REVISED PLANNING PERMIT APPLICATION
2	04.10.2025	PLANNING PERMIT SUBMITTAL

SCB Architecture, Planning, Interior and Urban Design

700 IRWIN STREET

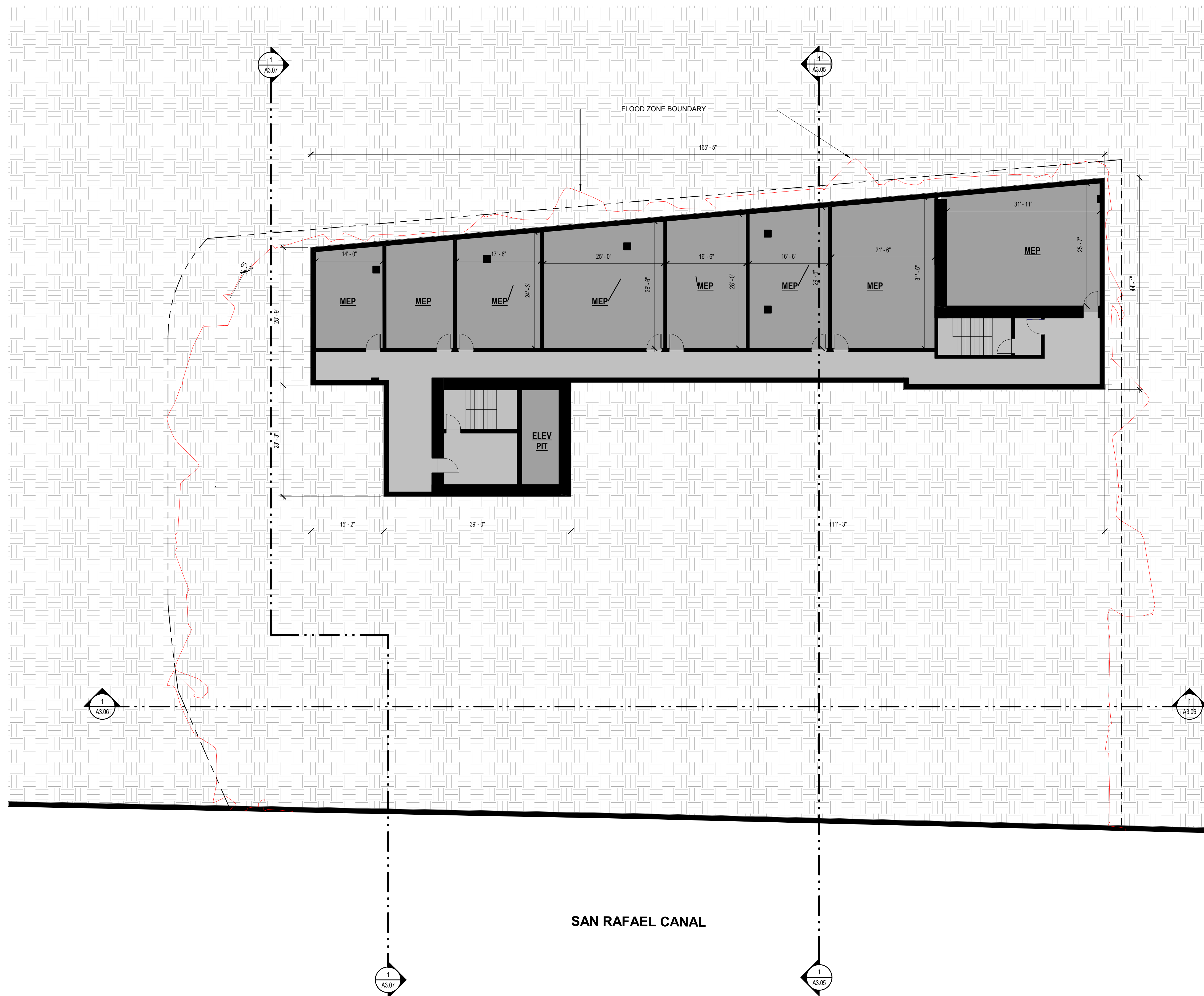
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STATE DENSITY BONUS - BASE PROJECT

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 Checked By: Checker
 Project Number: 2024022

Sheet Number: **A0.41A**



1 LEVEL B1 - FLOOR PLAN
3/32" = 1'-0"

AREA OF PROPOSED USE:
RESIDENTIAL 2,435 SF
(INCL. NON-UNIT AREA)
UTILITY 4,740 SF

NO.	DATE	DESCRIPTION
1	10.24.2025	REVISED PLANNING PERMIT APPLICATION
2	04.10.2025	PLANNING PERMIT SUBMITTAL

SCB
Architecture, Planning,
Interior and Urban Design

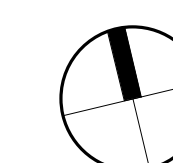
700 IRWIN STREET

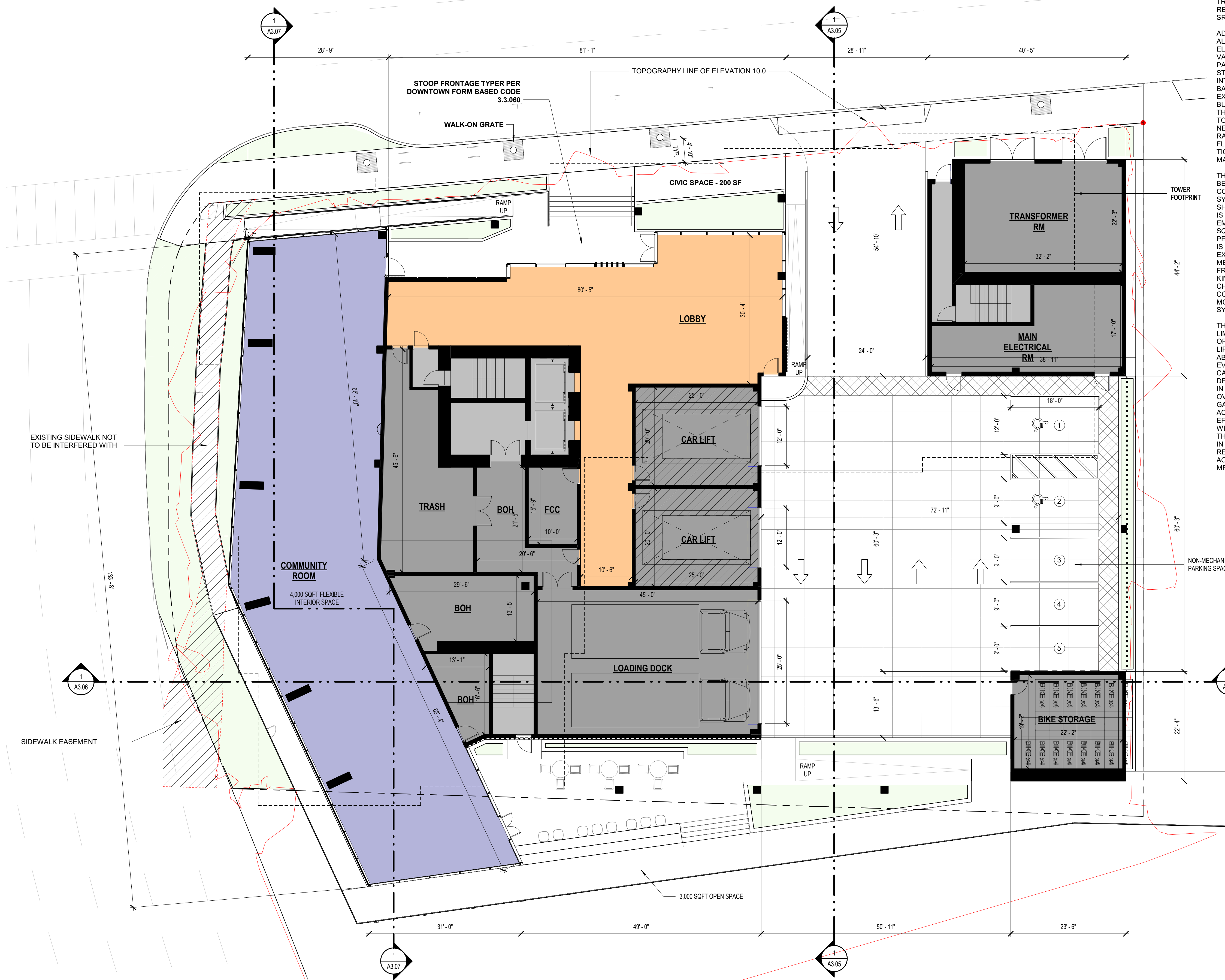
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LEVEL B1 - FLOOR
PLAN

Drawn By: Author
Checked By: Checker
Project Number: 2024022

Sheet Number: **A2.0B1**





WAIVER: PARKING CONTROLS (SMC SECTION 14.18)

THE PARKING CONTROLS SET FORTH IN SAN RAFAEL MUNICIPAL CODE CHAPTER 14.18 ANTICIPATE A TRADITIONAL PARKING GARAGE DESIGN AND CONFIGURATION WHERE USERS SELF-PARK THEIR VEHICLES, ACCESSING THE VARIOUS LEVELS OF THE GARAGE VIA A RAMP SYSTEM AND DRIVE AISLES. THE PROJECT WILL INSTEAD BE USING A MUCH MORE SPACE AND ENERGY EFFICIENT MECHANICAL PARKING SYSTEM THAT DOES NOT RELY ON TRADITIONAL PARKING DESIGN REQUIREMENTS AND REQUESTS A WAIVER FROM THE CONTROLS IN SRMC SECTION 14.18.

ADVANCES IN MECHANICAL PARKING TECHNOLOGY ALLOW FOR AN AUTOMATED SYSTEM THAT USES ELEVATOR CAR LIFTS AND INDIVIDUAL ELECTRIC VALET ROBOTS TO MOVE THE CARS FROM THE PARKING ACCESS AREA TO THE PARKING LEVEL CAR STORAGE AREA. EACH INDIVIDUAL CAR IS DRIVEN INTO ONE OF THE INDIVIDUAL CAR LIFT GARAGE BAYS, AND WHEN THE DRIVER AND PASSENGERS EXIT THE CAR THEY CAN WALK DIRECTLY INTO THE BUILDING. THE AUTOMATED SYSTEM THEN MOVES THE CAR UP INTO THE STORAGE AREA AND RESETS TO ACCEPT THE NEXT CAR. THIS ELIMINATES THE NEED FOR EXPENSIVE AND SPACE-CONSUMING CAR RAMP, AND AS CAN BE SEEN ON THE PARKING FLOOR PLANS (SEE SHEETS A2.03 - A2.05) CARS ARE TIGHTLY PACKED INTO THE AVAILABLE SPACE WITH MAXIMUM EFFICIENCY.

THE PROPOSED SYSTEM OFFERS MULTIPLE BENEFITS TO THE PROJECT AND SURROUNDING COMMUNITY, COMPARED WITH A STANDARD RAMP SYSTEM. BECAUSE CARS ARE TURNED OFF SHORTLY AFTER ENTERING THE PROPERTY, THERE IS AN IMMEDIATE DRAMATIC REDUCTION IN EXHAUST EMISSIONS, AMBIENT NOISE FROM ENGINES, TIRE SQUEAKING, AND HEADLIGHT GLARE BEYOND THE PERIMETER BUILDING WALLS. ADDITIONALLY, THERE IS SIGNIFICANTLY MORE FLEXIBILITY IN TERMS OF EXTERIOR ARCHITECTURAL TREATMENT, AS MECHANICALLY PARKED SPACES REQUIRE ONLY A FRACTION OF THE MECHANICAL VENTILATION. THIS KIND OF SYSTEM ALSO GREATLY REDUCES THE CHANCES FOR ANY PEDESTRIAN / VEHICULAR CONFLICTS AND IS SAFER, MORE SECURE, AND MORE EQUITABLE THAN TRADITIONAL PARKING SYSTEMS.

THIS PARKING SYSTEM ELIMINATES THE NEED FOR LIMITED NUMBERS OF SPECIAL DEDICATED TYPES OF PARKING SPACES. THE INDIVIDUAL GARAGE CAR LIFT USER EXPERIENCE GIVES THE SYSTEM THE ABILITY TO PROVIDE ACCESSIBLE ACCESS FOR EVERY PARKED CAR, AND ALLOWS EVERY PARKED CAR ACCESS TO EV CHARGING. THE HIGHER DENSITY ACHIEVED MEANS THE CARS ARE STORED IN A SHORTER BUILDING HEIGHT WITH A LOWER IMPACT ON OVERALL BUILDING HEIGHT THAN A TRADITIONAL GARAGE. FURTHER, A SMALLER FOOTPRINT CAN BE ACHIEVED DUE TO GREATLY IMPROVED SPATIAL EFFICIENCY WHILE THE FLAT AND LEVEL FLOORS WITH NO RAMPS MEANS PARKING LEVELS COULD IN THEORY BE EASILY CONVERTED TO ANOTHER USE IN THE FUTURE IF AND WHEN THERE IS LESS RELIANCE ON PERSONAL AUTOMOBILES. 2 PUBLICLY ACCESSIBLE CAR-SHARE SPACES INCLUDED IN THE MECHANICAL SYSTEM.

AREA OF PROPOSED USE:

RESIDENTIAL (INCL. NON-UNIT AREA)	3,535 SF
COMMERCIAL	5,000 SF
PARKING	2,980 SF
BOH/SERVICE	1,670 SF
UTILITY	1,715 SF

NO.	DATE	DESCRIPTION
1	10.24.2025	REVISED PLANNING PERMIT APPLICATION
2	04.10.2025	PLANNING PERMIT SUBMITTAL



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LEVEL 01 - FLOOR PLAN

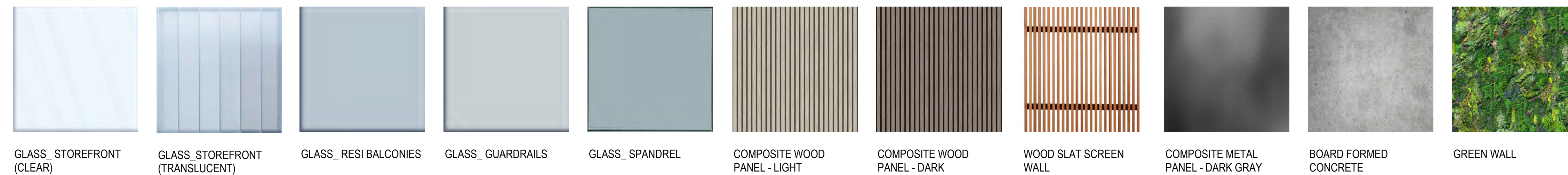
Drawn By: Author
Checked By: Checker
Project Number: 2024022

Sheet Number: **A2.01**

1 LEVEL 01 - FLOOR PLAN
3/32" = 1'-0"



MATERIALS LEGEND



NO.	DATE	DESCRIPTION
1	11.06.2024	SB330 APPLICATION
2	04.10.2025	PLANNING PERMIT SUBMITTAL
	10.24.2025	REVISED PLANNING PERMIT APPLICATION



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BUILDING NORTH ELEVATION

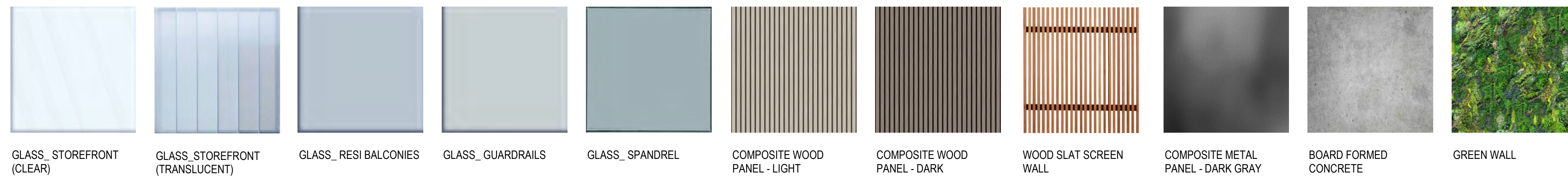
Drawn By: YOU (edit)
Checked By: PM / PD (edit this)
Project Number: 2024022

Sheet Number: **A3.01**



① WEST ELEVATION
1/16" = 1'-0"

MATERIALS LEGEND



NO.	DATE	DESCRIPTION
	10.24.2025	REVISED PLANNING PERMIT APPLICATION
2	04.10.2025	PLANNING PERMIT SUBMITTAL
1	11.06.2024	SB330 APPLICATION



700 IRWIN STREET

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BUILDING WEST ELEVATION

Drawn By: Author
Checked By: Checker
Project Number: 2024022

Sheet Number: **A3.04**

EXHIBIT 8

October 24, 2025
BKF No. C20232412-11



Mr. Chris Collins
Mr. David Kriozere
700 Irwin Street Partners, LLC
489 Harrison Street
San Francisco, CA 94105
Transmitted Via Email: ccollins@urbanpacificsf.com

**Subject: FEMA Flood Zone and Mitigation Strategies
700 Irwin Street, San Rafael**

Dear Chris and David:

This memorandum provides an overview of the Federal Emergency Management Agency (FEMA) floodplain management requirements and associated mitigation strategies applicable to the proposed project at 700 Irwin Street in San Rafael, California. Based on the current FEMA Flood Insurance Rate Map (FIRM) for the project area, the majority of the site is located outside of the Special Flood Hazard Area (SFHA); however, portions of the site fall within FEMA Zone AE, which is defined as an area subject to inundation by the 1% annual chance (100-year) flood (see Figure 1 below). Zone AE areas are associated with established Base Flood Elevations (BFEs) that identify the expected floodwater surface during the base flood event, which in this case is Elevation 9.7. In accordance with the National Flood Insurance Program (NFIP) regulations codified in Title 44 of the Code of Federal Regulations (CFR), new development within SFHAs must be designed and constructed to minimize flood risk, ensure public safety, and maintain eligibility for federal flood insurance. If the proposed building footprint is revised such that all portions of the structure are located entirely outside of the floodplain encroachment area, and the revised grading and building elevations are subsequently supported by a Letter of Map Amendment (LOMA) issued by FEMA, the project would be considered compliant with, or no longer subject to, the applicable floodplain management requirements. This memorandum summarizes the applicable standards/regulations, elevation and floodproofing requirements, and identifies potential mitigation measures that may be incorporated into the project design to achieve regulatory compliance and reduce flood-related vulnerabilities.



Figure 1: FEMA FIRM Map 06041C0457E

Federal Regulations for Floodplain Management

Below is a summary of federal regulations governing minimum floodplain management requirements under the National Flood Insurance Program (NFIP) which relate to our project and establish the framework a project must adopt to participate in the NFIP and maintain eligibility for federally backed flood insurance.

44 CFR Part 59 – General Provisions

Part 59 establishes the framework of the NFIP, administered by FEMA. It defines the program's authority under the National Flood Insurance Act of 1968, provides standardized terminology (e.g., "Special Flood Hazard Area," "Base Flood Elevation," etc.) and sets the conditions for community participation in the NFIP. Part 59's primary objectives are to make flood insurance available in communities that adopt compliant floodplain management ordinances and reduce future flood losses through regulation.

44 CFR Part 60 – Criteria for Land Management and Use (Zone AE)

For new construction within a Zone AE Special Flood Hazard Area (SFHA), Part 60 requires communities to adopt and enforce minimum floodplain management standards to reduce risk to life and property. The key provisions applicable to our site include the following:

- **Elevation Requirement:** The lowest floor of all new residential buildings, including basement, must be to a height equal to or exceeding one (1) foot above the Base Flood Elevation identified on the Flood Insurance Rate Map (FIRM) plus predicted thirty (30) years settlement. Base Flood Elevation (BFE) Non-residential buildings may either be elevated one foot above BFE plus predicted 30 years settlement or designed with certified dry floodproofing systems.
- **Design and Construction:** Enclosures below the BFE (such as crawlspaces or parking garages, excluding basements) must be constructed with engineered flood openings to allow for equalization of hydrostatic forces. Materials and methods used below the BFE must be flood-resistant.
- **Site Development:** Electrical panels, transformers, hot water heaters, and other MEP equipment essential for residential services and life safety of inhabitants must remain operational after a flood event and be located above the BFE.

ASCE 24 – Garage Use Designation

The below-grade parking garage shall not include signage, markings, or other indicators specifying a particular use, such as "residential," "commercial," or "tenant-only." For purposes of FEMA floodplain management and NFIP compliance, the garage is to be considered a general-use parking facility accessible to all building occupants. This designation ensures the space is not classified as a use subject to the more stringent elevation and floodproofing requirements applicable to residential occupancies within the Special Flood Hazard Area. This approach is consistent with FEMA regulations under 44 CFR § 60.3 and ASCE 24 Section 1.4

Flood Mitigation Strategies

1. Garage Entry Ramp

A proposed entry ramp serving a below-grade parking garage would start at the interface with Second Street at an elevation below the Base Flood Elevation (BFE) of 10.0 feet. The ramp would then ascend to a finished grade elevation of 12 feet at the garage entry threshold. To ensure compliance with applicable FEMA floodplain management standards and ASCE 24 requirements, the garage entrance will be dry-floodproofed to prevent the intrusion of floodwaters up to and including the design flood elevation. As a result, all points of



access to the garage will remain protected and maintain flood resistance consistent with minimum design criteria. See Item #2 below regarding dry floodproofing strategies that will be considered for use on the project to ensure that garage entry remains protected and accessible during flood conditions.

2. Dry Floodproofing

Dry floodproofing is a strategy utilized in **non-residential and mixed-use buildings** located in a FEMA SFHA with a Zone AE designation. Employed to protect structures and ancillary uses, dry floodproofing are designs and strategies that create a watertight solutions for structures to protect against floodwaters and associated damage. When implemented, dry floodproofing mitigations protect exterior building entrances and openings along the perimeter and may include the following:

- a. Elevating doors, windows, parking entrances and openings along the perimeter of the building above the BFE with 1-foot of freeboard where interior elements are located below the BFE, such as commercial parking areas in mixed-use buildings
- b. Sealing and flood-proofing exterior walls and foundations
- c. Providing watertight panels at walls and foundations

3. Wet Floodproofing

Wet floodproofing is a strategy utilized for enclosures below new **elevated residential structures** and accessory and agricultural structures that have been issued variances by a local community that are located in a FEMA SFHA where wet floodproofing is permitted. Wet floodproofing **is not permitted for new non-residential buildings**, but may be allowed by an Agency's floodplain manager for existing non-residential building on a case by case basis depending on specific site conditions.

In new residential buildings, primary livable areas, utilities, appliances, and other active uses of the structures must still be elevated above the BFE with the required freeboard while uses, such as parking, crawlspaces, building access and storage, may be wet floodproofed. Unlike dry floodproofing, wet floodproofing measures providing mitigations that allow floodwaters to enter permitted areas of a structure or enclosure while resisting and limiting damage from flooding. When implemented, wet floodproofing mitigations may include the following:

- a. Anchoring foundations and structures
- b. Using Flood resistant materials
- c. Sealing and flood-proofing interior walls and foundations
- d. Installing flood openings to allow flood water to enter and exit a wet floodproofed area and equalize hydrostatic forces
- e. Using mechanical solutions, such as pumps, to discharge floodwaters

4. Elevating Project Sites with Fill

Historically, filling of sites to raise building grades above the identified Base Flood Elevation is a common approach for provide protection against flood waters. Implementation and permitting of this solutions was processed through the Conditional Letters of Map Revision Based on Fill (CLOMR-F) and Letters of Map Revision Based on Fill (LOMR-F) applications. In the last few years, this approach has become more complex due to FEMA's current suspension of CLOMR-F and LOMR-F review. This suspension, which is a result of an ongoing environmental lawsuit, prevents FEMA from accepting applications based on fill to remove buildings from flood zones in certain areas ([FEMA Suspends Flood Map Reviews in 39 California Counties | FEMA.gov](#)). While this situation is subject to change, it is uncertain how long the moratorium will remain in place or what adjustments to regulations may occur afterward.



It is important to understand the implications of the moratorium, including the following considerations for projects seeking permits:

- The lowest finished floor of residential buildings will need to be designed above the Base Flood Elevation (BFE) with 1-foot of freeboard for insurance purposes.
- Owners will need to carry flood insurance until the moratorium is lifted.

While flood insurance will be required under the current moratorium, it could potentially be removed once the moratorium is lifted. Thus, the project should consider the following options:

- Elevation of Buildings (Fill): Raising the site grade to meet the BFE and constructing foundations on top.
- Elevation of Buildings (Structural): Constructing unfinished, flood-resistant enclosures for parking or storage at ground level. Lowest floor of the residential occupancy areas is constructed to a height equal to or exceeding one (1) foot above the BFE plus predicted thirty (30) years settlement.
- Elevating Utilities: Ensuring critical systems (electrical, plumbing, HVAC) are installed to a height equal to or exceeding one (1) foot above the BFE plus predicted thirty (30) years settlement.

See Table 2-1 below for the summary of requirements from the National Flood Insurance Program for Flood Mitigation Measures for Multifamily Buildings.

Table 2-1. NFIP requirements that vary based on occupancy

DESCRIPTION	RESIDENTIAL	NON-RESIDENTIAL	MIXED-USE
Wet floodproofed enclosures below the BFE (elevated building)	Allowed ² for parking, storage, and building access	Allowed ² for parking, storage, and building access	Allowed ¹ for parking, storage, and building access
Below-grade areas (basements)	Prohibited	Allowed ² if dry floodproofed	Allowed ¹ for non-residential portions, if dry floodproofed
Below-grade parking	Prohibited	Allowed ² if dry floodproofed	Allowed ¹ for non-residential portions, if dry floodproofed
Dry floodproofed areas below the BFE	Prohibited for dwelling units and areas that support dwelling units, including but not limited to: lobbies, foyers, and other ancillary areas (including offices, mail rooms, meeting rooms, and exercise rooms)	Allowed ² for all areas	Allowed ¹ for non-residential portions
Mechanical, Electrical, and Plumbing Systems	Located at or above the BFE or, if below the BFE, designed to resist flood loads and prevent water from entering or accumulating within the components	Located at or above the BFE or, if below the BFE, designed and/or located to prevent water from entering or accumulating within the components; can be within a dry floodproofed area ¹	Located at or above the BFE or, if below the BFE, designed and/or located to prevent water from entering or accumulating within the components; only components associated with non-residential uses can be within a dry floodproofed area ¹

² Allowed means in compliance with NFIP requirements. Dry floodproofing a building is only allowed in Zone A.



5. National Flood Insurance Program (NFIP) MT-2 process

The NFIP MT-2 process is part of FEMA's effort to update or modify flood hazard information on Flood Insurance Rate Maps (FIRMs). The MT-2 process is used when an entity (like a community, developer, or property owner) seeks changes to flood zone designations on FIRMs due to new or improved flood hazard data, developments, or other changes affecting flood risk. MT-2 process:

- **Application Submission:** The applicant submits a Letter of Map Revision (LOMR) or Physical Map Revision (PMR) request through FEMA's MT-2 application process. This includes new technical data (e.g., topographic data, hydrologic and hydraulic analyses) that justifies changes to the current FIRMs.
- **Technical Review:** FEMA reviews the submission to determine if the new data or proposed changes warrant a modification of the floodplain boundaries or other map features. The review evaluates factors such as flood elevations, floodway boundaries, and base flood elevations (BFEs).
- **Notification and Appeal:** If FEMA deems the change valid, they will issue a preliminary decision and notify affected communities and property owners. There is a 90-day appeal period during which anyone can submit comments or objections.
- **Final Determination:** After the appeal period and any necessary revisions, FEMA issues a final determination. The FIRMs are updated accordingly to reflect the new flood risk information.
- **Community Adoption:** Communities must adopt the updated flood map information into their local floodplain management regulations to maintain eligibility for the NFIP.

This process ensures that flood insurance rates and floodplain management practices are based on the most accurate and up-to-date flood risk data available and could remove the area from the flood zone.



Flood Mitigation Strategies by Property Use

Based on past experience in the City of San Rafael and working on residential, multi-family residential, non-residential, and mixed-use buildings located in FEMA SFHA zones, the following table summarizes the potential flood mitigations for building typologies in the SFHA AE zone that are under consideration for the 700 Irwin Street property:

Building Type	Use	Constructed Below BFE	Constructed Above BFE	Flood Openings	Flood Proofing	Elevate Utilities
Mixed-Use	Shared Primary Lobby	x			x (dry)	x
	Residential Only Lobby ¹		x			
	Commercial	x			x (dry)	x
	Commercial Parking	x			x (dry)	x
	Residential Parking		x ²			
	Residential		x			
Multi-Family Residential	Lobbies, Storage	X			x (wet)	x
	Parking		X			
	Residential		x			

If the proposed building footprint is revised such that all portions of the structure are located entirely outside of the floodplain encroachment area, and the revised grading and building elevations are subsequently supported by a Letter of Map Amendment (LOMA) issued by FEMA, the project would be considered compliant with, or no longer subject to, the applicable floodplain management requirements. This approach would effectively remove the development from the regulated Special Flood Hazard Area and associated elevation and floodproofing requirements.

Sincerely,

Cory Bannon, PE
 Project Manager
BKF Engineers

Cc: Simon North, PE – Principal/Project Executive

¹ A mixed-use building with a shared primary lobby below the BFE must include a separate residential only lobby constructed above the BFE.

² In California, proposed parking for buildings with residential uses have been amended as part of the State legislative process to remove minimum requirements. Thus, some jurisdictions have interpreted that parking in a mixed-use building is not assigned to a specific use and may be dry floodproofed. It is recommended that this interpretation be reviewed with the City of San Rafael.



EXHIBIT 9



Downtown Form-Based Code

CHAPTER

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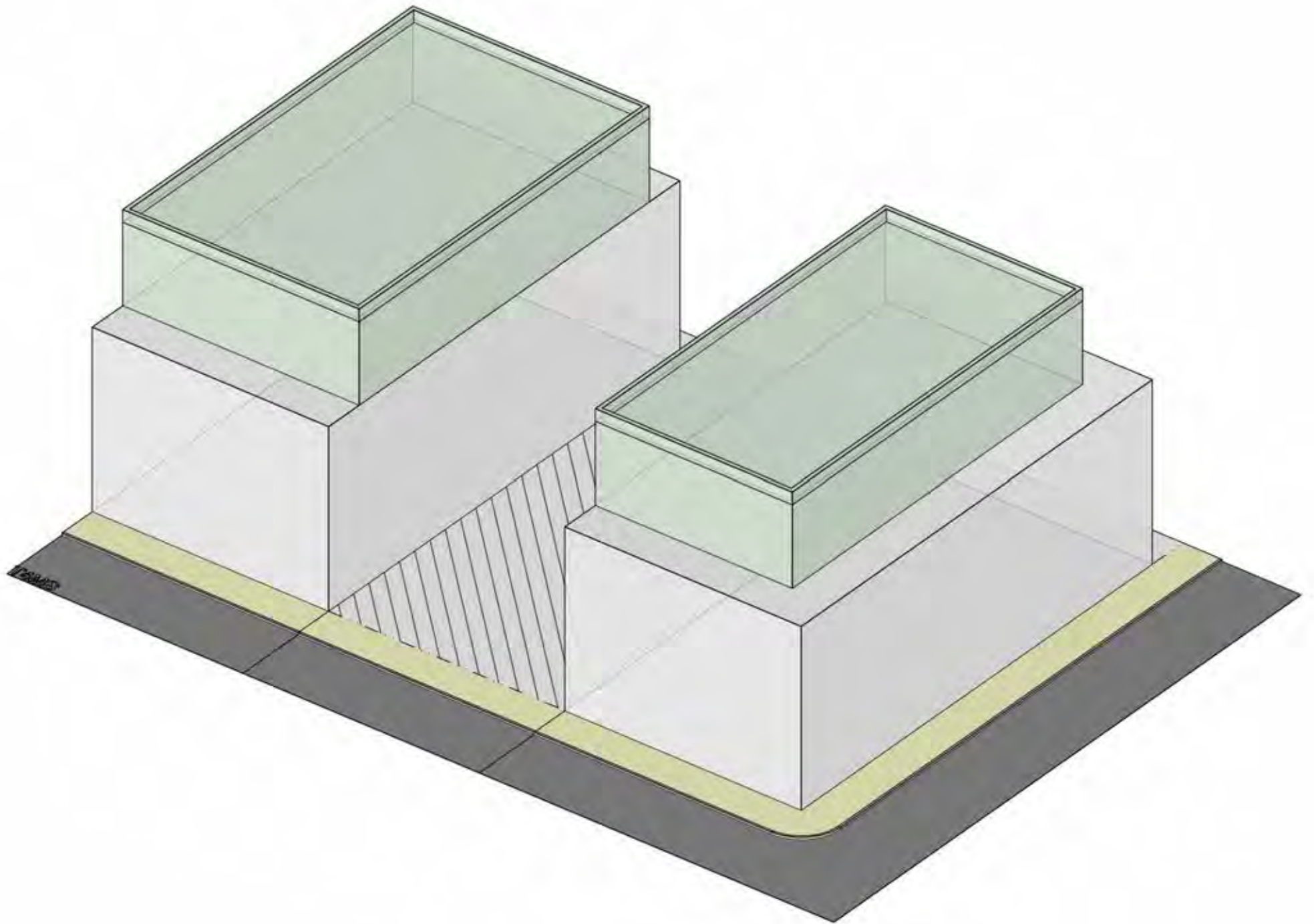
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Introduction

ARTICLE

1

Quick Code Guide



Note: *this diagram is intended to provide a simplified overview of the code's requirements and is descriptive, not regulatory.*



Division 1.1 Purpose

1.1.010 Purpose

This Downtown Form-Based Code (DTFBC) is enacted for three key purposes:

- To implement the vision described in the Downtown Precise Plan (Plan) for a variety of urban neighborhoods and main street environments;
- To provide the community with a clear understanding of what the code requires and what it allows and generates regarding physical form, character and uses; and
- To streamline the review and processing of development projects through the codes' coordination with the Plan and through its clarity of standards and expectations.

1.1.020 Applicability

Rules of Construction

The following general rules of construction apply to the text of this Chapter:

Terminology: shall, may and should. "Shall" is always mandatory and not permissive. "May" is permissive. "Should" is advisory and identifies guidance provided by the City in implementation of these standards.

Tenses and Numbers. Words used in the present tense include the future, words used in the singular include the

plural, and the plural includes the singular, unless the context clearly indicates the contrary.

Applicable. The applicable standards of Chapter Nine (Downtown Form-Based Code) are acknowledged to apply so as to not require stating the phrase "and all applicable standards" throughout.

Conjunctions. Unless the context clearly indicates otherwise, the following conjunctions shall be interpreted as follows:

- "And" indicates that all connected items or provisions apply;
- "Or" indicates that the connected items or provisions may apply; and
- "Either/or" indicates that the connected items or provisions apply singly but not in combination.

Relevance. In form-based zones, the standards in Chapter Nine (Downtown Form-Based Code) prevail unless stated otherwise.

Applicability of Standards

The standards in Chapter Nine (Downtown Form-Based Code) apply to all proposed development and improvements within form-based zones as identified below.

Non-Conformities. See San Rafael Municipal Code (SRMC) Section 14.16.270 (Nonconforming Structures

and Uses) for when the standards of the Chapter Nine (Downtown Form-Based Code) apply.

New Development. New development, additions and renovations are required to be designed per the zone standards identified for the parcel(s) in DTFBC Figure 2.2.040.A (Regulating Plan).

Blocks and Streets

- Development sites larger than two acres shall be divided into new blocks in compliance with DTFBC Table 3.1.050.A (Block Size Standards).
- Development sites larger than two acres are required to include new streets including any required in the approximate locations identified on the DTFBC Figure 2.2.040.A (Regulating Plan) in compliance with Chapter Six (Transportation and Parking).
- When designing a new street or retrofitting an existing street, the guidelines in Chapter Six (Transportation and Parking) apply.

New Buildings. New buildings and their additions are required to be designed in compliance with the building placement and size standards of the zone.

General. From the allowed types in the zone, and in compliance with the listed standards, the following shall be selected for each lot:

- At least one frontage type for each street or civic space frontage; and
- At least one use type.

Frontage types not listed in the zone's standards are not allowed in that zone.

Use types not listed in the DTFBC Table 2.3.070.A (Use Table) are not allowed in that zone except as set forth in SRMC Section 14.02.040.B (Land Use Categories).

Site Standards. When a development requires approval in compliance with SRMC Title 14 (Zoning), the standards of this Sub-Section apply to the following:

Screening. The standards of DTFBC Section 3.1.020 (Screening) apply to the following:

- All new development; and
- Improvements to existing development.

Landscaping and Tree Standards. The standards of DTFBC Section 3.1.030 (Landscaping) apply to the following:

- All new development; and
- Improvements to existing development.

Parking and Loading. The requirements of DTFBC Sub-Section H (Parking) of the zone standards apply to the following, except as superseded by other City ordinances regarding affordable housing:

- New development;
- Changes in intensity or uses of buildings or structures made after the effective date of this Article that cause an increase of 25 percent or greater in:
 - Gross floor area over 5,000 sf;
 - Seating capacity;
 - Units; and/or
 - Parking spaces.

Civic Space Standards

- New buildings or additions are required to include civic space as identified in DTFBC Sub-Section C (Building Placement) of the zone.
- Development sites larger than two acres are required to create new civic space(s) in the approximate locations identified on the DTFBC Figure 2.2.040.A (Regulating Plan) in compliance with the standards of DTFBC Civic Space in Sub-Section C of the zone.

Massing, Facade Articulation and Architectural Elements.

The standards of DTFBC Division 3.2 (Massing and Facade Articulation Standards) apply to the following:

- New building; and
- Building facade renovation facing a street or civic space (except public safety buildings).

Frontage Standards. The standards of DTFBC Division 3.3 (Frontage Standards) apply to the following:

- New building;
- Building facade renovation facing a street or civic space (except public safety buildings);
- Private property improvement along front or side street; and
- Modification of pedestrian entrance(s) along front or side street.

Signage Standards. See SRMC Chapter 14.19 (Signs) for signage standards and processing requirements.

1.1.030 Relationship to General Plan 2040 Update

Chapter Nine (Downtown Form-Based Code) of the Plan is a refinement of the community vision and intent in the General Plan 2040 Update (General Plan) for the parcels and rights-of-way within the Plan boundaries.

Chapter Nine (Downtown Form-Based Code) implements the City's General Plan vision within the boundaries of the Plan to implement the General Plan direction for a variety of walkable environments. Walkable is described as an environment that is pedestrian-oriented in nature, where bicycling and walking are viable daily options because services, retail, or restaurants are within a short walking distance of most dwellings.

1.1.040 Relationship to Downtown Precise Plan

Chapter Nine (Downtown Form-Based Code) of the Plan implements the Plan's updated community vision and intent for the parcels and streets within the Plan boundaries. In the event of a conflict the standards of this Chapter shall apply.

1.1.050 Relationship to Other City Code Standards

The standards described in this Chapter prevail over existing standards unless specifically stated otherwise in Table 1.1.040A. All parcels covered by the Downtown Form Based Code are considered to be in the Downtown Mixed Use (DMU) Zone District and are referenced as such in other parts of the San Rafael Municipal Code.

Table 1.1.040.A Relationship to California Fire Code, SRMC Title 2 (Administration), SRMC Title 14 (Zoning), and SRMC Title 19 (Open Space)

Division/ Chapter/ Section	Description	Status
California Fire Code		
Chapter 5: Section 503 and Appendix D, Section 504, Section 505, Section 507 and Section 509	Fire Service Features	Chapter Nine (Downtown Form-Based Code) relies on Chapter 5 (Five Service Features)
SRMC Title 2 (Administration)		
Chapter 2.18	Historic Preservation	Chapter Nine (Downtown Form-Based Code) relies on SRMC Chapter 2.18
SRMC Title 14 (Zoning)		
Division II	Base District Regulations	Replaced by form-based zones within the Plan boundaries.
Division III	Overlay District Regulations	Replaced by form-based zones within the Plan boundaries.
Division V	Administrative Regulations	Chapter Nine (Form-Based Zones Code) relies on SRMC Division V (Administrative Regulations).
Chapter 14.03	Definitions	DTFBC Division 4.1 (Definitions) adds definitions to SRMC Chapter 14.03 (Definitions) only within the Plan boundaries.
Chapter 14.17	Performance Standards	Chapter Nine (Downtown Form-Based Code) relies on SRMC Chapter 14.17 (Performance Standards).
Chapter 14.18	Parking Standards	SRMC Table 14.18.040 (Parking Requirements) replaced by Chapter Nine (Downtown Form-Based Code) within the Plan boundaries. All other standards of SRMC Chapter 14.18 (Parking Standards) apply.
Chapter 14.19	Signs	Chapter Nine (Downtown Form-Based Code) relies on SRMC Chapter 14.19 (Signs).
Section 14.16.270	Non-Conforming Structures and Uses	Chapter Nine (Downtown Form-Based Code) relies on SRMC Section 14.16.270 (Non-Conforming Structures and Uses).
Section 14.16.295	Sight Distance	SRMC Section 14.16.295 (Sight Distance) does not apply within the Plan boundaries.
Sections 14.16.243 and 14.18.160	Screening Standards	Chapter Nine (Downtown Form-Based Code) replaces SRMC Sections 14.16.243 (Mechanical Equipment Screening) and 14.18.160 (Parking Lot Screening and Landscaping) within the Plan boundaries.
Sections 14.18.170, 14.16.227, and 14.19.055	Outdoor Lighting Standards	Chapter Nine (Downtown Form-Based Code) relies on SRMC Sections 14.18.170 (Lighting), 14.16.227 (Light and Glare) , and 14.19.055 (Illumination Standards).
Tables 14.04.020, 14.05.020, 14.05.022, 14.08.030, 14.09.020, 14.10.020 and Section 14.07.020	Land use regulations	DTFBC Table 2.3.070.A (Use table) replaces SRMC Title 14 (Zoning) Use Tables within the Plan boundaries. All other standards of Division II apply.
SRMC Title 19 (Open Space)		
Chapter 19.10	Land and Water Areas	Chapter Nine (Downtown Form-Based Code) relies on SRMC Title 19 (Open Space).

1.1.060 Permit Required

New buildings, renovations, additions, and signage require City approval as identified in DTFBC Table 1.1.060.A (Permit Requirements). Please refer to SRMC Title 14 (Zoning) for permit application requirements and

procedures. For projects involving a historic resource, refer to SRMC Chapter 2.18 (Historic Preservation).

Table 1.1.060.A Permit Requirements				
Application	Permit Required			
	Administrative Environmental and Design Review Permit	Minor Environmental and Design Review Permit	Major Environmental and Design Review Permit	Certificate of Appropriateness
Alteration to Historic Resource	-	-	•	•
Renovation/ expansion < 50% of existing building	•	-	-	-
Renovation/ expansion > 50% of existing building	-	•	-	-
New building up to 30' overall height	-	•	-	-
With height bonus up to 40' overall height	-	•	-	-
New building up to 40' overall height	-	•	-	-
With height bonus up to 50' overall height	-	-	•	-
New building up to 40' overall height	-	•	-	-
With height bonus up to 60' overall height	-	-	•	-
New building up to 50' overall height	-	-	•	-
With height bonus up to 70' overall height	-	-	•	-
New building up to 60' overall height	-	-	•	-
With height bonus up to 80' overall height	-	-	•	-
New building up to 70'	-	-	•	-
With height bonus up to 90' overall height	-	-	•	-

Table 1.1.060.A Permit Requirements (Continued)				
Application	Permit Required			
	Administrative Environmental and Design Review Permit	Minor Environmental and Design Review Permit	Major Environmental and Design Review Permit	Certificate of Appropriateness
Development of site > 2 acres	-	-	•	-
Alteration adjacent to a historic resource in compliance with SRMC Section 2.18.065 (Development Standards)	•	-	-	-
Alteration adjacent to a historic resource not in compliance with SRMC Section 2.18.065 (Development Standards)	-	-	•	-

For Minor and Major Environmental and Design Review, see SRMC Section 14.25. Major review applies to “major physical improvements” [see 14.25.040 (A)] and Minor review applies to “minor physical improvements” [see 14.25.040 (B)]. See also Administrative Design Review process for smaller projects.

For review criteria for Major and Minor Environmental and Design Review, see SRMC Section 14.25.050; and Section 14.25.060 for hearing/public review requirements. Minor Review is done in a public hearing convened by the Zoning Administrator and Major Review is performed by the Planning Commission.

For Permanent Sign see SRMC Section 4.19.041 (Sign Permit Required).





Downtown Form-Based Zones

ARTICLE

2

Division 2.1 Preamble



2.1.010 The Natural-to-Urban Transect: The Framework for Form-Based Planning and Coding

The Natural-to-Urban Transect is an organizing principle used in Form-Based planning and coding that establishes a hierarchy of places/ contexts from the most natural to the most urban. The designation of each transect along this hierarchy is determined first by the character and form, intensity of development, and type of place and secondly by the mix of uses within the area. This hierarchy of places becomes the framework or organizing principle for the entire plan and code, replacing use as the organizing principle as is used in conventional or Euclidean zoning. Transect zones are used to reinforce existing or to create new walkable mixed-use urban environments.

“The Natural-to-Urban Transect is a means for considering and organizing the human habitat in a continuum of intensity that ranges from the most rural condition to the most urban. It provides a standardized method for differentiating between the intentions for urban form in various areas using gradual transitions rather than harsh distinctions. The zones are primarily classified by the physical intensity of the built form, the relationship between nature and the built environment, and the complexity of uses within the zone.”

~ Form-Based Codes Institute

The model transect for American towns is divided into six transects: Natural (T1), Rural (T2), Sub-Urban (T3), General Urban (T4), Urban Center (T5), and Urban Core (T6), together with a District (D), often referred to as a Special District, a designation for areas with specialized purposes (e.g., heavy industrial, transportation, entertainment, or university districts, among other possibilities). Each transect is given a number. Higher numbers designate progressively more urban environments, and lower numbers designate more rural environments.

For additional information visit the Form-Based Codes Institute website at www.formbasedcodes.org

2.1.020 Regulating for Different Contexts

The Form-Based Code applies a community-character-based approach to zoning that is based upon the classification of three distinct context types. Each context type—Natural, Walkable, and Auto-Oriented Suburban—needs to be regulated differently in order to effectively reinforce the intended context.

Natural Context Type consists of land not intended for development. In these areas, nature dominates a person's experience, but may include an occasional recreation-oriented or utility building or other man-made feature. The use of cars is integrated, but does not dominate the physical character. These areas are implemented through conventional, use-based zones.

Auto-Oriented Suburban Context Type consists of areas developed mainly after the 1950's. In this context type land uses are segregated and often buffered, leaving large distances between them contributing to the need for the automobile for day-to-day functions. Walking and cycling occur in these areas, but generally for recreational purposes due to low connectivity. These areas are implemented through conventional, use-based zones.

Walkable Context Type consists of areas where a person can walk, bicycle or ride transit to work to fulfill most shopping and recreation needs. These areas allow for but do not require the use of a vehicle to accommodate most daily needs. These areas were primarily developed prior to the 1940's in a pattern where a person can live with limited reliance on the automobile. Today, these areas are still conducive to destination walking and cycling and supported through public transit, a network of interconnected, tree-lined streets, a diversity of housing choices, and a mix of appropriate commercial and residential uses in a compact form. These areas are implemented through the T3-T5 form-based zones.

2.1.030 The Transect

Each of the six transects is implemented by form-based zones (Zones) as illustrated in DTFBC Figure 2.1.030.A (The Transect). The zones needed to implement the transect for Downtown San Rafael are identified with the highlighted purple.

2.1.040 The Transect Applied to Downtown San Rafael

To enhance Downtown's San Rafael current walkable character, this Form-Based Code applies transect zones related to the Walkable Context Type (i.e., T4 and T5 Neighborhood and Main Street). In this way, this FBC enables distinct walkable development patterns that are based on interconnected streets and blocks, variety of housing choices, and proximity to services, shopping and/or transit.

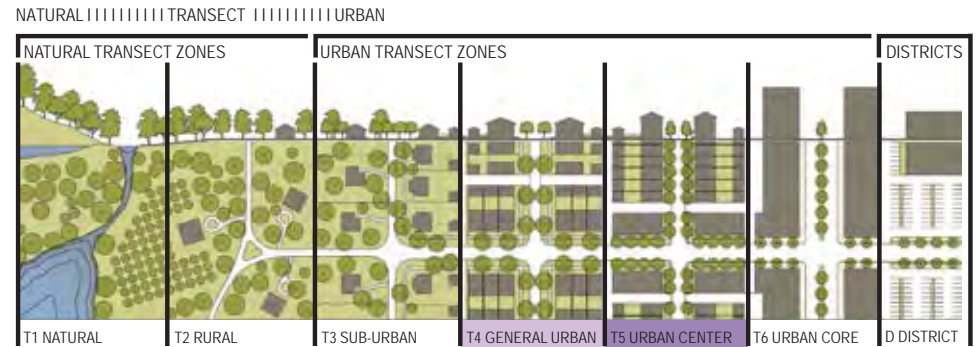


Figure 2.1.030.A The transect



Figure 2.1.040.A The Transect applied to Downtown San Rafael.

Division 2.2 Establishment and Designation of Downtown Zones

2.2.010 Downtown Zones Established

Form-Based Zones and Standards. This Section describes the form-based zones and standards. Each zone is established based on the intended physical form and character of the environments described in the Plan. These zones focus on mixed-use, walkable environments and range in function and intensity:

Moderate intensity neighborhoods:

- T4 Neighborhood T4N 30/40 (Section 2.3.030)
- T4 Neighborhood T4N 40/50 (Section 2.3.030)

A community-serving Downtown:

- T4 Main Street T4MS 40/50 (Section 2.3.040)
- T4 Main Street T4MS 40/60 (Section 2.3.040)
- T4 Main Street T4MS 50/70 (Section 2.3.040)
- T4 Main Street T4MS 60/80 (Section 2.3.040)

High intensity neighborhoods:

- T5 Neighborhood T5N 40/60 (Section 2.3.050)
- T5 Neighborhood T5N 50/70 (Section 2.3.050)

A regional-serving transit center and main street:

- T5 Main Street T5MS 70/90 (Section 2.3.060)

The naming of the form-based zones is based on a spectrum of context types in the City from less urban to more urban as listed in Table 2.3.020.A (Downtown Zones Overview).

2.2.020 Height Distinctions within Zones

Height bonuses listed below and shown on the Regulating Plan may only be used in lieu of those allowed under State Density Bonus law for qualifying projects. The bonuses allowed by the form-based zones may not be added to or combined with State Density Bonuses. The maximum height allowed per zone is identified on the Regulating Plan as a suffix to the zone name. For example, the T4N 40/50 allows up to 40' base height and up to 50' with a bonus. See DTFBC Table 2.2.020.A (Height Distinctions per Zone).

Table 2.2.020.A Height Distinctions per Zone

Zone	Base Height (Overall)	With Height Bonus (Overall)
T4 Neighborhood		
T4N 30/40	30' max.	40' max.
T4N 40/50	40' max.	50' max.
T4 Main Street		
T4MS 40/50	40' max.	50' max.
T4MS 40/60	40' max.	60' max.
T4MS 50/70	50' max.	70' max.
T4MS 60/80	60' max.	80' max.
T5 Neighborhood		
T5N 40/60	40' max.	60' max.
T5N 50/70	50' max.	70' max.
T5 Main Street		
T5MS 70/90	70' max.	90' max.

2.2.030 Sub-Zones

Sub-zones are slight variations of the base zone, lot-specific, and mapped on DTFBC Figure 2.2.040.A (Regulating Plan). This Article includes two types of sub-zones:

Open. The open sub-zone is applied for either or both of the following purposes:

- To allow more uses than the base zone allows in specific areas but within the same form and character of the base zone; and/or
- To more easily allow certain uses that are already allowed in the base zone. In addition in this way, the open sub-zone can provide additional flexibility to lots located at or near intersections that function or can function as a neighborhood node of non-residential uses.

2.2.040 Regulating Plan

The zones established in this Article are mapped on DTFBC Figure 2.2.040.A (Regulating Plan). In addition to identifying the zoning for each lot, DTFBC Figure 2.2.040.A (Regulating Plan) identifies specific urban design requirements and height limitations based on location and intended physical character.

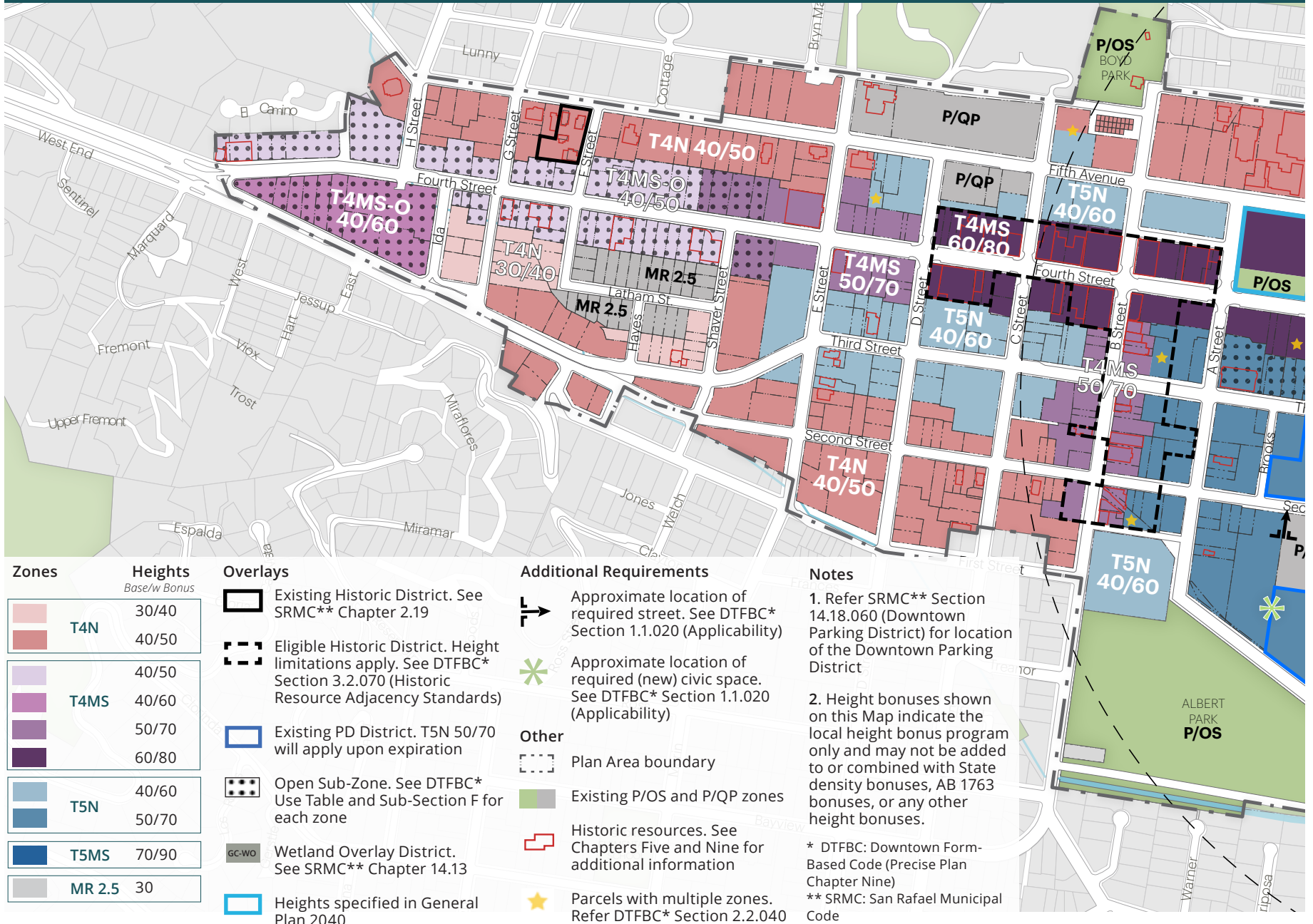
Where DTFBC Figure 2.2.040.A (Regulating Plan) identifies two zones on one parcel, the boundary of each shall be

determined by a minimum depth of 100 feet measured perpendicular to the adjacent right-of-way or civic space from the midpoint of the lot width. In cases where the block depth is inadequate to support this measurement, zone boundaries may be adjusted during the project review process. As a general rule boundaries of adjacent parcels must be considered in boundary identification and adjustment during the review process.

When a project combines multiple parcels from two or more zones, the zone boundaries shown on Figure 2.2.040.A (Regulating Plan) may be modified to accommodate the new parcel boundary but shall not result in new zones to be added to the parcel. This boundary adjustment is subject to Director review and approval unless referred to the Planning Commission.

The front of a parcel is assumed to be along the primary abutting street. In situations where parcels abut two primary streets (such as corner parcels, parcels in the West End between Second and Fourth Street, etc.), both primary street frontages should be treated as the front with applicable standards. Alternately, the determination of the front of such parcels shall be made by the Director.

Figure 2.2.040.A Regulating Plan. Note this is the same map as Figure 4.5 in Chapter Four: Design Vision



Division 2.3 Downtown Zones

2.3.010 Purpose

This Division establishes the palette of form-based zones to implement the Plan by generating and supporting the intended physical form and character and range of uses.

2.3.020 Overview of Downtown Zones

DTFBC Table 2.3.020.A (Downtown Zones Overview) provides an overview of each zone and its intent. This information is provided as background on the intended physical character, allowed range of uses, and direction for the detailed standards in each zone.

Following the Overview, each of the Downtown Zones and its standards begin at DTFBC Section 2.3.030 T4 Neighborhood (T4N 30/40 and T4N 40/50).

Collectively, the Downtown Zones comprise the Downtown Mixed Use (DMU) district, as referenced in Chapter 14 of the San Rafael Municipal Code.

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Table 2.3.020.A Downtown Zones Overview

← Less Urban

T4 Neighborhood



Zone Abbreviation

T4N 30/40 and T4N 40/50

Sub-Zone(s)

T4N 40/50 Open

The open sub-zone allows more uses, additional signage and sometimes additional frontages, within the same form and character of the base zone.

Intent

A walkable neighborhood environment of small-to-medium footprint, moderate-intensity mixed-use buildings and housing choices, supporting and within short walking distance of neighborhood-serving retail and services. This zone provides a transition in scale between the Downtown and adjacent residential neighborhoods.

Desired Form

Primarily House-Form Buildings

Building Height 30' to 50', as per Regulating Plan

Primarily Detached Buildings

Small Front Setbacks

Small Side Setbacks

Residential and Shopfront Frontages

T4 Main Street



Zone Abbreviation

T4MS 40/50, T4MS 40/60, T4MS 50/70, and T4MS 60/80

Sub-Zone(s)

T4MS 40/50 Open, T4MS 40/60 Open and T4MS 50/70 Open

The open sub-zone allows more uses, additional signage and sometimes additional frontages, within the same form and character of the base zone.

Intent

A walkable, vibrant district of medium-to-large footprint, moderate intensity, mixed-use buildings and housing choices, supporting neighborhood and community-serving ground floor shopping, food and services, including civic, institutional, maker/ craft/ artisan businesses (both indoor and outdoor).

Desired Form

Primarily Block-Form Buildings

Building Height 40' to 80', as per Regulating Plan

Attached Buildings

Small-to-No Front Setbacks

No Side Setbacks

Predominantly Shopfront Frontages

Table 2.3.020.A Downtown Zones Overview (Continued)

More Urban 

T5 Neighborhood



Zone Abbreviation

T5N 40/60 and T5N 50/70

Sub-Zone(s)

T5N 40/60 Open and T5N 50/70 Open

The open sub-zone allows more uses, additional signage and sometimes additional frontages, within the same form and character of the base zone.

Intent

A walkable neighborhood environment of large footprint, high-intensity mixed-use buildings, supporting and within short walking distance of neighborhood shopping, services, and transit.

Desired Form

Primarily Block-Form Buildings

Building Height 40' to 70', as per Regulating Plan

Primarily Attached Buildings

Small-to-No Front Setbacks

Small-to-No Side Setbacks

Residential and Shopfront Frontages

T5 Main Street



Zone Abbreviation

T5MS

Sub-Zone(s)

None

Intent

A walkable, urban neighborhood environment with large footprint, high-intensity mixed-use buildings in close proximity to the multimodal transit station, with neighborhood-serving shopping and services.

Desired Form

Primarily Block-Form Buildings

Building Height 70' to 90'

Attached Buildings

Small-to-No Front Setbacks

No Side Setbacks

Predominantly Shopfront Frontages

Note:

The standards for each zone begin on the following page.

2.3.030 T4 Neighborhood (T4N 30/40 and T4N 40/50)



A. Intent

A walkable neighborhood environment of small-to-medium footprint, moderate-intensity mixed-use buildings and housing choices, supporting and within short walking distance of neighborhood-serving retail and services. This zone provides a transition in scale between the Downtown and adjacent residential neighborhoods.

B. Sub-Zone(s)

T4N 40/50 Open. The open sub-zone allows more uses, additional signage and sometimes additional frontages, within the same form and character of the base zone.

The following are generally appropriate form elements in the zone.

- Primarily House-Form Buildings **A**
- Building Height 30' to 50', as per Regulating Plan **B**
- Primarily Detached Buildings **C**
- Small Front Setbacks **D**
- Small Side Setbacks **E**
- Porch Projecting, Porch Engaged, Dooryard, Stoop, Maker Shopfront, Shopfront, Terrace Frontage Types **F**

C. Building Placement	
Setback (Distance from ROW/ Lot Line)	
Front (Facade Zone)	7' min.; 15' max. G
Side Street (Facade Zone)	7' min.; 15' max. H
Total length of facade required within or abutting the Facade Zone, exclusive of setbacks ¹	
Front	70% min.
Side Street	50% min.
Side	5' min. I
Rear ²	15' min. J
Footprint	
Building Length	75' max.
Civic Space	
Site Size (sf) or Lot Width	Required Area (min.)
15,000 to 30,000 or 100'-150'	200 sf
> 30,000 or 150'-250'	1,000 sf
Lot Width >250'	5% of Site

¹ Facades facing a street or civic space shall be designed in compliance with DTFBC Division 3.2 (Massing and Facade Articulation Standards).

² See Sub-Section G (Adjacency Standards) of this Section for additional refinements.

D. Encroachments ¹				
Encroachment Type	Front	Side St.	Side	Rear
Frontage Types ²	A	A	X	A
Steps to Building Entry	5' max.	5' max.	3' max.	5' max.
Canopies/ Overdoors, Signage ³	3' max.	3' max.	2' max.	3' max.
Balconies, Unenclosed Porches, Bay Windows ³	5' max.	5' max.	2' max.	5' max.

Key A = Allowed X = Not Allowed



D. Encroachments ¹ (Continued)				
Encroachment Type	Front	Side St.	Side	Rear
Corner Elements	3' min. to 5' max.	n/a	n/a	n/a

¹ Includes encroachments into building setbacks and facade zone.

² See Sub-Section F (Frontages) of this Section for allowed types and standards.

³ May also encroach into required stepbacks.

Key

- ROW/ Lot Line
- Buildable Area
- Building Setback Line
- ▨ Facade Zone

E. Building Form and Maximum Envelope		
Height	Base	Height Bonus ¹
Overall Building Height^{2,3,4}		
T4N 30/40	30' max. 40' max.	K
T4N 40/50 and T4N 40/50 Open	40' max. 50' max.	K
Highest Top Plate/ Highest Eave Measurement		
T4N 30/40	25' max. 35' max.	L
T4N 40/50 and T4N 40/50 Open	35' max. 45' max.	L
Stepback⁵		
T4N 30/40		
Front	10' min. at 25'	M
Side Street	10' min. at 25'	N
Rear	10' min. at 25'	O
T4N 40/50 and T4N 40/50 Open		
Front	10' min. at 35'	M
Side Street	10' min. at 35'	N
Rear	10' min. at 35'	O
Ground Floor Standards		
Ground Floor Finish Level		P
Residential ⁶	12" min.	
Non-Residential ⁷	Flush with Sidewalk	
Ground Floor Ceiling	9' min.	Q
Depth, Ground-Floor Space	30' min. front; 12' min. other	R

¹ See SRMC Section 14.16.190 (Height Bonus) for requirements to qualify for height bonus.

² See DTFBC Figure 2.2.040.A (Regulating Plan) for further refinements.

³ Corner elements may exceed the maximum allowed height by up to 10' for a horizontal area up to 20' x 20'.

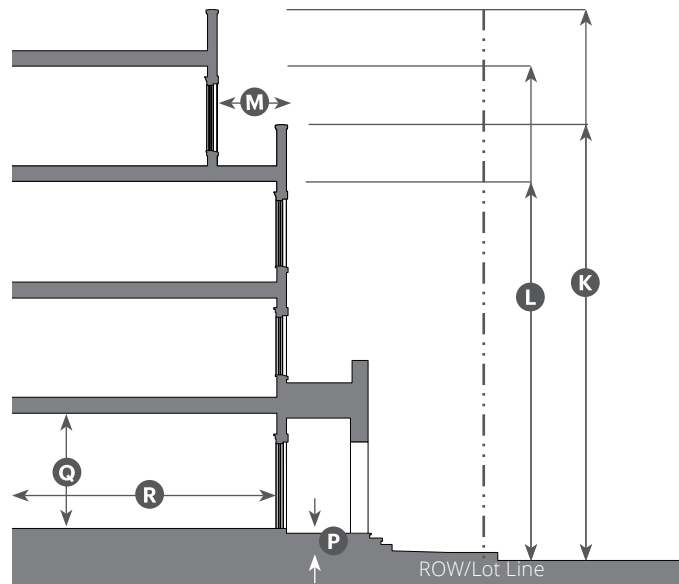
E. Building Form and Maximum Envelope (Continued)

⁴ See SRMC Section 14.16.120 (Exclusions to the Maximum Height Requirement) for exclusions to the overall building height.

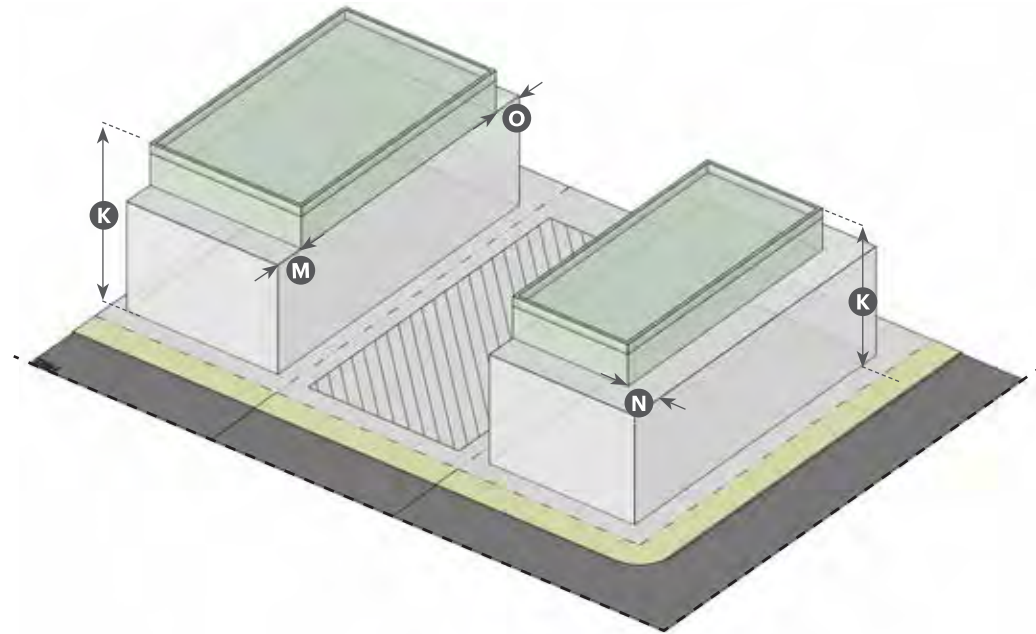
⁵ Stepback not required when the uppermost floor and up to two floors including the uppermost floor are within a mansard roof. See DTFBC Section 3.2.060 (Windows and Openings).

⁶ Common entries may be set at grade in compliance with local and federal accessibility standards.

⁷ Or as required to comply with FEMA base flood elevation.



Maximum Envelope in Mid-Block and Corner Condition



F. Frontages¹

Frontage Type	Front	Side St.	Standards
Porch Projecting	A	A	3.3.030
Porch Engaged	A	A	3.3.040
Dooryard	A	A	3.3.050
Stoop	A	A	3.3.060
Maker Shopfront	A ²	A ²	3.3.080
Shopfront	A ²	A ²	3.3.090
Terrace	A	A	3.3.100

¹ Minimum of one Frontage Type per street-facing facade.

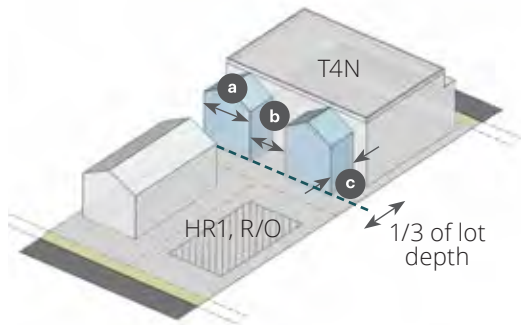
² Only in Open Sub-Zone.

Key A = Allowed

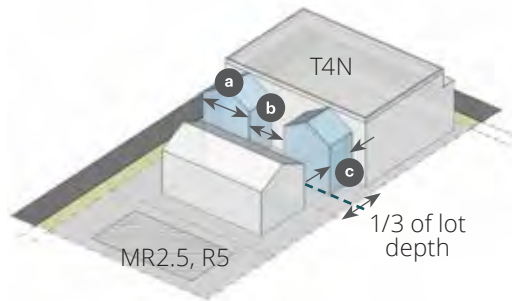
Key

- Max. Envelope without Height Bonus (Base)
- Max. Envelope with Height Bonus
- Street Centerline

Adjacent to HR1 and R/O



Adjacent to MR2.5 and R5



Key

- Main Body
- House-Form Massing
- Rear setback per Sub-Section C (Building Placement)
- a Wing Width
- b Wing Separation
- c Wing Depth

G. Adjacency Standards

Historic Resource Adjacency

Where adjacent to a designated historic resource refer to DTFBC Section 3.2.070 (Historic Resource Adjacency Standards) in addition to the standards of this zone.

House-Form Massing

Where abutting the MR2.5, R5, HR1 or R/O zones, house-form massing is required within rear 1/3 of the lot.

Building Width	Required Wing(s)	Wing Size (max.) ¹ a x c	Height
< 75'	None	-	-
> 75' - 100'	1 min.	25' x 35'	35'
>100' - 150'	2 min.;	25' x 35'	35'
	or 1 at	35' x 35'	
	and 1 at	35' x 65'	
>150'	3 min.;	25' x 35'	35'
	1 at	25' x 35'	
	and 2 at	35' x 65'	
Wing Separation: 10' min.			b

¹ Min. wing size is 15' by 15'.

H. Parking		
Required Spaces (min.) for New Buildings ¹		
Use Type	Vehicular Spaces	Bicycle Spaces
Residential Uses ^{2,3}		
Studio or 1 Bedroom	0.75	1.0
2 Bedrooms	1.0	2.0
3 or more Bedrooms	1.5	3.0
Non-Residential Uses (amount per tenant in building)		
≤ 2,500 sf	None	
> 2,500 sf	3 per 1,000 sf	

¹ For buildings located within the Downtown parking District, the off-street parking requirement is waived for up to 1.0 FAR of the total square footage. For buildings with square footage above 1.0 FAR and for all residential uses off-street parking shall be provided. Refer to SRMC Section 14.18.060 (Downtown Parking District).

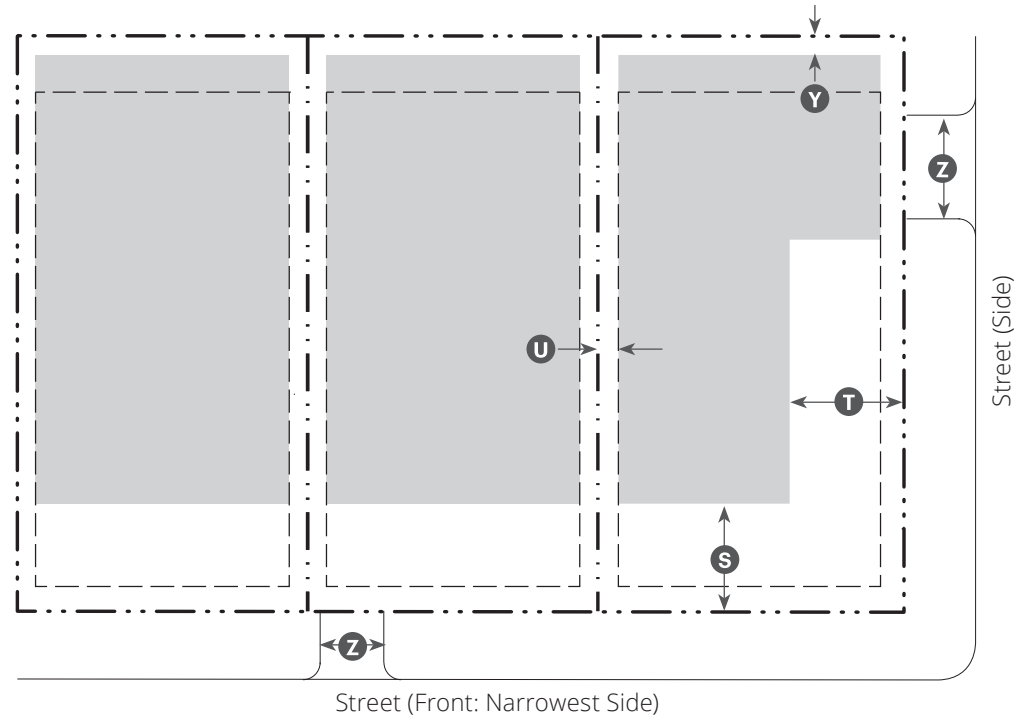
² In West End Village see SRMC Section 14.18.040 (Parking Requirements).

³ Guest parking not required except in West End Village where 1 parking space per 5 spaces is required.

Setback (Distance from ROW/ Lot Line) ¹		
Front	25' min.	S
Side Street	25' min. when enclosed within building, 5' min unenclosed ²	T
Side	5' min.	U
Rear	5' min.	Y

¹ 0' minimum setback for subterranean parking in compliance with frontage requirements.

² In compliance with facade zone requirements in Sub-Section C (Building Placement) of this Section.



H. Parking (Continued)

Driveway		
Driveway Width, within parking setbacks		Z
Front	12' max.	
Side Street/ Alley	20' max.	
Curb Cut	14' max.	
Planter (on each side)	2'	
Distance between Driveways	40' min.	

Curb Cut Width along alley may exceed 12'.
 Driveways may be shared between adjacent lots and may exceed max. allowed width by 3'.
 Driveways are not allowed in front on a corner lot.
 Refer SRMC Section 14.18.040 (Parking Requirements) for dimensions of parking spaces.

Key

- - - ROW/ Lot Line
- - - Building Setback Line
- Parking Area (at or above grade)

2.3.040 T4 Main Street (T4MS 40/50, T4MS 40/60, T4MS 50/70, and T4MS 60/80)



A. Intent

A walkable, vibrant district of medium-to-large footprint, moderate intensity, mixed-use buildings and housing choices, supporting neighborhood and community-serving ground floor shopping, food and services, including civic, institutional, maker/ craft/ artisanal businesses (both indoor and outdoor).

B. Sub-Zone(s)

T4MS 40/50 Open, T4MS 40/60 Open and T4MS 50/70 Open. The open sub-zone allows more uses, additional signage and sometimes additional frontages, within the same form and character of the base zone.

The following are generally appropriate form elements in the zone.

- Primarily Block-Form Buildings **A**
- Building Height 40' to 80', as per Regulating Plan **B**
- Attached Buildings **C**
- Small-to-No Front Setbacks **D**
- No Side Setbacks **E**
- Porch Projecting, Porch Engaged, Dooryard, Stoop, Forecourt, Maker Shopfront, Shopfront, Terrace, Gallery Frontage Types **F**

C. Building Placement

Setback (Distance from ROW/ Lot Line)

Front (Facade Zone)	0' min.; 10' max.	G
Side Street (Facade Zone)	0' min.; 10' max.	H

Total length of facade required within or abutting the Facade Zone, exclusive of setbacks¹

Front	80% min.
Side Street	70% min.

Side	0' min.	I
Rear ²	0' min.	J

Civic Space

Site Size (sf) or Lot Width Required Area (min.)

15,000 to 30,000 or 100' - 150'	200 sf
> 30,000 or 150' - 250'	1,000 sf
Lot Width >250'	5% of Site

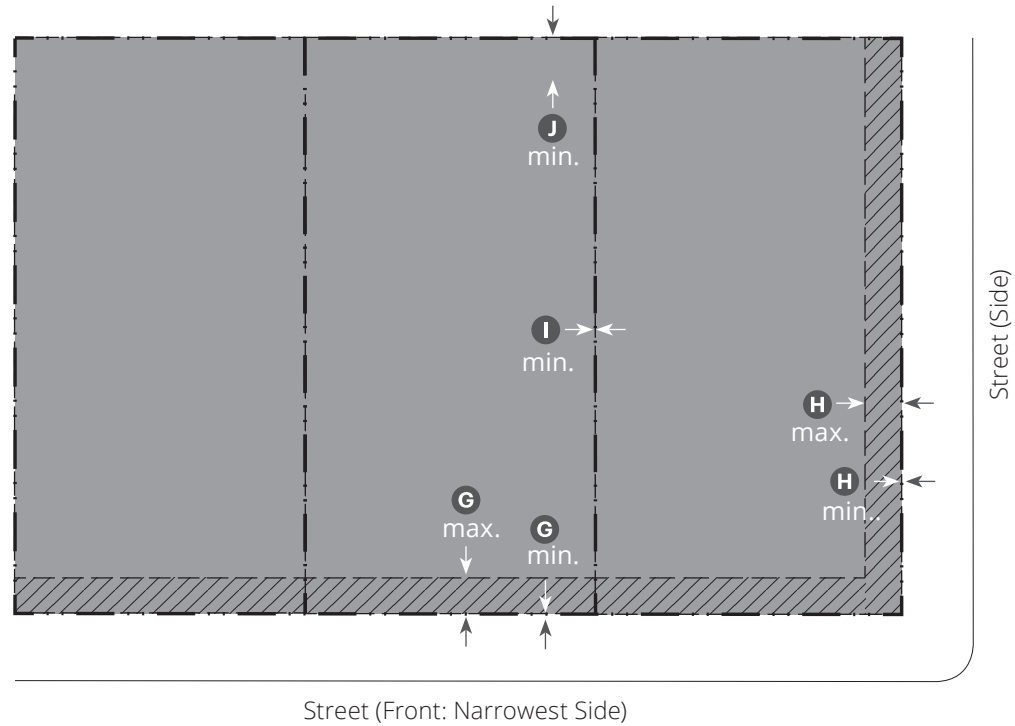
¹ Facades facing a street or civic space shall be designed in compliance with DTFBC Division 3.2 (Massing and Facade Articulation Standards).

² See Sub-Section G (Adjacency Standards) of this Section for additional refinements.

D. Encroachments¹

Encroachment Type	Front	Side St.	Side	Rear
Frontage Types ²	A	A	X	A
Steps to Building Entry ³	3' max.	3' max.	-	3' max.
Canopies/ Overdoors, Signage ⁴	4' max.	4' max.	2' max.	3' max.
Balconies, Unenclosed Porches, Bay Windows ⁴	4' max.	4' max.	-	5' max.
Corner Elements	3' min. to 4' max.	n/a	n/a	n/a

Key A = Allowed X = Not Allowed



D. Encroachments¹ (Continued)

¹ Includes encroachments into building setbacks and facade zone.

² See Sub-Section F (Frontages) of this Section for allowed types and standards.

³ Requires building setback.

⁴ May also encroach into required setbacks.

Key

- ROW/ Lot Line
- Buildable Area
- Building Setback Line
- ▨ Facade Zone

E. Building Form and Maximum Envelope		
Height	Base	Height Bonus ¹
Overall Building Height^{2,3,4}		
T4MS 40/50 and T4MS 40/50 Open	40' max. 50' max.	K
T4MS 40/60 and T4MS 40/60 Open	40' max. 60' max.	K
T4MS 50/70 and T4MS 50/70 Open	50' max. 70' max.	K
T4MS 60/80	60' max. 80' max.	K
Highest Top Plate/ Highest Eave Measurement		
T4MS 40/50 Open	35' max. 45' max.	L
T4MS 40/60 Open	35' max. 55' max.	L
T4MS 50/70 and T4MS 50/70 Open	45' max. 65' max.	L
T4MS 60/80	55' max. 75' max.	L
Stepback⁵		
T4MS 40/50 and T4MS 40/50 Open		
Front	10' min. at 35'	M
Side Street	10' min. at 35'	N
Rear	10' min. at 35'	O
T4MS 40/60 and T4MS 40/60 Open		
Front	10' min. at 35'	M
Side Street	10' min. at 35'	N
Rear	10' min. at 35'	O
T4MS 50/70 and T4MS 50/70 Open		
Front	10' min. at 45'	M
Side Street	10' min. at 45'	N
Rear	10' min. at 45'	O
T4MS 60/80⁶		
Front	10' min. at 55'	M
Side Street	10' min. at 55'	N
Rear	10' min. at 55'	O

E. Building Form and Maximum Envelope (Continued)		
Ground Floor Standards		
Ground Floor Finish Level		P
Residential ⁷	12" min.	
Non-Residential ⁸	Flush with Sidewalk	
Ground Floor Ceiling	14' min.	Q
Depth, Ground-Floor Space	30' min. front; 12' min. other	R

¹ See SRMC Section 14.16.190 (Height Bonus) for requirements to qualify for height bonus.

² See DTFBC Figure 2.2.040.A (Regulating Plan) for further refinements.

³ Corner elements may exceed the maximum allowed height by up to 10' for a horizontal area up to 20' x 20'.

⁴ See SRMC Section 14.16.120 (Exclusions to the Maximum Height Requirement) for exclusions to the overall building height.

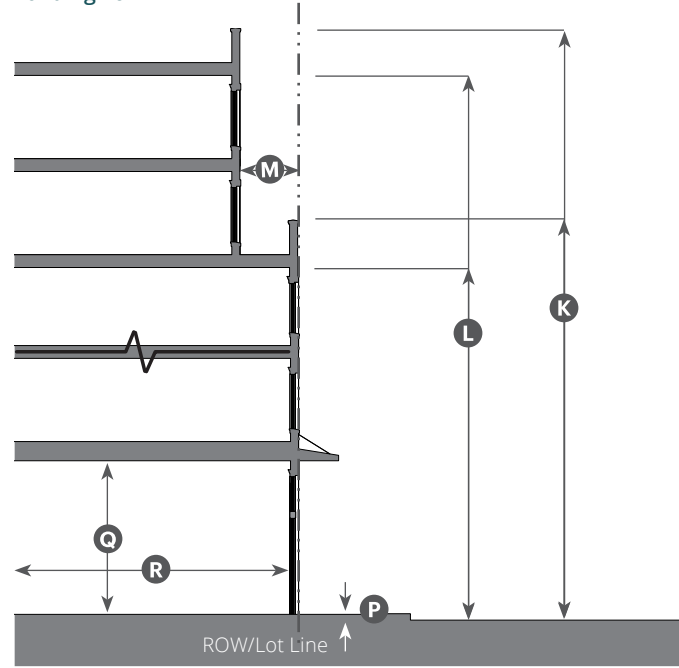
⁵ Stepback not required when the uppermost floor and up to two floors including the uppermost floor are within a mansard roof. See DTFBC Section 3.2.060 (Windows and Openings).

⁶ Buildings in T4MS 60/80 zone when located on the south side of the street and subject to a height bonus shall require a solar study to determine that the opposing sidewalk and/or public space shall not be more than 50% shaded at 12 noon on the Spring equinox (March 21st). Exemptions to this requirement may apply to affordable housing projects.

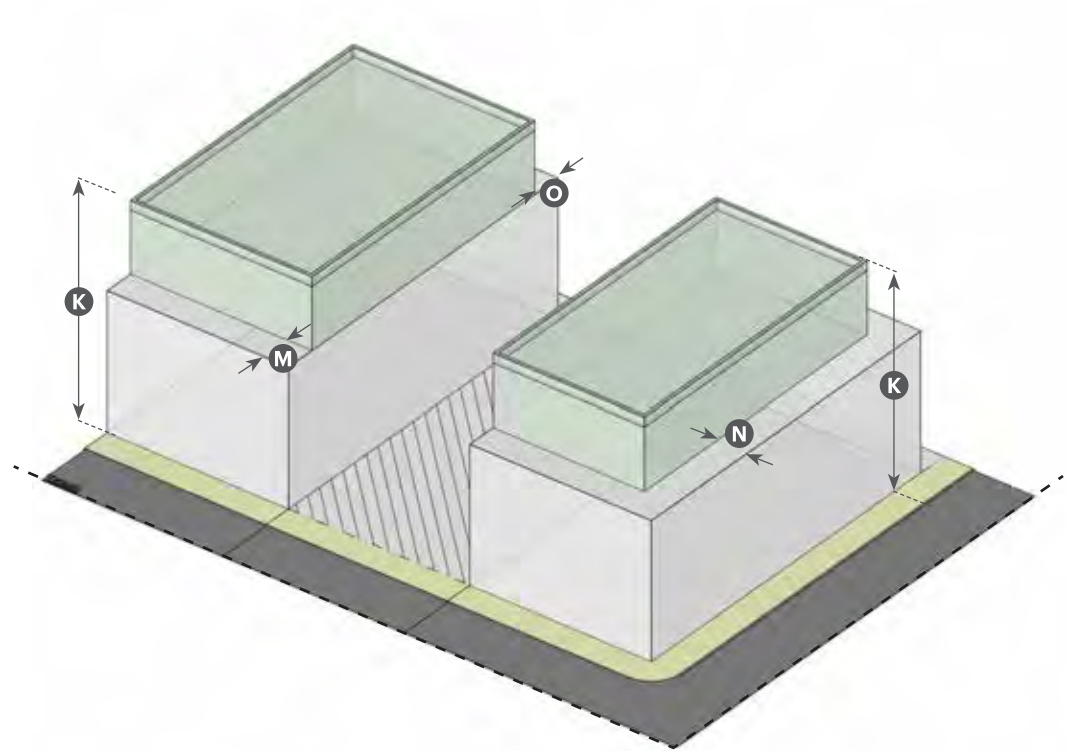
⁷ Common entries may be set at grade in compliance with local and federal accessibility standards.

⁸ Or as required to comply with FEMA base flood elevation.

Building Form



Maximum Envelope in Mid-Block and Corner Condition



F. Frontages¹

Frontage Type	Front	Side St.	Standards
Porch Projecting	A ²	A ²	3.3.030
Porch Engaged	A ²	A ²	3.3.040
Dooryard	A	A	3.3.050
Stoop	A ²	A ²	3.3.060
Forecourt	A	A	3.3.070
Maker Shopfront	A ²	A ²	3.3.080
Shopfront	A	A	3.3.090
Terrace	A	A	3.3.100
Gallery	A	A	3.3.110

¹ Minimum of one Frontage Type per street-facing facade.

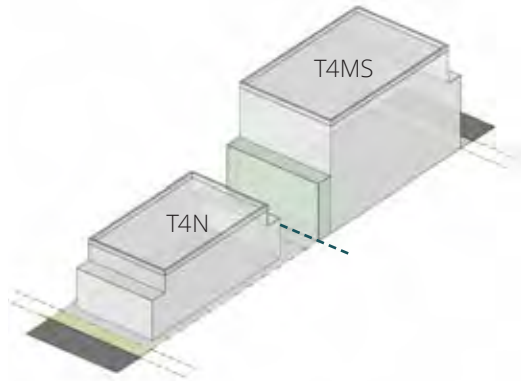
² Only in Open Sub-Zone.

Key A = Allowed

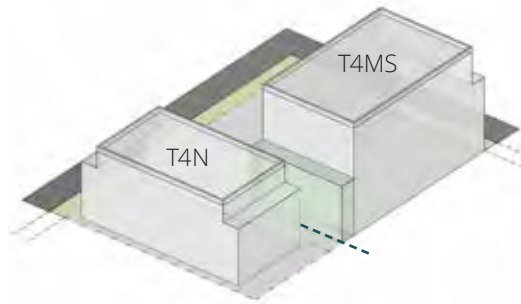
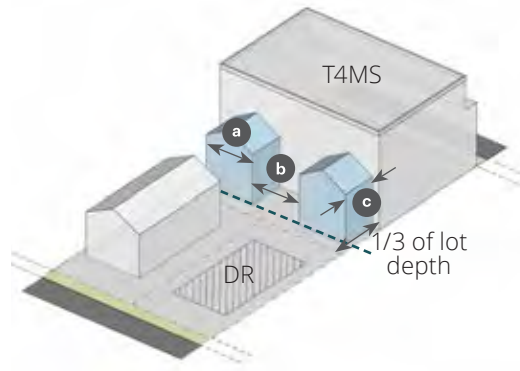
Key

- Max. Envelope without Height Bonus (Base)
- Max. Envelope with Height Bonus
- Street Centerline

Adjacent to T4N 30/40 and T4N 40/50



Adjacent to DR



Key

- Main Body
- Rear Stepback
- House-Form Massing
- Rear setback per Sub-Section C (Building Placement)
- a Wing Width
- b Wing Separation
- c Wing Depth

G. Adjacency Standards

Historic Resource Adjacency

Where adjacent to a designated historic resource refer to DTFBC Section 3.2.070 (Historic Resource Adjacency Standards) in addition to the standards of this zone.

Building Placement and Height

	Setback	Stepback
Open		
Rear	10' min.	10' min at 35'

House-Form Massing

Where abutting the DR zone, house-form massing is required within rear 1/3 of the lot.

Building Width	Required Wing(s)	Wing Size (max.) ¹ a x c	Height
< 75'	None	-	-
> 75' - 100'	1 min.	25' x 35'	35'
>100' - 150'	2 min.;	25' x 35'	35'
	2 at	25' x 35'	
	or 1 at	35' x 35'	
	and 1 at	35' x 65'	
>150'	3 min.;	25' x 35'	35'
	1 at	25' x 35'	
	and 2 at	35' x 65'	

Wing Separation: 10' min. b

¹Min. wing size is 15' by 15'.

H. Parking		
Required Spaces (min.) for New Buildings ¹		
Use Type	Vehicular Spaces	Bicycle Spaces
Residential Uses ^{2,3}		
Studio or 1 Bedroom	0.5	1.0
2 Bedrooms	1.0	2.0
3 or more Bedrooms	1.5	3.0
Non-Residential Uses (amount per tenant in building)		
≤ 6,000 sf	None	
> 6,000 sf	2.75 per 1,000 sf	

¹ For buildings located within the Downtown parking District, the off-street parking requirement is waived for up to 1.0 FAR of the total square footage. For buildings square footage above 1.0 FAR and for all residential uses off-street parking shall be provided. Refer to SRMC Section 14.18.060 (Downtown Parking District).

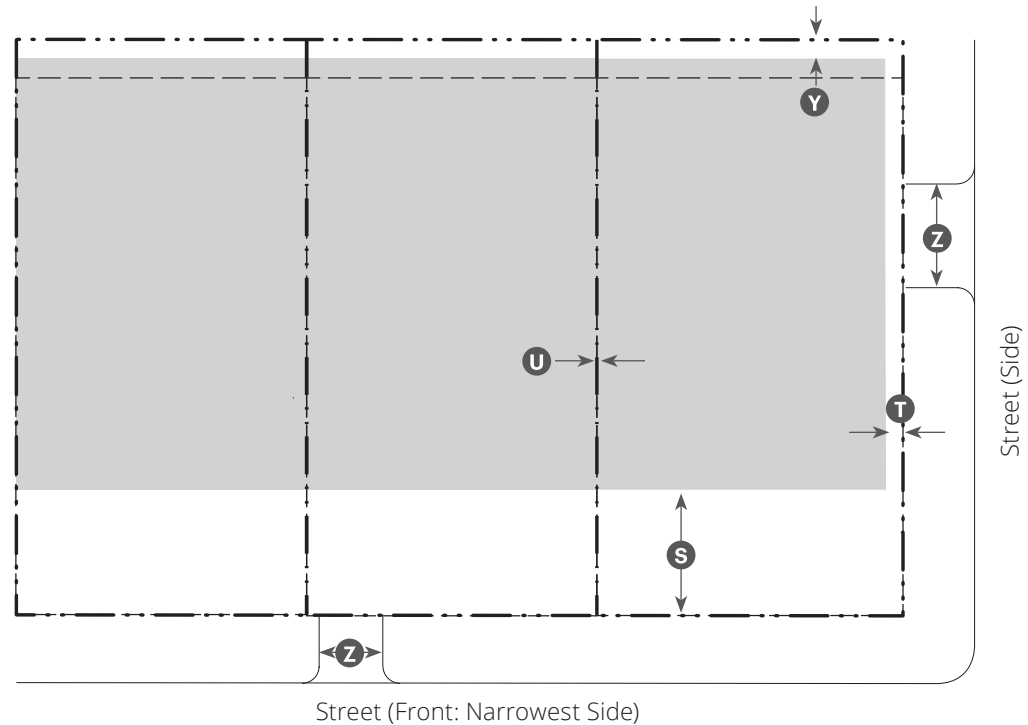
² In West End Village see DTFBC Section 14.18.040 (Parking Requirements).

³ Guest parking not required except in West End Village where 1 parking space per 5 spaces is required.

Setback (Distance from ROW/ Lot Line) ¹		
Front	35' min.	S
Side Street	25' min when enclosed within building, 5' min unenclosed ²	T
Side	0' min.	U
Rear	5' min.	Y

¹ 0' minimum setback for subterranean parking in compliance with frontage requirements.

² In compliance with facade zone requirements in Sub-Section C (Building Placement) of this Section.



H. Parking (Continued)

Driveway

Driveway Width, within parking setbacks	Z
Front	12' max.
Side Street/ Alley	20' max.
Curb Cut	14' max.
Planter (on each side)	2'
Distance between Driveways	40' min.

Curb Cut Width along alley may exceed 12'.

Driveways may be shared between adjacent lots and may exceed max. allowed width by 3'.

Driveways are not allowed in front on a corner lot.

Refer SRMC Section 14.18.040 (Parking Requirements) for dimensions of parking spaces.

Key

- ROW/ Lot Line
- Building Setback Line
- Parking Area (at or above grade)

2.3.050 T5 Neighborhood (T5N 40/60 and T5N 50/70)



A. Intent

A walkable neighborhood environment of large footprint, high-intensity mixed-use buildings, supporting and within short walking distance of neighborhood shopping, services, and transit.

B. Sub-Zone(s)

T5N 40/60 Open and T5N 50/70 Open. The open sub-zone allows more uses, additional signage and sometimes additional frontages, within the same form and character of the base zone.

The following are generally appropriate form elements in the zone.

- Primarily Block-Form Buildings A
- Building Height 40' to 70', as per Regulating Plan B
- Primarily Attached Buildings C
- Small-to-No Front Setbacks D
- No Side Setbacks E
- Porch Projecting, Porch Engaged, Dooryard, Stoop, Forecourt, Shopfront, Terrace Frontage Types F

C. Building Placement

Setback (Distance from ROW/ Lot Line)

Front (Facade Zone) ³	0' min.; 15' max.	G
Side Street (Facade Zone) ³	0' min.; 15' max.	H

Total length of facade required within or abutting the Facade Zone, exclusive of setbacks¹

Front	70% min.
Side Street	60% min.

Side ²	0' min.	I
Rear ²	0' min.	J

Civic Space

Site Size (sf) or Lot Width Required Area (min.)

15,000 to 30,000 or 100' - 150'	200 sf
> 30,000 or 150' - 250'	1,000 sf
Lot Width >250'	5% of Site

¹ Facades facing a street or civic space shall be designed in compliance with DTFBC Division 3.2 (Massing and Facade Articulation Standards).

² See Sub-Section G (Adjacency Standards) of this Section for additional refinements.

D. Encroachments¹

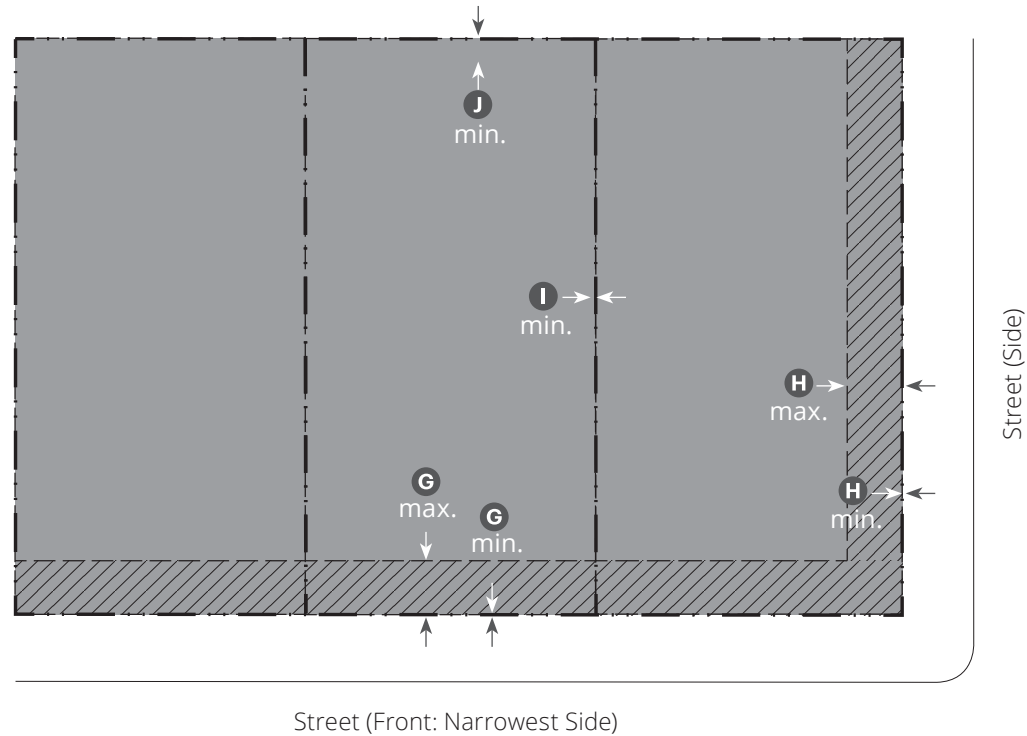
Encroachment Type	Front	Side St.	Side	Rear
Frontage Types ²	A	A	X	A
Steps to Building Entry ³	3' max.	3' max.	-	5' max.
Canopies/ Overdoors, Signage ⁴	4' max.	4' max.	2' max.	3' max.
Balconies, Bay Windows ⁴	4' max.	4' max.	-	5' max.
Corner Elements	3' min. to 4' max.	n/a	n/a	n/a

¹ Includes encroachments into building setbacks and facade zone.

² See Sub-Section F (Frontages) of this Section for allowed types and standards.

³ Requires building setback.

⁴ May also encroach into required setbacks.



Key

- ROW/ Lot Line
- Buildable Area
- Building Setback Line
- ▨ Facade Zone

Key A = Allowed X = Not Allowed

E. Building Form and Maximum Envelope		
Height	Base	Height Bonus ¹
Overall Building Height^{2,3,4}		
T5N 40/60 and T5N 40/60 Open	40' max. 60' max.	K
T5N 50/70 and T5N 50/70 Open	50' max. 70' max.	K
Highest Top Plate/ Highest Eave Measurement		
T5N 40/60 and T5N 40/60 Open	35' max. 55' max.	L
T5N 50/70 and T5N 50/70 Open	45' max. 65' max.	L
Stepback⁵		
T5N 40/60 and T5N 40/60 Open		
Front	10' min. at 35'	M
Side Street	10' min. at 35'	N
Rear	10' min. at 35'	O
T5N 50/70 and T5N 50/70 Open		
Front	10' min. at 45'	M
Side Street	10' min. at 45'	N
Rear	10' min. at 45'	O
Ground Floor Standards		
Ground Floor Finish Level		P
Residential ⁶	12" min.	
Non-Residential ⁷	Flush with Sidewalk	
Ground Floor Ceiling	10' min.	Q
Depth, Ground-Floor Space	30' min. front; 12' min. other	R

¹ See SRMC Section 14.16.190 (Height Bonus) for requirements to qualify for height bonus.

² See DTFBC Figure 2.2.040.A (Regulating Plan) for further refinements.

³ Corner elements may exceed the maximum allowed height by up to 10' for a horizontal area up to 20' x 20'.

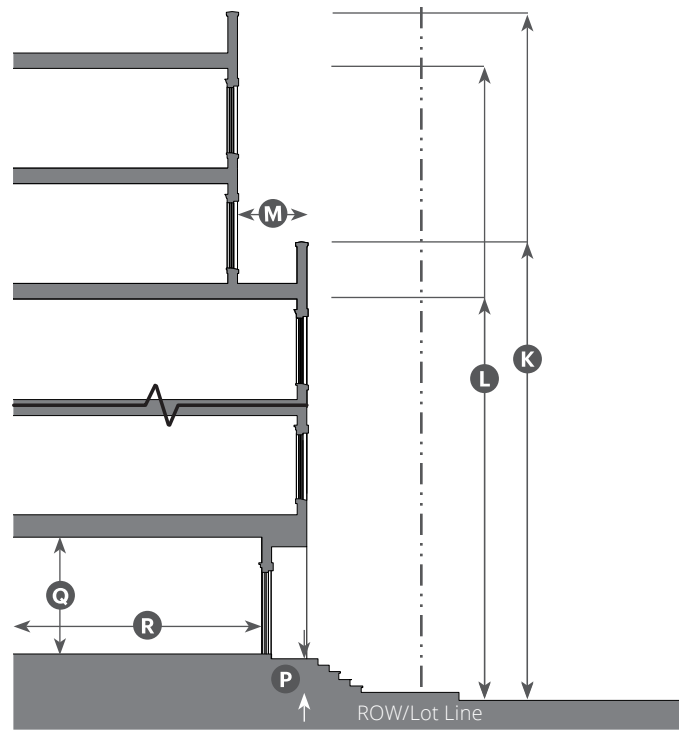
E. Building Form and Maximum Envelope (Continued)

⁴ See SRMC Section 14.16.120 (Exclusions to the Maximum Height Requirement) for exclusions to overall building height.

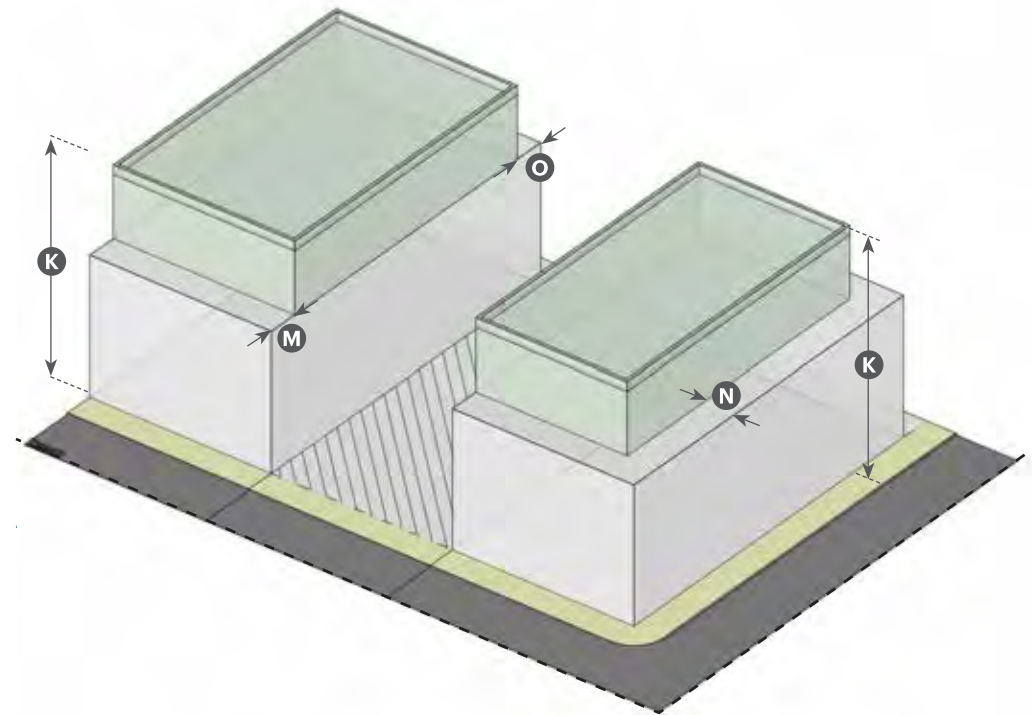
⁵ Stepback not required when the uppermost floor and up to two floors including the uppermost floor are within a mansard roof. See DTFBC Section 3.2.060 (Windows and Openings).

⁶ Common entries may be set at grade in compliance with local and federal accessibility standards.

⁷ Or as required to comply with FEMA base flood elevation.



Maximum Envelope in Mid-Block and Corner Condition



F. Frontages¹

Frontage Type	Front	Side St.	Standards
Porch Projecting	A	A	3.3.030
Porch Engaged	A	A	3.3.040
Dooryard	A	A	3.3.050
Stoop	A	A	3.3.060
Forecourt	A	A	3.3.070
Shopfront	A ²	A ²	3.3.090
Terrace	A	A	3.3.100

¹ Minimum of one Frontage Type per street-facing facade.

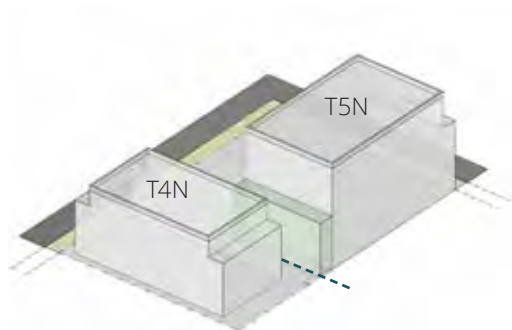
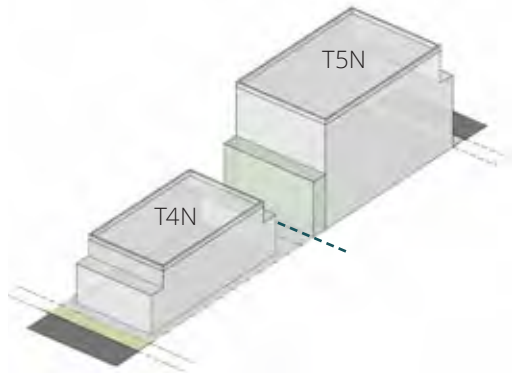
² Only in Open Sub-Zone.

Key A = Allowed

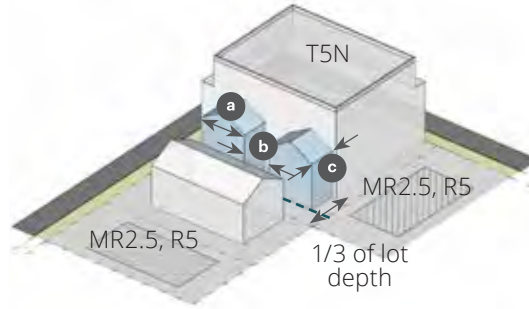
Key

- Max. Envelope without Height Bonus (Base)
- Max. Envelope with Height Bonus
- Street Centerline

Adjacent to T4N 30/40 and T4N 40/50



Adjacent to MR2.5 and R5



G. Adjacency Standards

Historic Resource Adjacency

Where adjacent to a designated historic resource refer to DTFBC Section 3.2.070 (Historic Resource Adjacency Standards) in addition to the standards of this zone.

Building Placement and Height

	Setback	Stepback
Open		
Rear	10' min.	10' min at 35'

Where abutting the MR2.5 or R5

Side	10' min.
------	----------

House-Form Massing

Where abutting the MR2.5 or R5 zones, house-form massing is required within rear 1/3 of the lot.

Building Width	Required Wing(s)	Wing Size (max.) ¹ a x c	Height
< 75'	None	-	-
> 75' - 100'	1 min.	25' x 35'	35'
>100' - 150'	2 min.;	25' x 35'	35'
	or 1 at	35' x 35'	
	and 1 at	35' x 65'	
>150'	3 min.;	25' x 35'	35'
	and 2 at	35' x 65'	

Wing Separation: 10' min.



¹Min. wing size is 15' by 15'.

Key

- Main Body
- Rear Stepback
- House-Form Massing
- Rear setback per Sub-Section C (Building Placement)
- a Wing Width
- b Wing Separation
- c Wing Depth

H. Parking		
Required Spaces (min.) for New Buildings ¹		
Use Type	Vehicular Spaces	Bicycle Spaces
Residential Use ^{2,3}		
Studio or 1 Bedroom	0.75	1.0
2 Bedrooms	1.0	2.0
3 or more Bedrooms	1.5	3.0
Non-Residential Uses (amount per tenant in building)		
≤ 2,500 sf	None	
> 2,500 sf	2.75 per 1,000 sf	

¹ For buildings located within the Downtown parking District, the off-street parking requirement is waived for up to 1.0 FAR of the total square footage. For buildings square footage above 1.0 FAR and for all residential uses off-street parking shall be provided. Refer to SRMC Section 14.18.060 (Downtown Parking District).

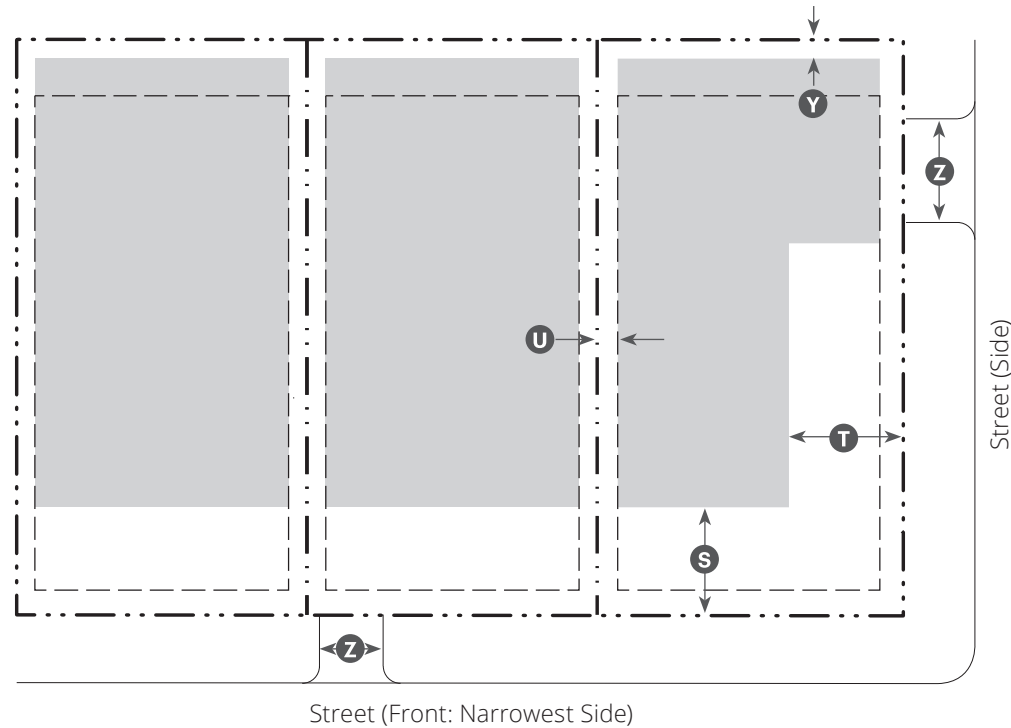
² In West End Village see SRMC Section 14.18.040 (Parking Requirements).

³ Guest parking not required except in West End Village where 1 parking space per 5 spaces is required.

Setback (Distance from ROW/ Lot Line) ¹		
Front	30' min.	S
Side Street	25' min when enclosed within building, 5' min unenclosed ²	T
Side	5' min.	U
Rear	5' min.	Y

¹ 0' minimum setback for subterranean parking in compliance with frontage requirements.

² In compliance with facade zone requirements in Sub-Section C (Building Placement) of this Section.



H. Parking (Continued)

Driveway

Driveway Width, within parking setbacks	Z
Front	12' max.
Side Street/ Alley	20' max.
Curb Cut	14' max.
Planter (on each side)	2'
Distance between Driveways	40' min.

Curb Cut Width along alley may exceed 12'.

Driveways may be shared between adjacent lots and may exceed max. allowed width by 3'.

Driveways are not allowed in front on a corner lot.

Refer SRMC Section 14.18.040 (Parking Requirements) for dimensions of parking spaces.

Key

- - - ROW/Lot Line
- - - Building Setback Line
- Parking Area (at or above grade)

2.3.060 T5 Main Street (T5MS 70/90)



A. Intent

A walkable, urban neighborhood environment with large footprint, high-intensity mixed-use buildings in close proximity to the multimodal transit station, with neighborhood-serving shopping and services.

B. Sub-Zone(s)

None

The following are generally appropriate form elements in the zone.

- Primarily Block-Form Buildings A
- Building Height 70' to 90' B
- Attached Buildings C
- Small-to-No Front Setbacks D
- No Side Setbacks E
- Forecourt, Shopfront, Terrace, Gallery Frontage Types F

C. Building Placement

Setback (Distance from ROW/ Lot Line)

Front (Facade Zone)	0' min.; 5' max.	G
Side Street (Facade Zone)	0' min.; 5' max.	H

Total length of facade required within or abutting the Facade Zone, exclusive of setbacks¹

Front	90% min.
Side Street	80% min.

Side	0' min.	I
Rear ²	0' min.	J

Civic Space

Site Size (sf) or Lot Width Required Area (min.)

15,000 to 30,000 or 100' - 150'	200 sf
> 30,000 or 150' - 250'	1,000 sf
Lot Width >250'	5% of Site

¹ Facades facing a street or civic space shall be designed in compliance with DTFBC Division 3.2 (Massing and Facade Articulation Standards).

² See Sub-Section G (Adjacency Standards) of this Section for additional refinements.

D. Encroachments¹

Encroachment Type Front Side St. Side Rear

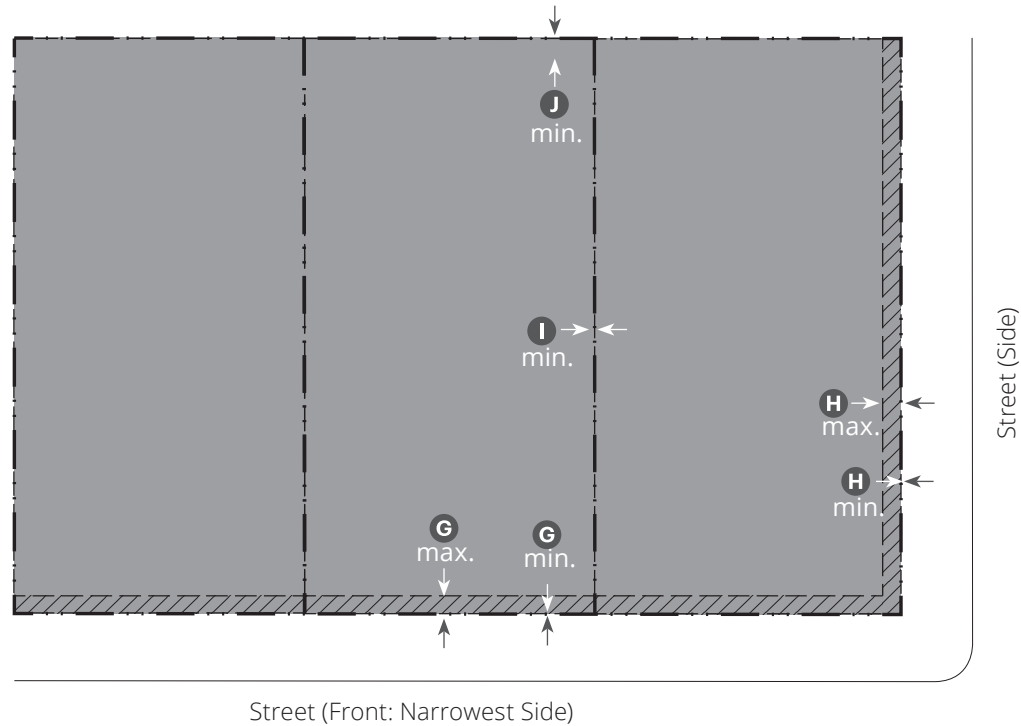
Frontage Types ²	A	A	X	A
Steps to Building Entry ³	3' max.	3' max.	-	5' max.
Canopies/ Overdoors, Signage ⁴	4' max.	4' max.	2' max.	3' max.
Balconies, Bay Windows ⁴	4' max.	4' max.	-	5' max.
Corner Elements	3' min. to 4' max.	n/a	n/a	n/a

¹ Includes encroachments into building setbacks and facade zone.

² See Sub-Section F (Frontages) of this Section for allowed types and standards.

³ Requires building setback.

⁴ May also encroach into required setbacks.



Key

- ROW/ Lot Line
- Buildable Area
- Building Setback Line
- ▨ Facade Zone

Key

- A = Allowed
- X = Not Allowed

E. Building Form and Maximum Envelope		
Height	Base	Height Bonus ¹
Overall Building Height^{2,3,4}		
T5MS 70/90	70' max. 90' max.	K
Highest Top Plate/ Highest Eave Measurement		
T5MS 70/90	65' max. 85' max.	L
Stepback^{5,6}		
T5MS 70/90		
Front	10' min. at 65'	M
Side Street	10' min. at 65'	N
Rear	10' min. at 65'	O
Ground Floor Standards		
Ground Floor Finish Level		P
Residential ⁷	12" min.	
Non-Residential ⁸	Flush with Sidewalk	
Ground Floor Ceiling	14' min.	Q
Depth, Ground-Floor Space	30' min. front; 12' min. other	R

¹ See SRMC Section 14.16.190 (Height Bonus) for requirements to qualify for height bonus.

² See DTFBC Figure 2.2.040.A (Regulating Plan) for further refinements.

³ Corner elements may exceed the maximum allowed height by up to 10' for a horizontal area up to 20' x 20'.

⁴ See SRMC Section 14.16.120 (Exclusions to the Maximum Height Requirement) for exclusions to overall building height.

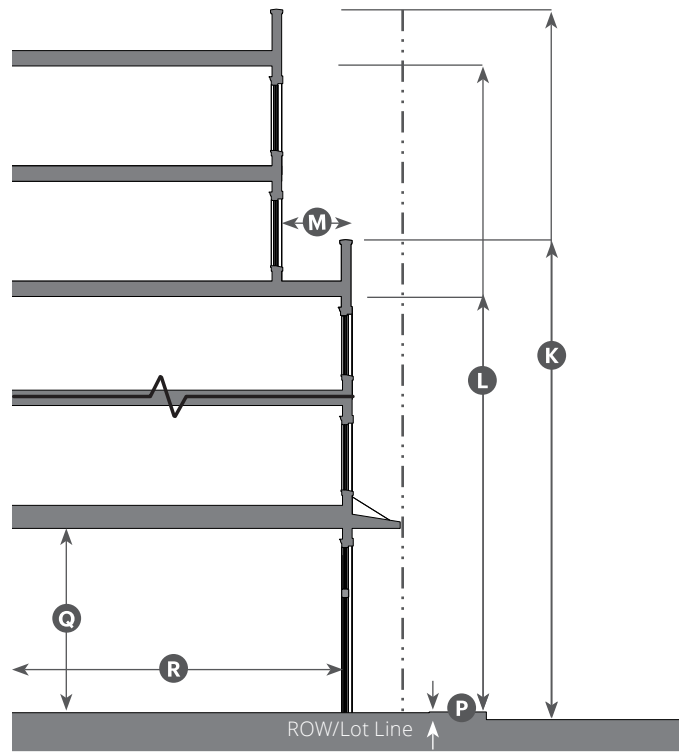
⁵ Stepback not required when the uppermost floor and up to two floors including the uppermost floor are within a mansard roof. See DTFBC Section 3.2.060 (Windows and Openings).

E. Building Form and Maximum Envelope (Continued)

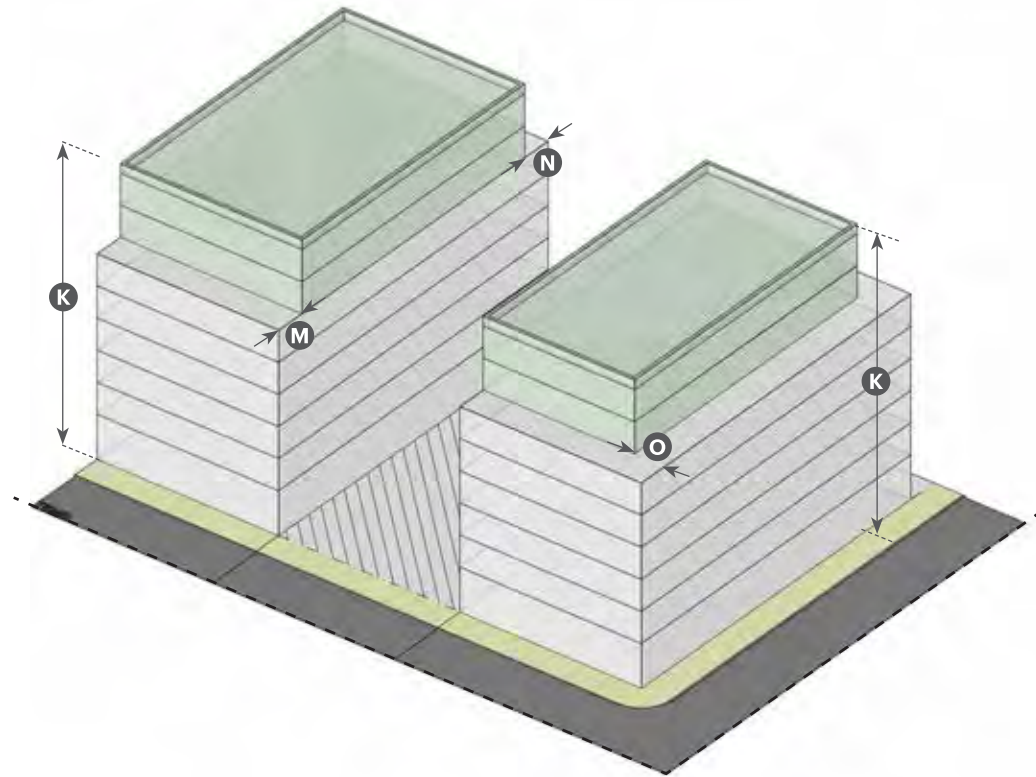
⁶ Buildings in T5MS 70/90 zone when subject to a height bonus shall require a solar study to determine that the opposing sidewalk and/or public space shall not be more than 50% shaded at 12 noon on the Spring equinox (March 21st). Exemptions to this requirement may apply to affordable housing projects.

⁷ Common entries may be set at grade in compliance with local and federal accessibility standards.

⁸ Or as required to comply with FEMA base flood elevation.



Maximum Envelope in Mid-Block and Corner Condition



F. Frontages¹

Frontage Type	Front	Side St.	Standards
Forecourt	A	A	3.3.070
Shopfront	A	A	3.3.090
Terrace	A	A	3.3.100
Gallery	A	A	3.3.110

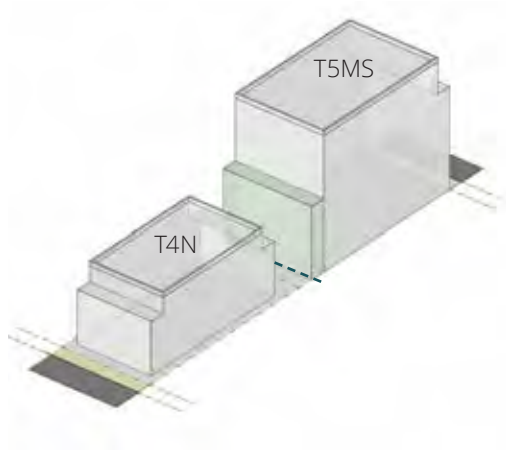
¹ Minimum of one Frontage Type per street-facing facade.

Key A = Allowed

Key

- Max. Envelope without Height Bonus (Base)
- Max. Envelope with Height Bonus
- Street Centerline

Adjacent to T4N 40/50 and T5N 50/70



Key

- Main Body
- Rear Stepback
- Rear setback per Sub-Section C (Building Placement)

G. Adjacency Standards

Historic Resource Adjacency

Where adjacent to a designated historic resource refer to DTFBC Section 3.2.070 (Historic Resource Adjacency Standards) in addition to the standards of this zone.

Building Placement and Height

	Setback	Stepback
Where abutting the T4N 40/50		
Rear	10' min.	10' min at 45'

H. Parking		
Required Spaces (min.) for New Buildings ¹		
Use Type	Vehicular Spaces	Bicycle Spaces
Residential Uses		
Studio or 1 Bedroom	0.50	1.0
2 Bedrooms	1.0	2.0
3 or more Bedrooms	1.50	3.0
Non-Residential Uses (amount per tenant in building)		
≤ 2,500 sf	None	
> 2,500 sf	2.75 per 1,000 sf	

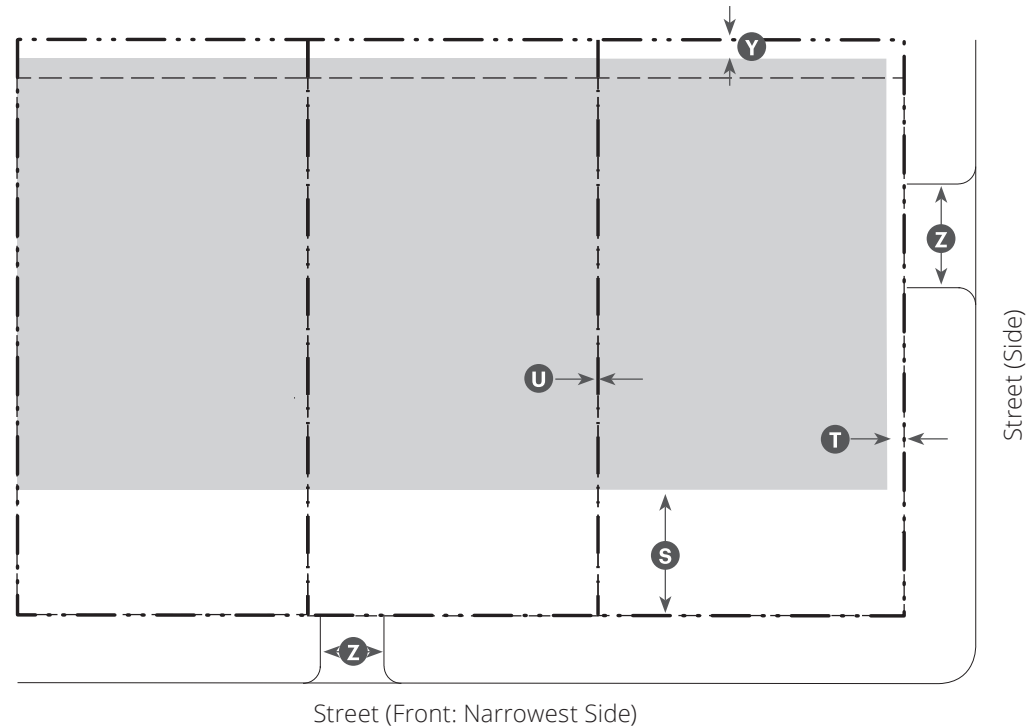
¹ For buildings located within the Downtown parking District, the off-street parking requirement is waived for up to 1.0 FAR of the total square footage. For buildings square footage above 1.0 FAR and for all residential uses off-street parking shall be provided. Refer to SRMC Section 14.18.060 (Downtown Parking District).

Setback (Distance from ROW/ Lot Line) ¹		
Front	35' min.	S
Side Street	25' min. when enclosed within building, 5' min. unenclosed ²	T
Side	0' min.	U
Rear	5' min.	Y

¹ 0' minimum setback for subterranean parking in compliance with frontage requirements.

² In compliance with facade zone requirements in Sub-Section C (Building Placement) of this Section.

Driveway		
Driveway Width, within parking setbacks Z		
Front	12' max.	
Side Street/ Alley	20' max.	
Curb Cut	14' max.	



H. Parking (Continued)	
Driveway (Continued)	
Planter (on each side)	2'
Distance between Driveways	40' min.

Curb Cut Width along alley may exceed 12'.
Driveways may be shared between adjacent lots and may exceed max. allowed width by 3'.
Driveways are not allowed in front on a corner lot.
Refer SRMC Section 14.18.040 (Parking Requirements) for dimensions of parking spaces.

Key	
----	ROW/Lot Line
---	Building Setback Line
■	Parking Area (at or above grade)

2.3.070 Use Table

The following table identifies the allowed uses in each zone. All uses are required to be in compliance with frontage of this Chapter and signage standards of SRMC Chapter 14.19 (Signs). The identified permit below is for the use only and shall be processed per City procedures for uses. Individual developments require additional review and approval as identified in DTFBC Table 1.1.050.A (Permit Requirements).

Uses Not Listed. Uses not listed are not allowed unless the Director determines that the proposed use is similar to a listed use.

Existing Buildings. Existing buildings may be occupied by new uses, allowed in the zone, up to the size of the prior use without requiring additional parking.

Table 2.3.070.A Use Table	Form-Based Zones						
	T4				T5		
	N	N-O	MS	MS-O	N	N-O	MS
Artisanal							
Coffee Roaster	CZ ²	A ²	CZ	A ²	CZ ²	A ²	CZ
Tobacco Retailer ⁴	C ²	CZ ²	C	CZ	C ²	CZ ²	C
Automotive							
Motor Vehicle Sales and Services, including automobiles, motorcycles, trailers, trucks and recreational vehicles							
Auto Detailing	CZ ²	A ²	-	-	CZ ²	A ²	-
Coin-op Washing	C	CZ	-	-	C	CZ	-
Fueling Station ⁵	C	C	-	-	C	C	-
Rentals, Vehicle	CZ ²	A ²	-	-	CZ ²	A ²	-
Repairs Major	C ¹	C ¹	-	-	C ¹	C ¹	-
Repairs, Minor ⁶	CZ ^{1,2}	A ^{1,2}	-	-	CZ ^{1,2}	A ^{1,2}	-
Sales, new or used vehicles	C	CZ	-	-	C	CZ	-
Sales, parts and supplies	P ²	P ²	P	P	P ²	P ²	P
Sales, tires and ancillary service	CZ	A	-	-	CZ	A	-

Key P = Permitted by Right A = Administrative Use Permit CZ = Conditional Use Permit/ Zoning Administrator C = Conditional Permit/ Planning Commission - = Not Allowed

Table 2.3.070.A Use Table (Continued)	Form-Based Zones						
	T4				T5		
	N	N-O	MS	MS-O	N	N-O	MS
Commercial							
Animal Retail Sales, excluding exterior kennel, pen or run ⁷	P ²	P ²	P	P	P ²	P ²	P
Artisan/ Craft Production	P ²	P ²	P	P	P ²	P ²	P
Building Materials and Supplies ⁸	P ²	P ²	P	P	P ²	P ²	P
Convenience Market ⁹	CZ	A	CZ	A	CZ	A	CZ
Grocery Store and Supermarket ⁹	P	P	P	P	P	P	P
Gun Shop	-	-	-	-	-	-	-
Kiosk ¹⁰	A ²	P ²	A	P	A ²	P ²	A
Liquor Store	P ²	P ²	P	P	P ²	P ²	P
Retail							
Retail General ^{9,11}	P ²	P ²	P	P	P ²	P ²	P
Curbside pick up	P	P	P	P	P	P	P
Drive-thru service ²⁴	-	-	-	-	-	-	-
Secondhand Store, Pawnshop ¹²	CZ ²	A ²	CZ	A	CZ ²	A ²	CZ
Entertainment							
Bar/ Cocktail Lounge/ Nightclub	CZ ²	A ²	CZ	A	CZ ²	A ²	CZ
Recreational Facility, indoors or outdoors							
Bowling Alley ¹³	-	-	-	-	C	CZ	-
Game Center ¹⁴	-	-	C ¹	CZ ¹	C ¹	CZ ¹	-
Poolhalls/ Billiard, Theater ¹⁵	P	P	P	P	P	P	P
Food							
Indoor Eating Area							
Food Service Establishment	P	P	P	P	P	P	P
Food Service Establishments with Alcohol Sales	A	A	P	P	A	A	P
Key	P = Permitted by Right	A = Administrative Use Permit	CZ = Conditional Use Permit/ Zoning Administrator	C = Conditional Permit/ Planning Commission	- = Not Allowed		

	Form-Based Zones							
	T4				T5			
	N	N-O	MS	MS-O	N	N-O	MS	
Food (Continued)								
Curbside pick up	P	P	P	P	P	P	P	
Drive-thru service ^{16,24}	-	-	-	-	-	-	-	
Outdoor Eating Area ^{17,1}	A ¹	P ¹	P ¹	P ¹	A ¹	P ¹	P ¹	
Lodging								
Bed and breakfast inn	C	CZ	C	CZ	C	CZ	C	
Hotel/ Motel	C	CZ	C	CZ	C	CZ	C	
Medical Office								
Medical Service	p ²	p ²	p ³	p ³	p ²	p ²	p ³	
Office								
Laboratory	CZ	A	CZ ³	A ³	CZ	A	CZ ³	
Research and Development	CZ	A	CZ ³	A ³	CZ	A	CZ ³	
Office, General	p ^{1,2}	p ^{1,2}	p ^{1,3}	p ^{1,3}	p ^{1,2}	p ^{1,2}	p ^{1,3}	
Services								
Animal Care								
Animal Hospital	C	CZ	C ³	CZ ³	C	CZ	C ³	
Indoor/Outdoor kennels	C	CZ	C ³	CZ ³	C	CZ	C ³	
Veterinary Clinic	CZ	A	CZ ³	A ³	CZ	A	CZ ³	
Business Sales and Services	p ²	p ²	P	P	p ²	p ²	P	
Catering Establishment	P	P	CZ ³	A ³	P	P	CZ ³	
Financial Service and Institution	p ¹	p ¹	p ^{1,3}	p ^{1,3}	p ¹	p ¹	p ^{1,3}	
Hospital/Major Medical Facility	C	CZ	-	-	C	CZ	-	
Outdoor Storage, including temporary or permanent storage containers	CZ	A	-	-	CZ	A	-	
Key	P = Permitted by Right		A = Administrative Use Permit		CZ = Conditional Use Permit/ Zoning Administrator		C = Conditional Permit/ Planning Commission	- = Not Allowed

	Form-Based Zones							
	T4				T5			
	N	N-O	MS	MS-O	N	N-O	MS	
Services, Personal								
Day Care Center (14 or more children or adults)	CZ	A	CZ	A	CZ	A	CZ	
Dry Cleaning Establishment, with on-site processing	CZ	A	CZ	A	CZ	A	CZ	
Family Day Care								
14 children or less, if within residence	P	P	P	P	P	P	P	
Small Day Care Facility, 14 children or less not in a residence	P	P	P	P	P	P	P	
Large Day Care Facility, over 14 children	CZ ¹	A ¹	CZ ¹	A ¹	CZ ¹	A ¹	CZ ¹	
Fitness/ Recreation Facility								
Less than 2,500 sf	P	P	P	P	P	P	P	
2,500 sf or more	A	A	A	A	A	A	A	
Fortunetelling ¹⁸	-	-	A ^{1,3}	A ^{1,3}	A ¹	A ¹	A ^{1,3}	
Funeral and Interment Services, including mortuary, but excluding crematory	CZ	A	-	-	CZ	A	-	
Music Rehearsal/ Recording Studio	CZ	A	CZ ³	A ³	CZ	A	CZ ³	
Personal Service ^{9,19}	P	P	P	P	P	P	P	
Personal Service, Drive-thru	-	-	-	-	-	-	-	
School								
Parochial, Private	C	CZ ²	C	CZ ²	C	CZ ²	C	
Public	P	P	P	P	P	P	P	
Specialized Education and Training								
Less than 2,000sf	P	P	P	P	P	P	P	
2,000sf or more	CZ	A	CZ	A	CZ	A	CZ	
Key	P = Permitted by Right		A = Administrative Use Permit		CZ = Conditional Use Permit/ Zoning Administrator		C = Conditional Permit/ Planning Commission	- = Not Allowed

	Form-Based Zones						
	T4				T5		
	N	N-O	MS	MS-O	N	N-O	MS
Public Assembly							
Religious Institution (church, temple, mosque, place of worship)	C	CZ ²	C	CZ ²	C	CZ ²	C
Public and Quasi-Public							
Community Garden	P ¹	P ¹	-	-	P ¹	P ¹	-
Club/ Lodge, including youth group	C	CZ	P	P	C	CZ	P
Public Park, Playground, and Recreational Facility	P	P	P	P	P	P	P
Public and Utility Facility	C	CZ	C	CZ	C	CZ	C
Public Facility, Other (Police, Fire Department, Paramedic, Post Office, etc.)	C	CZ	C	CZ	C	CZ	C
Residential							
Accessory Dwelling Unit (ADU)	P	P	P	P	P	P	P
Animal Keeping	A ¹	A ¹	A ^{1,3}	A ^{1,3}	A ¹	A ¹	A ^{1,3}
Boarding House	A	A	A ^{1,3}	A ^{1,3}	A	A	A ^{1,3}
Caretaker's Residence	A	A	A ^{1,3}	A ^{1,3}	A	A	A ^{1,3}
Emergency Shelter for the Homeless							
Temporary	C	CZ	C	CZ	C	CZ	C
Home Occupation ²⁰	P	P	P ¹	P ¹	P	P	P ¹
Live/ Work ²¹	P	P	P	P ¹	P	P	P
Residential ²¹	P	P	P ³	P ³	P	P	P ³
Residential Care Facility for the disabled in dwelling unit ²¹	P	P	P	P	P	P	P
Residential Care Facility, Other							
Small (0—6 residents)	P	P	P	P	P	P	P
Large (7 or more residents)	C	CZ	C	CZ	C	CZ	C
Key	P = Permitted by Right	A = Administrative Use Permit	CZ = Conditional Use Permit/ Zoning Administrator	C = Conditional Permit/ Planning Commission	- = Not Allowed		

	Form-Based Zones						
	T4				T5		
	N	N-O	MS	MS-O	N	N-O	MS
Temporary Use							
Temporary Use ²²	A ¹	A ¹	A ¹	A ¹	A ¹	A ¹	A ¹
Transportation							
Bus Station	-	-	-	-	C	CZ	C
"Park and Ride" Facility	CZ	A	-	-	CZ	A	CZ
Parking Facility, commercial or municipal	CZ	A	CZ	A	CZ	A	CZ
Rideshare Station ²³	C	CZ	-	-	C	CZ	C
Transit Station, public or transitway	C	CZ	-	-	C	CZ	C
Other Uses							
Wireless Telecommunication Facilities ²⁵	A	A	A	A	A	A	A

¹ See SRMC Chapter 14.21 (Administrative Use Permits) Performance Standards for additional requirements.

² Max. 2,500 sf per building.

³ Not allowed on the ground floor within 30' of the sidewalk. Allowed on ground floor along side street if at least 15' from the front of the lot.

⁴ Shall not be located within one thousand feet (1,000') from: schools (public and private elementary, junior high, and high schools), public parks, public libraries, arcades, youth/teen centers, community/recreation centers, licensed day care centers for children, shopping malls, and houses of worship with organized youth programs, as measured from the property lines of each parcel.

⁵ See SRMC Section 14.16.160 (Gasoline Stations)

⁶ See SRMC Section 14.17.090 (Motor Vehicle Repair Uses)

⁷ See SRMC Chapter 10.24 (Dog Kennels and Pet Shops)

⁸ See SRMC Section 14.17.120 (Outdoor Storage)

⁹ Operating after eleven p.m. (11:00 p.m.) requires a use permit (CZ)

¹⁰ See SRMC Section 14.16.115 (Emergency Shelters - Permanent)

¹¹ See SRMC Chapter 10.16 (Auctions)

¹² See SRMC Chapter 10.20 (Pawnbrokers and Secondhand Dealers)

Key P = Permitted by Right A = Administrative Use Permit CZ = Conditional Use Permit/ Zoning Administrator C = Conditional Permit/ Planning Commission - = Not Allowed

¹³ See SRMC Chapter 10.32 (Bowling Alleys)

¹⁴ See SRMC Section 14.17.070 (Game Arcades)

¹⁵ CZ applies if there is a liquor sales use with no food service.

¹⁶ This use allowed in West End Village and Montecito Commercial.

¹⁷ For outdoor eating areas on private property, see SRMC Section 14.17.110 (Outdoor Eating Areas Proposed in Conjunction with Food Service Establishments) standards. For outdoor seating areas located on city sidewalks or rights-of-way, see SRMC Section 14.16.277 (Use Of City Sidewalks and Rights-of-way for Outdoor Eating Areas) standards.

¹⁸ See SRMC Section 14.17.060 (Fortunetelling)

¹⁹ See SRMC Chapter 8.34.

²⁰ See SRMC Section 14.16.220 (Home Occupations)

²¹ See SRMC Section 14.17.100 (Residential Uses in Commercial Districts)

²² See SRMC Section 14.17.130 (Temporary Uses)

²³ See SRMC Chapter 10.60 (Vehicles for Hire)

²⁴ Existing as of December 31, 2020.

²⁵ Subject to stealth design requirements.

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Supplemental to Downtown Form-Based Zones

ARTICLE

3

Division 3.1 Site Standards

3.1.010 Purpose

These standards are intended to ensure that development of property within form-based zones accomplishes the following:

- Makes a positive contribution to the development pattern of the area;
- New or altered structures are compatible with the scale, design and use of existing structures on neighboring properties;
- Respects the existing conditions and safety of neighboring properties; and
- Does not adversely affect neighboring properties, with "adversely affect" meaning to impact in a substantial, negative manner the habitability or enjoyability of these properties.

3.1.020 Screening

This Section prescribes standards for screening, fences, and walls for the conservation and protection of property, the assurance of safety and security, the enhancement of privacy, the attenuation of noise, and the improvement of the visual environment of the neighborhood.

Design Standards for Screening

Screening shall comply with the following:

- **Maximum Screening Height.** Screening shall not exceed the maximums identified in DTFBC Table 3.1.020.A (Maximum Screening Height).
- **Screening Height Measurement.** Screening height shall be measured as the vertical distance between the finished grade at the base of the screen and the top edge of the screen material.

Additional Screening Requiring Director Approval

In compliance with the standards, the following screening types require Director approval.

Courtyard. Along a street or civic space, fences, walls and other screening installed to create a courtyard without a roof shall not exceed four feet in height and be set back a minimum of 10 feet from the front property line or back of sidewalk, whichever is the least. In reviewing the plan for the proposed courtyard, the Director shall consider, but not be limited to, the following:

- Building characteristics including the dimensions, color and architectural design;
- Compatibility of the architectural and design features of the proposed courtyard with the features of the adjoining, as well as neighboring buildings; and

roof screens are subject to Director review and approval and may be referred to the Planning Commission, as determined by the Director.

Wall- and Ground-Mounted Equipment

- The equipment is not allowed between the face of the building and the street;
- All screen devices shall be as high as the highest point of the equipment being screened. Equipment and screening shall be in compliance with the setbacks of the zone;
- Screening shall be architecturally compatible and include matching paint, finish and trim cap of the building; and
- All new mechanical screens for ground or wall-mounted equipment are subject to Director review and approval.

Temporary Fencing

Temporary fencing may be used to provide security for approved special events, construction sites, or vacant structures and land, which cannot otherwise be secured. All temporary fencing shall:

- Consist of chain link fencing or other materials as approved by the Director;
- Not exceed six feet in height;
- Be removed at the conclusion of the special event or completion of construction activities (i.e. final inspection) for which it was approved; and
- All approved fencing for construction sites shall not be installed until a Building Permit or Grading Permit has been issued and shall be removed prior to final inspection. The use of temporary fencing around occupied structures that can be secured by other means is prohibited. The use of temporary fencing around vacant land or vacant structures is subject

to the terms and conditions specified in the Permit authorizing the fencing.

Barbed Wire and Razor Wire

Barbed Wire and Razor Wire screening is not allowed.

Safety

Fences, walls, and other screening and landscaping shall be in compliance with the following:

Maximum height of four feet within 10 feet of the point of intersection of:

- A vehicular access way or driveway and a street; and/or
- A vehicular access way or driveway and a sidewalk.

Two or more vehicular access ways, including driveways, alleys, or streets.

As used in this Sub-Section, "point of intersection" is measured from the face of curb or if none, from the edge of pavement.

3.1.030 Landscaping and Lighting

This Section prescribes landscaping standards for protection and enhancement of the environmental and visual quality of the Downtown, enhancement of privacy, and the control of dust.

Required Landscaping

The landscaping required by this Sub-Section shall be installed as part of the development or improvement requiring the landscaping. Standards for landscaping in parking areas shall be in combination with DTFBC Section 3.1.040 (Parking and Loading).

Standards for Landscape Plans

Landscaping Plans are required for development in all form-based zones.

Design Standards

Acceptable required landscaping materials are defined as follows:

- Shrubs, of one-gallon size or larger;
- Trees, including street trees, of 15-gallon size or larger, and double-staked;
- Ground cover; and
- Decorative nonliving landscaping materials including, but not limited to sand, stone, gravel, wood or water may be used to satisfy a maximum of 25 percent of required landscaping area when approved by the Director.

Species Selection. Native and drought tolerant species are required to meet the minimum standards, in conformance with MMWD Water Conservation Ordinance 414.

Separation. Any landscaped area shall be separated from an adjacent vehicular area by a wall or curb at least six inches higher than the adjacent vehicular area. The curb shall include inlets from the parking surface to collect rainwater.

Existing Vegetation. Every effort shall be made to incorporate mature on-site trees into the required landscaping, subject to approval by the Director.

Maintenance. Required landscaping shall be maintained in a neat, clean and healthy condition. This shall include pruning, weeding, removal of litter, fertilizing, replacement of plants when necessary, and the appropriate watering of all plantings.

Lighting Standards

Refer to SRMC Section 14.1.170 for guidance on lighting standards for Downtown development.

3.1.040 Parking and Loading

This Section prescribes standards for and limits on the development of motor vehicle and bicycle parking, loading and access drives; and standards for reducing motor vehicle trips per capita to and from development. These standards are intended to ensure that development of property within form-based zones accomplishes the following:

- Establishes and/or reinforces the character and scale of walkable, urban neighborhood environments, where development supports and is within a short walking distance of retail and services;
- Ensures the provision of appropriately designed bicycle parking, in order to increase bicycle trips and reduce motor vehicle trips per capita;
- Appropriately limits, screens and landscapes motor vehicle parking, in order to protect and enhance the environmental and visual quality of the Downtown, enhance privacy, attenuate noise, and control dust; and
- Reduces motor vehicles trips per capita to and from development.

General Parking Standards

On-Site Parking. Sharing of parking between different land uses and developments is allowed subject to approval by the City per SRMC Section 14.18.080 (Parking Requirements for Reciprocal Uses with Shared Parking Facilities).

Larger Vehicle Parking

- Trucks, tractors or tractor-trailers having a capacity of more than a one-and-one-half-ton load, front- and rear-end loaders, or any kind of commercial, industrial, agricultural or transportation vehicles/ equipment used primarily for business purposes, shall not be parked or

stored in any zone for purposes other than unloading, loading or delivery services.

- Automobiles, small trucks, vans, vehicle trailers allowed in conjunction with an approved home occupation (one per home occupation), and recreational vehicles, utilized for personal or business use, are excluded from the provisions of this Sub-Section.

Storage of Unregistered or Inoperable Motor Vehicles.

Automotive vehicles, trailers, or vehicles of any kind or type, requiring licenses, but without current plates or inoperable, shall be only parked within completely enclosed buildings.

Cargo or Freight Container. Portable cargo or freight storage containers in any zone for purposes of loading or unloading, may be parked or stored on-premise for a period not to exceed 10 days in any one calendar year.

Commercial Auto Repairs. New businesses offering commercial repair service and/or restoration of vehicles are not allowed.

Number of Motor Vehicle Parking Spaces Required

The maximum number of parking spaces required is listed in DTFBC Sub-Section H (Parking) of the zone. For any use not listed in that Item, parking shall not exceed a ratio equivalent to the average peak parking occupancy rate for the most comparable use in the Institute of Transportation Engineers Parking Generation Manual. The Director required shall determine the most comparable use.

Required Number of Parking Spaces in Form-Based Zones. When calculating the required number of parking spaces, numbers shall be rounded to the closest whole number. Parking systems that stack individual vehicles are counted as three spaces for every horizontal space identified.

Exception in the Event of Changes of Use or Alterations to Existing Buildings or Structures. If an existing building or structure is altered or existing land uses are changed, the existing number of parking spaces on a property may be retained, even if the resulting building, structure or land use would ordinarily be subject to a lower maximum parking allowance.

Electric Vehicle Charging

Electric vehicle charging facilities shall be provided in compliance with SRMC Section 14.18.045 (Designated Parking for Clean Air Vehicles).

Traffic-Minimizing Parking Standards

Carshare Parking Spaces

- Carshare parking spaces shall be provided in the amounts specified in DTFBC Table 3.1.040.A (Required Carshare Parking Spaces).
- The required carshare space(s) shall be made available, at no cost, to a carshare service for purposes of providing carshare services to its members. At the election of the property owner, the carshare spaces may be provided:
 - On the lot; or
 - On another off-street site within 800 feet of the lot.
- Required carshare space or spaces shall be designed in a manner that will make the spaces accessible to non-resident subscribers from outside the building as well as building residents.
- Prior to City approval a building subject to the carshare standard, a Notice of Special Restriction on the property shall be recorded indicating the nature of standards of this Sub-Section and identifying the minimum number and location of the required carshare parking spaces. The form of the notice and the location or locations of

Table 3.1.040.A Required Carshare Parking Spaces

Use	Carshare Parking Spaces Required
Residential	
0-49 Units	None
50-100 Units	1
101 or more Units	2 + 1 per additional 200 Units

Office/ Research and Development	
≤ 10,000 sf	None
> 10,000 sf	1/10,000 sf

the carshare parking spaces shall be approved by the City.

- If it is demonstrated to the satisfaction of the City that no carshare service can make use of the dedicated carshare parking spaces, the spaces may be occupied by non-carshare vehicles; provided, however, that upon 90 days of advance written notice to the property owner from a carshare service, the property owner shall terminate any non-carsharing leases for such spaces and shall make the spaces available to the carshare service for its use of such spaces.

Carpool Spaces. If parking is provided at a development, parking spaces reserved for use by carpool/ vanpool vehicles shall be designated in preferred locations (include, but are not limited to closest building entries). The locations of these spaces shall be approved by the City. The minimum number of carpool spaces required is

listed in DTFBC Table 3.1.040.B (Required Carpool Parking Spaces).

Parking Costs Unbundled from the Cost of Other Goods and Services

- **Residential Uses.** All off-street parking spaces accessory to residential uses in structures of four dwellings or more shall be leased or sold separately from the rental or purchase fees for dwellings for the life of the dwellings. It is the applicants' responsibility to provide renters or buyers of on-site inclusionary affordable units with an equal opportunity to rent or buy a parking space on the same terms and conditions as offered to renters or buyers of other dwellings.
- **Exception.** The Director may grant an exception from this standard for developments which include financing

Table 3.1.040.B Required Carpool Parking Spaces

Use	Carpool Parking Spaces Required
Office/ Research and Development	
≤ 10 Parking Spaces	None
> 10 Parking Spaces	10% of the total number of Spaces
All other Uses	None

for affordable housing that requires that costs for parking and housing be bundled together.

- **Non-Residential Uses.** All off-street parking spaces accessory to non-residential uses may be leased or sold separately from the rental or purchase fees for non-residential building space for the life of the building, such that potential renters or buyers have the option of renting or buying building space at a price lower than would be the case if there were a single price for both the building space and the parking space.
- **Exception.** Off-street parking spaces accessory to retail uses are not required to be leased or sold separately from retail space and may be offered to shoppers and other visitors free of charge for stays of up to two hours.

Parking Spaces, Lot Design and Layout

Access. The following standards are applicable to on-site parking lot access design:

- All on-site parking facilities shall be designed with an appropriate means of vehicular access to a street or to an alley to cause the least interference with traffic flow.
- Parking spaces in any parking lot or parking structure shall not be designed or located so as to allow a vehicle to enter or exit a parking space directly from a public street. Ingress to and egress from parking spaces shall be from an on-site aisle or driveway, except parking spaces within lots may be designed or located so as to allow a vehicle to enter or exit a parking space directly from a public alley or rear lane.
- On-site loading space(s) is not required.

Driveway. Access

- Driveway access to and from developments of two or fewer dwellings onto public streets shall be, where practical, by forward motion of the vehicle; and

- Driveway access to and from developments of three or more dwellings onto public streets shall be by forward motion of the vehicle.

Driveways shall extend to and include the area between the lot line and the edge of the street pavement.

The design and construction of all on-site parking access driveways shall meet City Standards.

Identification as to Purpose and Location. On-site parking areas of four or more spaces shall include painted lines, wheel stops, or other methods of identifying individual parking spaces and loading areas, while distinguishing such spaces from aisle and other circulation features.

Materials

- All on-site parking areas and driveways shall be surfaced with materials as approved by the City Engineer and maintained in compliance with the City Standards.
- The use of pervious or semi-pervious parking area surfacing materials, include, but are not limited to “grasscrete,” or recycled materials including, but not limited to glass, rubber, used asphalt, brick, block and concrete, is subject to approval by the Director and City Engineer. Where possible, such materials should be used in areas in proximity to and in combination with on-site stormwater control devices.

Landscaping, Fencing, and Screening. The landscaping, fencing and screening standards identified in DTFBC Table 3.1.040.C (Required Parking Lot Landscaping) shall be applied with the standards of DTFBC Section 3.1.020 (Screening) and DTFBC Section 3.1.030 (Landscaping).

- Parking and loading areas shall be screened from adjacent residential zones by a six foot wall, fence, or evergreen, subject to approval by the Director.

- Screening is not required when parking area(s) is adjacent to an alley.
- Landscaping areas shall accommodate stormwater management features to the extent feasible as determined by the City.
- For the portion of a parking area over which photovoltaic solar collectors are installed where they also function as shade structures, the minimum standard for trees shall be waived, and shrubs and ground covers shall be planted for every eight parking spaces.

Refer also to SRMC Section 14.18.160 for guidance on screening standards as applicable to the Downtown Zones.

Location. Location of required on-site parking in all zones is regulated by setbacks set forth in DTFBC Sub-Section H (Parking) of the zone and the following:

- Parking lots with 20 or fewer spaces shall have all on-site parking areas separated at least five feet from buildings in order to provide a sidewalk between the building and the parking area;
- Parking lots with more than 20 spaces shall have all on-site parking areas separated at least 10 feet from buildings in order to make room for a sidewalk, landscaping, and other planting between the building and the parking area; and

Table 3.1.040.C Required Parking Lot Landscaping

Number of Parking Spaces	Percent of Gross Parking Area Required to be Landscaped
6 or fewer	5' min. wide planter between lot line, building(s)
7 to 15	4%; 5' min. wide planter between lot line, building(s)
16 to 30	8%; 5' min. wide planter between every 5 spaces, lot line, building(s)
31 to 70	12%; 5' min. wide planter between every 5 spaces, lot line, building(s)
71 and over	16%; 5' min. wide planter between every 5 spaces, lot line, building(s)
Required Shade Trees	
Amount	1 tree per 2,700 sf of gross lot area, minus building coverage (footprint)
Box Size/ Quantity	24" min. size for 20% min. of total trees
Can Size/ Quantity	15 gallon for 80% max. of total trees
Height Clearance	6-8" min. along pedestrian paths
Characteristics	High branching, broad headed, shading form
Required Border	6" high curb or equivalent
Border and Stormwater	Curb shall include breaks every 4" to provide drainage to retention and filtration areas
Tree Well Size ¹	5' x 5' min.
Car Overhangs	Limited to 12" max. by curb or wheel stops

¹Any vehicle overhang requires the minimum planter area width to be expanded by an equivalent dimension.

- This separation may be eliminated to the rear of buildings in areas designed for unloading and loading of materials.

Size of Parking Lot. Parking lots larger than one-quarter of an acre in size shall be broken down into smaller parking areas with planted landscape areas with a minimum width of 15 feet between them to minimize the perceived scale of the total field of stalls.

Tandem Parking. Tandem parking is allowed in all zones for all uses.

Bicycle Parking Standards. Bicycle parking is required in all zones and subject to the standards in SRMC Section 14.18.090 (Bicycle Parking).

3.1.050 Block Size Standards

This Section establishes standards for the maximum size of new or modified blocks in the Plan boundaries to maintain Downtown's fine-grained network of walkable blocks and streets.

Table 3.1.050.A Block Size Standards		
All Zones		
	Without Paseo	With Paseo ¹
Block Face Length	500' max.	600' max.
Perimeter	2,000' max.	2,400 max.

¹Paseo must be at least 20' wide and is subject to design review.

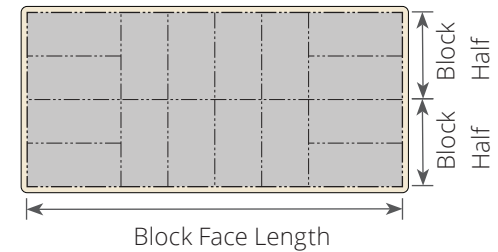


Figure 3.1.050.A Block Size

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Division 3.2 Massing and Facade Articulation Standards

3.2.010 Purpose

The following standards are intended to promote local prevalent patterns that strongly contribute to the Downtown's unique physical character while allowing for creativity and innovation.

3.2.020 Overview of Massing and Facade Articulation Standards

Massing and Facade Articulation Standards. As required by DTFBC Table 3.2.020.A (Massing and Facade Articulation Standards Overview), facades on a street or civic space shall be designed in compliance with the standards identified in this Section.

DTFBC Table 3.2.020.A (Massing and Facade Articulation Standards Overview) provides an overview of the requirements.

Table 3.2.020.A Massing and Facade Articulation Standards Overview

Requirement	Building length along adjacent sidewalk ¹			
	< 75'	75' to 100'	>100'	>150'
3.2.030 (Tripartite Facade Articulation)	No	Yes	Yes	Yes
3.2.040 (Massing and Composition)	Yes	Yes	Yes	Yes
3.2.050 (Corner Elements)	No	No	No	Yes
3.2.060 (Windows and Openings)	Yes	Yes	Yes	Yes
3.2.070 (Historic Resource Adjacency Standards)	Required where adjacent to identified historic resource			

¹ As measured along street or adjacent civic space.

3.2.030 Tripartite Facade Articulation



The top of the building is articulated with an entablature and parapet wall, Image source: www.719larkin.com



Example of a distinct base, middle, and top.

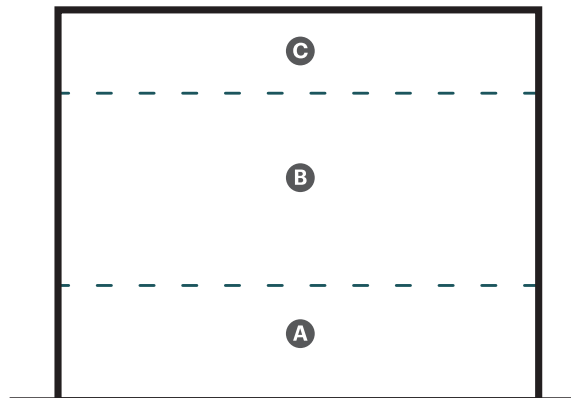


Diagram of tripartite facade articulation.

A. Description

New facades and facade modifications along a street or civic space shall be designed to visually express a base, middle, and top.

B. Applicability

New buildings and facade modifications over 75' in length along the adjacent street or civic space.

C. Standards

Ground Floor/ Distinct Base ¹	A
Middle	B
Top ²	C

Combinations of colors, materials, and massing may be used to visually express a base, middle, and top.

¹The base may include multiple stories.

²The top may include multiple stories. Note that upper stories that are stepped back do not count as part of the "top". To articulate the top, count only stories that are not stepped back from the "base" and "middle" of the building facade.

General Note: Photos on this page are illustrative, not regulatory.

3.2.040 Massing and Composition



Illustrative example.



Illustrative example, Image source: www.realtors.com



Illustrative example.

A. Description

New facades and facade modifications along a street or civic space shall be designed to appear as multiple buildings no greater than 75' in length.

B. Applicability

New buildings and facade modifications.

C. Standards/ General Character

Building facades shall be arranged in an orderly composition of window bays/openings based on prevalent patterns of 5, 7 or 9 bays. **A**

Facades shall be designed in an orderly symmetrical or asymmetrical composition.

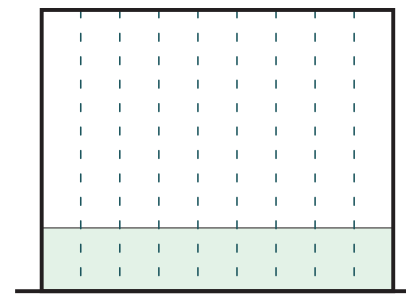
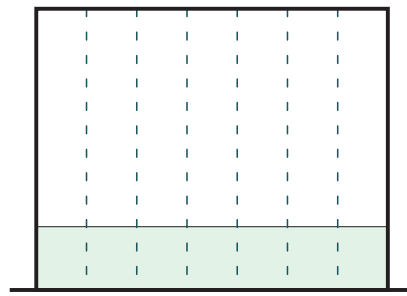
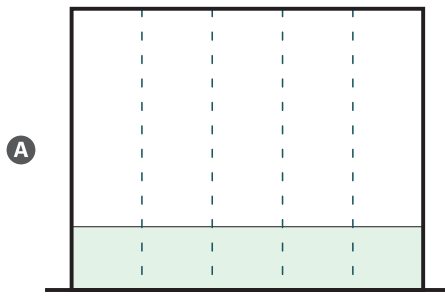
Compositions (symmetrical) **B**

Compositions (asymmetrical) **C**

The pattern shall be visually expressed through the spacing of openings, physical recesses, projections or other techniques.

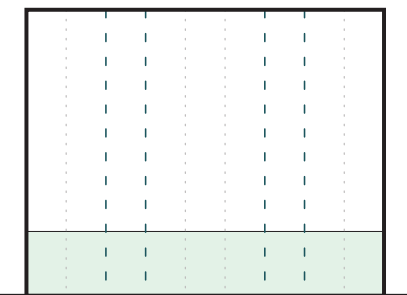
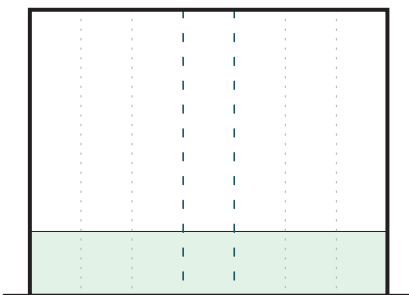
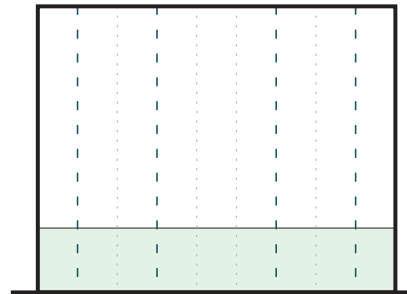
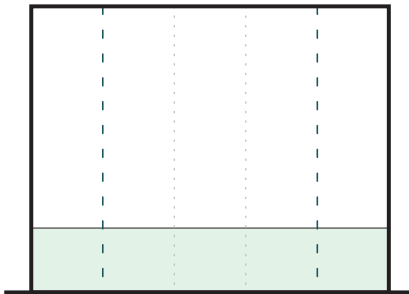
The pattern may include the ground floor and its mezzanine.

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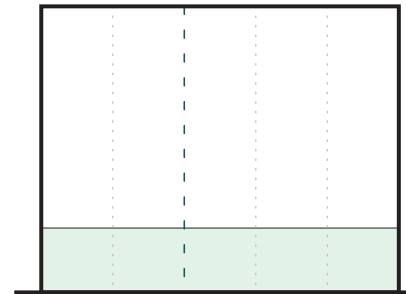


Examples of 5, 7, 9 Bay Openings.

B Examples of Symmetrical Compositions.



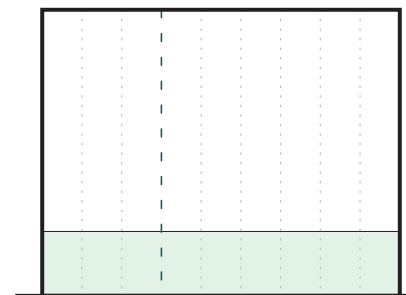
C Examples of Asymmetrical Compositions.



Elevation



Plan



Elevation



Plan

3.2.050 Corner Elements



Example of Corner Element, Image source: www.sitephocus.com



Example of an upper story Corner Element.



Example of an upper story Corner Element.

A. Description

New facades and facade modifications shall be designed to include a corner element to give visual importance to the corner and enhance the public realm.

B. Applicability

New buildings and facade modifications where at least 1 facade along a street corner is over 150' in length.

C. Standards/ General Character

- Square A
- Octagonal B
- Chamfer C

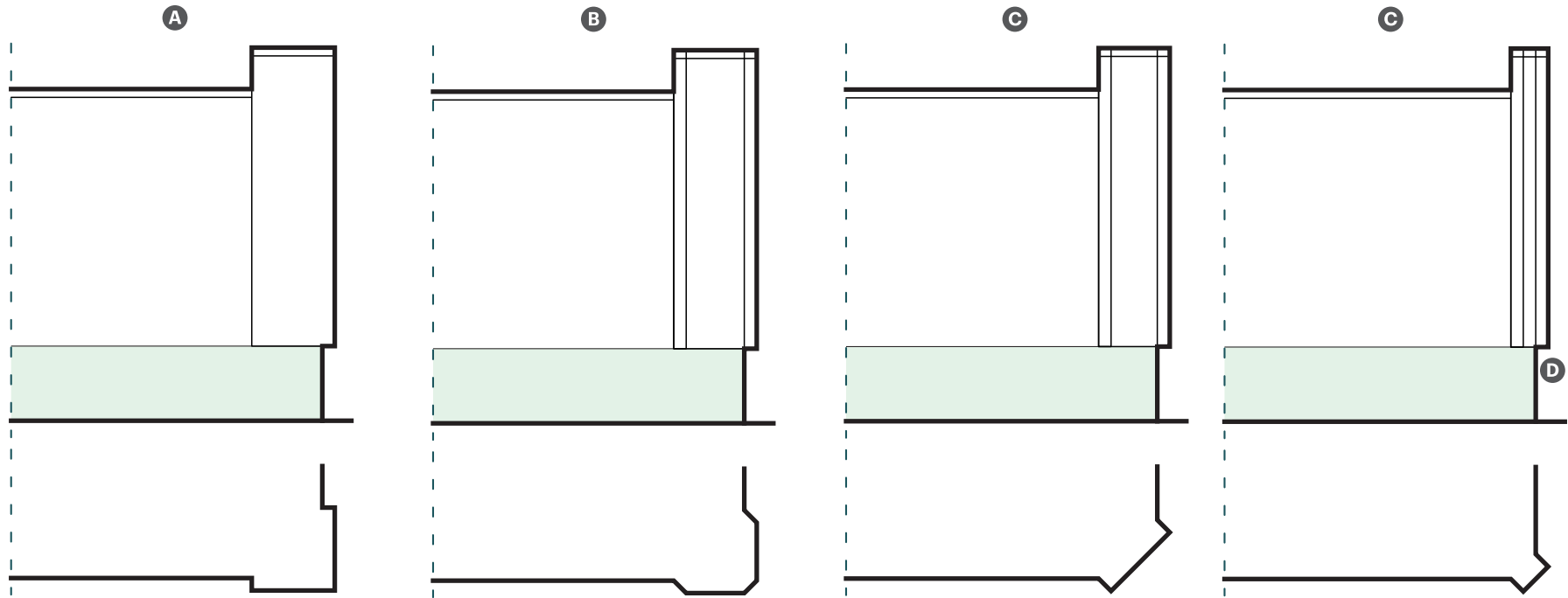
Only one corner element is required per eligible facade.

Corner elements shall incorporate at least 1 entire story within the composition and may extend from the ground floor through the top of the building.

Corner elements may exceed the maximum height allowed for the building by up to 10' for a horizontal area up to 20' x 20', only in cases where the top story is at the maximum allowed height.

Corner elements may be recessed or projected from the main facade by up to 4'. Projections over the right of way are allowed when at least 12' above the adjacent sidewalk. D

General Note: Photos on this page are illustrative, not regulatory.



Plan and elevation diagrams of Corner Elements.

3.2.060 Windows and Openings



Examples of vertical oriented Bay Windows and Openings.



Example of Square Bay Window.



Example of Chamfer Bay Window.

A. Description

New facades and facade modifications shall be designed to include an orderly composition of window bays and openings.

B. Applicability

All new buildings and facade modifications.

C. Standards/ General Character

Buildings 100' or more in length along the street are required to include projected or recessed window bays, and shall be designed per Item A below. The recess may be achieved by a partial or complete window surround. Upto 3 bays may be grouped.

Allowed patterns for buildings at least 100' long:

Bay Windows:

Square

Chamfer

A

a

b

Required stepbacks may be replaced by mansard roof forms with windows as follows:

30' to 50' overall height

Single Mansard **B**

50' to 90' overall height

Single Mansard **B** or

Double Mansard **C**

Mansard roof forms are exempt from stepbacks.

Mansard roofs can include a maximum of 2 stories and must include the uppermost story.

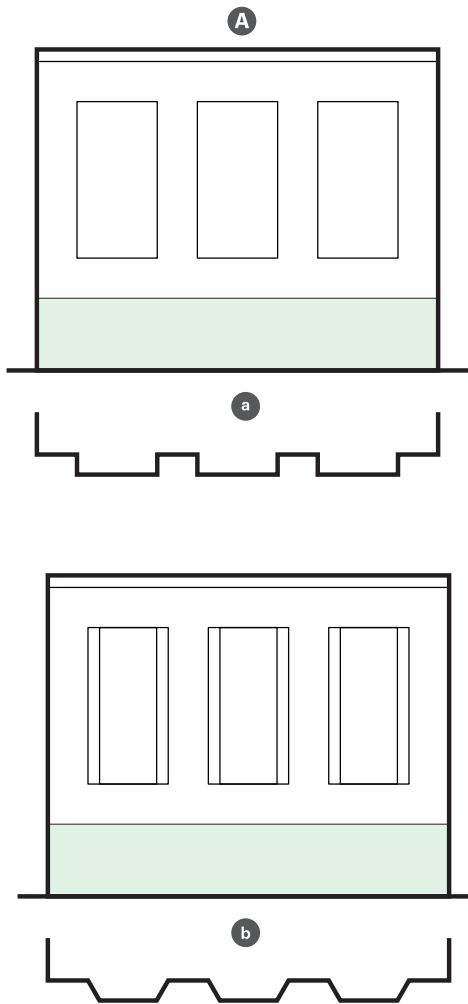
All buildings facades shall be designed to include square or vertically - oriented windows and openings.

Windows may be grouped but the overall grouping shape is required to be square or vertical, except on shopfronts.

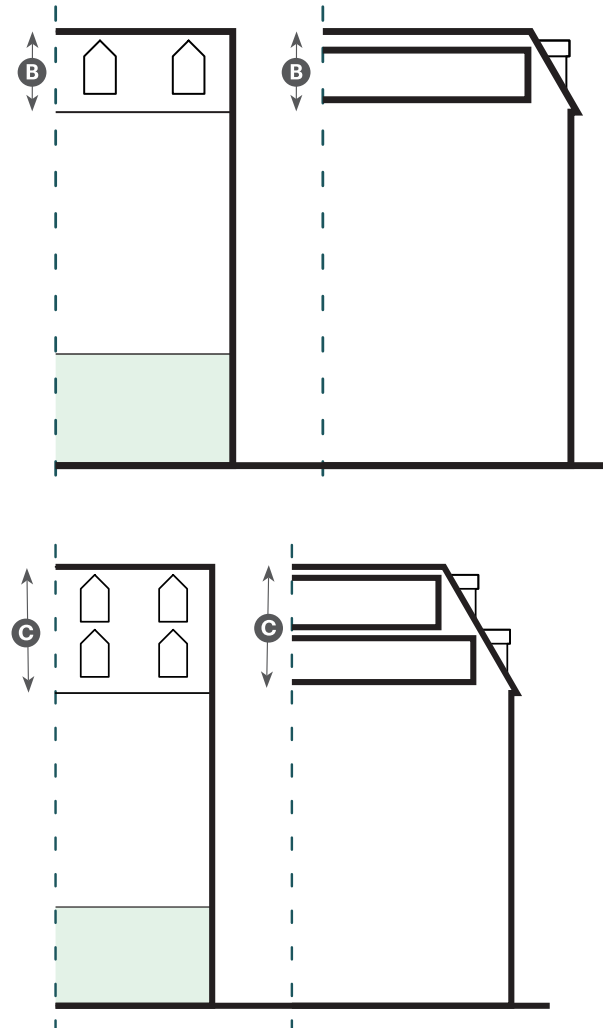
Facades within 1 block of a building with recessed windows and/or openings are required to include recessed windows with at least 4" of recess for at least 25% of the new or modified facade.

General Note: Photos on this page are illustrative, not regulatory.

Bay Windows



Mansard Roof Forms



Examples of vertical and rectangular Window Compositions, Images source: www.flirck.com, Jay Sterlin



Example of Mansard Windows.

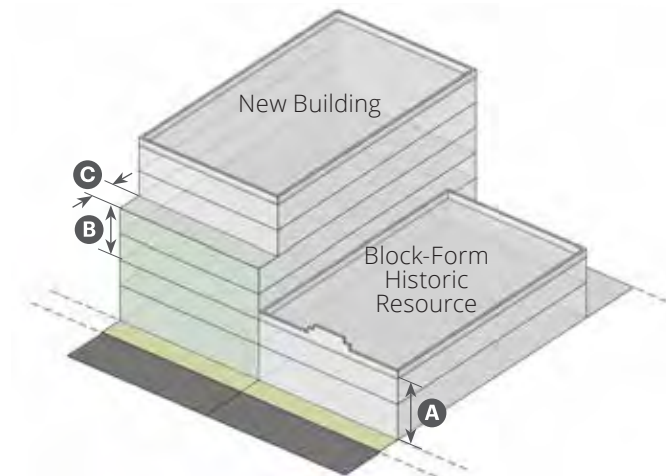
3.2.070 Historic Resource Additions and Adjacency Standards

Any new building or proposed modification on or immediately adjacent to a historic resource identified in DTFBC Figure 2.2.040.A (Regulating Plan) shall be designed in compliance with the additional massing standards identified in this Section. All new construction within the Historic Districts shown in DTFBC Figure 2.2.040.A (Regulating Plan) is considered adjacent to a resource and thus it needs to follow the standards of this Section. Additional height and/or building width/depth is possible if supported by the recommendation of the City's

Architectural Historian and Design Review. The provisions of this section shall not apply to parcels outside of eligible historic districts unless they are adjacent to historic district boundaries or are adjacent to buildings outside the districts that are identified as "individual resources" on Figure 5.6 of the Precise Plan. In addition, the limitations on upper story additions shall only apply to historic resources within eligible districts and to buildings outside the districts that are identified as "individual resources" on Figure 5.6 of the Precise Plan.

Table 3.2.070.A Historic Resource Adjacency Standards

1. All New Buildings Immediately Adjacent to a Block-Form Historic Resource

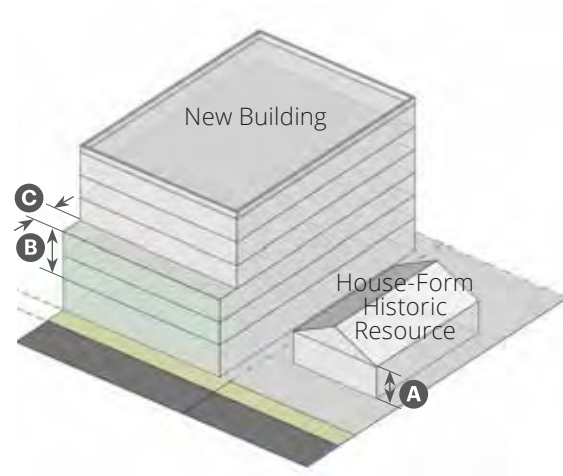


Required stepback for upper floors at maximum 20' above the height of the adjacent block-form historic resource.

- Height of existing historic resource **A**
- Maximum 20' height above height of historic resource **B**
- Minimum 10' stepback. Beyond stepback, maximum heights as allowed by zone standards. **C**

New building must comply with all standards of the zone.

2. All New Buildings Immediately Adjacent to a House-Form Historic Resource



Required stepback for upper floors at maximum 20' above the height of the adjacent house-form historic resource.

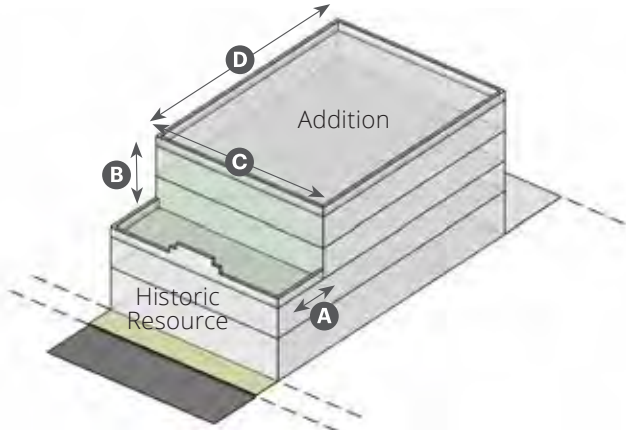
- Height of existing historic resource **A**
- Maximum 20' height above height of historic resource **B**
- Minimum 10' stepback. Beyond stepback, maximum heights as allowed by zone standards. **C**

New building must comply with all standards of the zone.

Table 3.2.070.B Additions to Historic Resources

1. Additions to an Existing Block-Form Historic Resource

2. Additions to an Existing House-Form Historic Resource



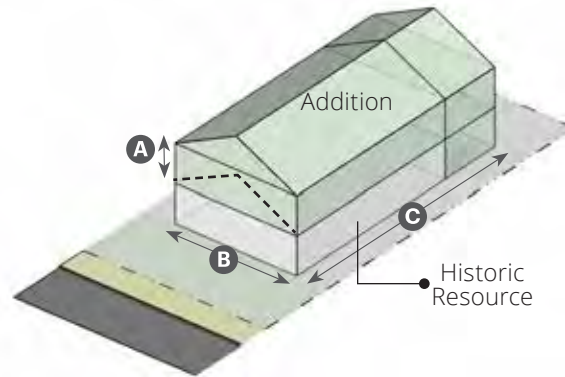
Intensification of an existing block-form historic resource with an addition on top

Minimum 10' stepback from historic facade above height of existing historic resource, or a distance determined by a Qualified Architectural Historian and/or Design Review. **A**

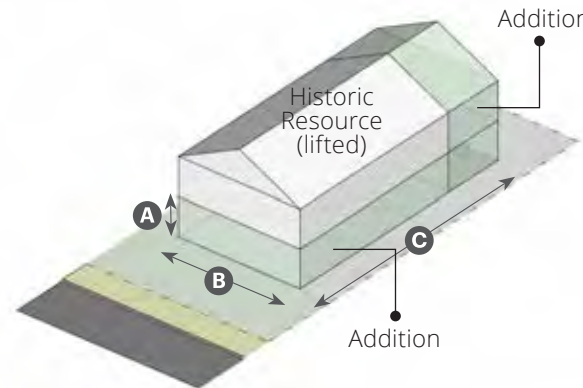
Maximum 20' height addition above existing height of historic resource. For addition greater than 20', review required from a Qualified Architectural Historian and/or Design Review. **B**

Width **C** and Depth **D** of addition.¹

¹ New building must be in compliance with all standards of the zone, and Secretary of the Interior's Standards for Rehabilitation (SISR) #9..



Intensification with an addition on top



Intensification by lifting up resource and constructing below

Maximum 10' height addition above existing height of historic resource. For addition greater than 10', review required from a Qualified Architectural Historian and/or Design Review. **A**

Width **B** and Depth **C** of addition.¹

¹ New building must be in compliance with all standards of the zone, and Secretary of the Interior's Standards for Rehabilitation (SISR) #9.

Division 3.3 Frontage Standards

3.3.010 Purpose

This Section sets forth standards applicable to all frontages. Frontages are the components of a building that provide the transition and interface between the public realm (street and sidewalk) and the private realm (yard or building):

- The names of the frontage types indicate their particular configuration or function and are not intended to limit uses within the associated building. For example, a porch may be used by non-residential uses including, but not limited to a restaurant or office as allowed by the zone.
- Each building shall include at least one frontage type along each street frontage or along a civic space.
- Each building may have multiple frontage types in compliance with the allowed types in DTFBC Sub-Section F (Frontages) of the zone.
- Frontage types not listed in DTFBC Sub-Section F (Frontages) of the zone are not allowed in that zone.
- Each frontage type shall be located in compliance with the facade zone per DTFBC Sub-Section C (Building Placement) of the zone.

Standards are stated for the front of a lot and are to be adjusted for side street facades in compliance with the setbacks of the zone.

3.3.020 Overview of Frontage Types

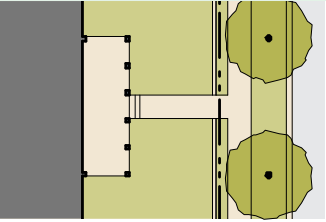
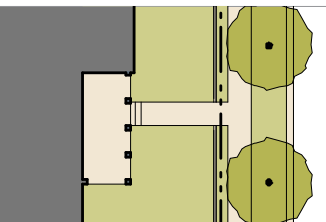

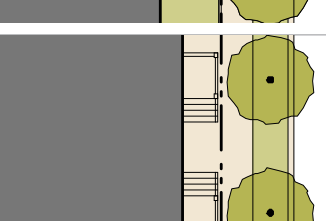
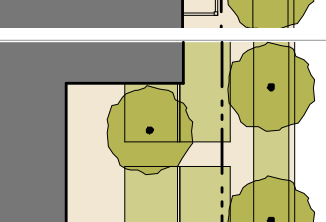
DTFBC Table 3.3.020.B (Frontage Types Overview) provides a summary of the allowed frontage types.

Each Frontage Type is allowed in more than one zone. For the purpose of clarity, the individual zones (e.g. T4N 30/40) are coordinated by their grouping (e.g. T4 Neighborhood). This Division uses these groupings to identify the allowed types in each zone and standards. See DTFBC Table 3.3.020.A (Zone Groupings).

Table 3.3.020.A Zone Groupings

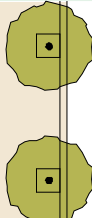
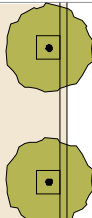
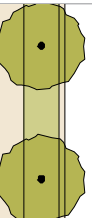
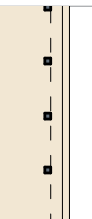
	Zone	Key	Sub-Zone	Key
T4 Neighborhood	T4N 30/40	T4N	T4N 40/50 Open	T4N-O
	T4N 40/50			
T4 Main Street	T4MS 40/50	T4MS	T4MS 40/50 Open	T4MS-O
	T4MS 40/60		T4MS 40/60 Open	
	T4MS 50/70		T4MS 50/70 Open	
	T4MS 60/80			
T5 Neighborhood	T5N 40/60	T5N	T5N 40/60 Open	T5N-O
	T5N 50/70		T5N 50/70 Open	
T5 Main Street	T5MS 70/90	T5MS		

Table 3.3.020.B Frontage Types Overview

Frontage Type		Downtown Zones								
	<p>Porch Projecting 3.3.030. The main facade of the building is set back from the front lot line with a covered structure encroaching into the front setback. The resulting setback area can be defined by a fence or hedge to spatially maintain the edge of the street. The Porch may be one or two stories, is open on three sides, with all habitable space located behind the building setback line.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										
	<p>Porch Engaged 3.3.040. A portion of the main facade of the building is set back from the front lot line to create an area for a covered structure that projects from the facade that is set back. The Porch may project into the front setback. The resulting yard may be defined by a fence or hedge to spatially maintain the edge of the street. The Porch may be one or two stories and has two adjacent sides that are engaged to the building, while the other two sides are open.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										
	<p>Dooryard 3.3.050. The main facade of the building is set back from the front lot line, which is defined by a low wall, hedge, or other allowed screening, creating a small private area between the sidewalk and the facade. Each Dooryard is separated from adjacent Dooryards. The Dooryard may be raised or at grade.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										
	<p>Stoop 3.3.060. The main facade of the building is near the front lot line with steps to an elevated entry. The Stoop is elevated above the sidewalk to provide privacy along the sidewalk-facing rooms. Stairs or ramps from the Stoop may lead directly to the sidewalk or may be parallel to the sidewalk.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										
	<p>Forecourt 3.3.070. The main facade of the building is at or near the front lot line and a portion is set back, extending the public realm into the lot for an entry court or shared garden space for housing, or as an additional shopping or restaurant seating area within retail and service areas.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										

Key T# Allowed T# Allowed in Open Sub-Zone T# Not Allowed

Table 3.3.020.B Frontage Types Overview (Continued)

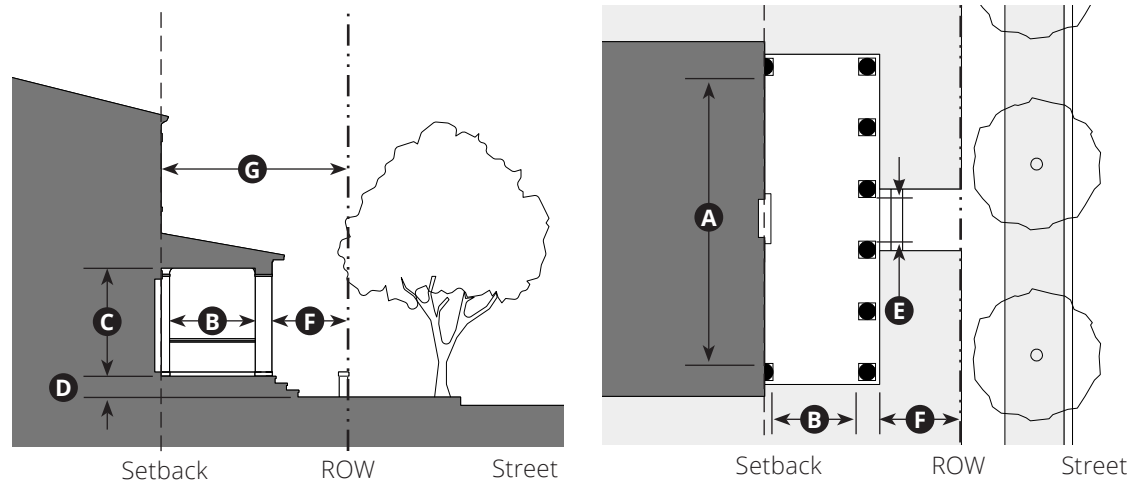
Frontage Type	Downtown Zones									
	<p>Maker Shopfront 3.3.080. The main facade of the building is at or near the front lot line with an at-grade or elevated entrance from the sidewalk. This type is only allowed on side streets from the adjacent main street and is intended for industrial artisan businesses to show their activity to people passing by on the sidewalk, as well as for retail sales of products made on-site. The Maker Shopfront may include a decorative roll-down or sliding door, including glazing and an awning that overlaps the sidewalk.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										
	<p>Shopfront 3.3.090. The main facade of the building is at or near the front lot line with at-grade entrance along the sidewalk. This type is intended for service, retail, or restaurant use and includes substantial glazing between the Shopfront base and the ground floor ceiling and may include an awning that overlaps the sidewalk.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										
	<p>Terrace 3.3.100. The main facade is at or near the front lot line with steps leading to an elevated area providing public circulation along the facade. This type is used to provide outdoor areas along the sidewalk for housing or to accommodate an existing or intended grade change for retail, service or office uses.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										
	<p>Gallery 3.3.110. The main facade of the building is setback from the front lot line and an at-grade covered structure, typically articulated with colonnade or arches, covers an area not in the right-of-way. This type may be one or two stories. When used in non-residential settings, the Shopfront Type is included; when used in residential settings, Stoops, Dooryards, and Forecourts are included.</p>	<table border="1"> <tr> <td>T4N</td> <td>T4N-O</td> </tr> <tr> <td>T4MS</td> <td>T4MS-O</td> </tr> <tr> <td>T5N</td> <td>T5N-O</td> </tr> <tr> <td>T5MS</td> <td></td> </tr> </table>	T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O	T5MS	
T4N	T4N-O									
T4MS	T4MS-O									
T5N	T5N-O									
T5MS										

Key T# Allowed T# Allowed in Open Sub-Zone T# Not Allowed

3.3.030 Porch Projecting



Example is a Projecting Porch.



Key
 - - - - ROW/Lot Line - - - - Setback Line

A. Description

The main facade of the building is set back from the front lot line with a covered structure encroaching into the front setback. The resulting setback area can be defined by a fence or hedge to spatially maintain the edge of the street. The Porch may be one or two stories, is open on three sides, with all habitable space located behind the building setback line.

B. Size

Width, Clear	10' min.	A
Depth, Clear	6' min.	B
Height, Clear	8' min.	C
Overall Height	1 floor max. above ground floor in T4N Zones; 2 floors max. above ground floor in T5N Zones	
Finish Level above Sidewalk	12" min.	D
Pedestrian Access	3' wide min.	E
Distance between Porch and Sidewalk	6' min.	F
Depth	15' min.	G

C. Miscellaneous

Porch shall be open on three sides and have a roof. Clear glass may be installed between the porch columns if the minimum size of individual panes is 24".

Porches are an allowable encroaching element into setbacks. When ramps are included, Design Review is required.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					

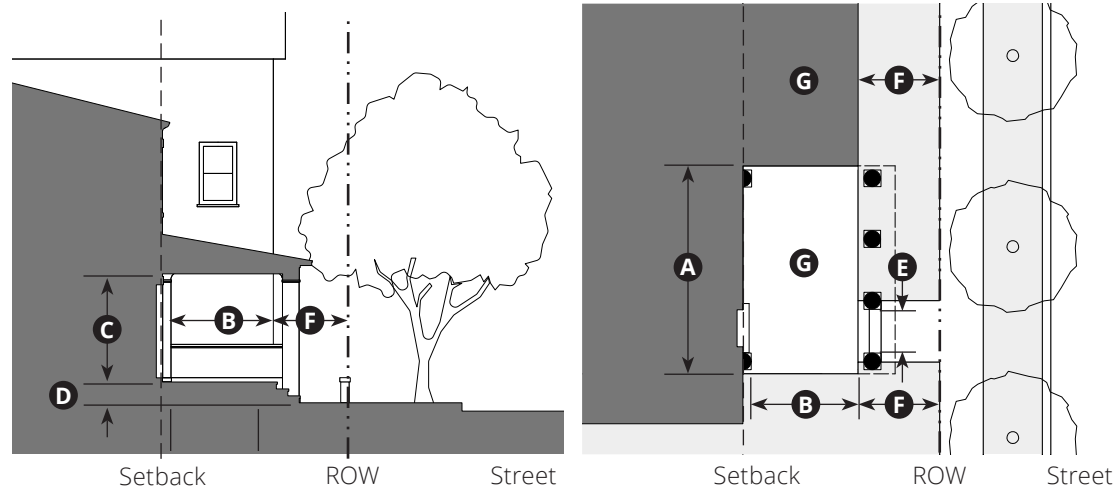
General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

3.3.040 Porch Engaged



Example of a 2-story Engaged Porch.



Key
 - - - - ROW/ Lot Line - - - - Setback Line

A. Description

A portion of the main facade of the building is set back from the front lot line to create an area for a covered structure that projects from the facade that is set back. The Porch may project into the front setback. The resulting yard may be defined by a fence or hedge to spatially maintain the edge of the street. The Porch may be one or two stories and has two adjacent sides that are engaged to the building, while the other two sides are open.

B. Size

Width, Clear	8' min.	A
Depth, Clear	6' min.	B
Height, Clear	8' min.	C
Stories	3 Stories max.	
Finish Level above Sidewalk	12" min.	D
Pedestrian Access	3' wide min.	E
Distance between projected Building Facade, Porch and Sidewalk	6' min.	F

Up to 20% of the building facade and associated porch may project beyond the setback line into the required setback. **G**

C. Miscellaneous

Porch shall be open on two sides and have a roof. Clear glass maybe installed between the porch columns if the minimum size of individual panes is 24".

When ramps are included, Design Review is required.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					

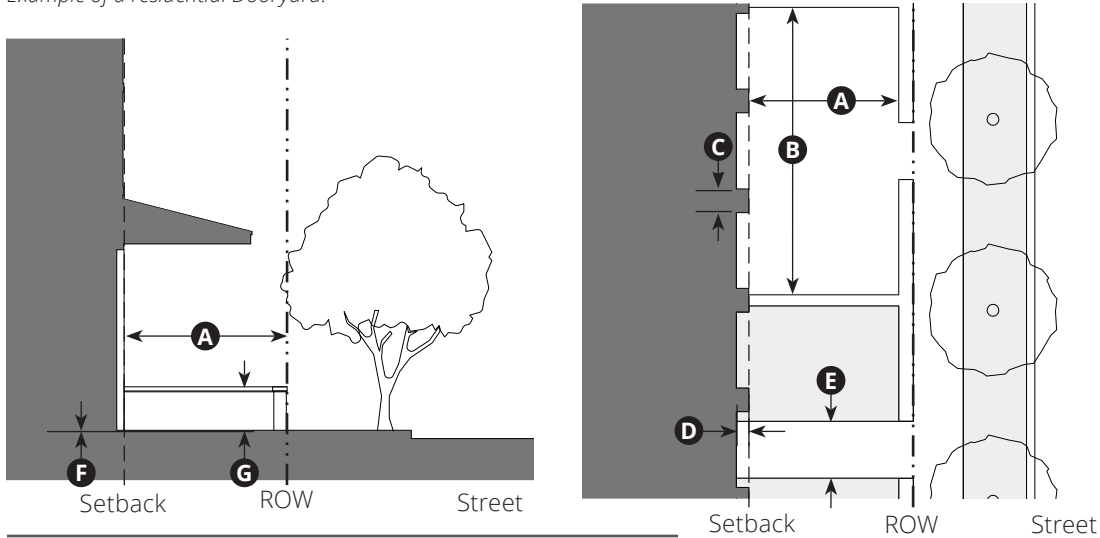
General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

3.3.050 Dooryard



Example of a residential Dooryard.



Key
 ---- ROW/ Lot Line - - - - - Setback Line

A. Description

The main facade of the building is set back from the front lot line, which is defined by a low wall, hedge, or other allowed screening, creating a small private area between the sidewalk and the facade. Each Dooryard is separated from adjacent Dooryards. The Dooryard may be raised or at grade.

B. Size

Depth, Clear	6' min.	A
Length	10' min.	B
Distance between Glazing	4' max.	C
Depth of Recessed Entries	3' max.	D
Pedestrian Access	3' wide min.	E
Finish Level above Sidewalk	0" min., 12" max.	F
Height of Dooryard Fence/ Wall above Finish Level	36" max.	G

C. Miscellaneous

Each Dooryard shall provide access to only one ground floor entry.

For live/ work, retail, service, and restaurant uses, the Shopfront Frontage Type is to also be applied.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					

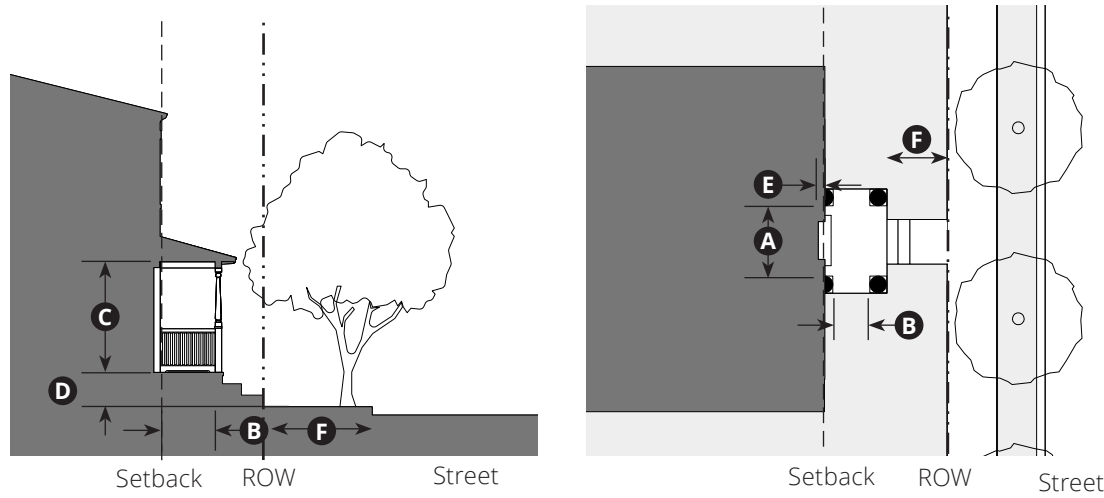
General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

3.3.060 Stoop



Example of Stoop.



Key
 - - - - ROW/ Lot Line - - - - - Setback Line

A. Description

The main facade of the building is near the front lot line with steps to an elevated entry. The Stoop is elevated above the sidewalk to provide privacy along the sidewalk-facing rooms. Stairs or ramps from the Stoop may lead directly to the sidewalk or may be parallel to the sidewalk.

B. Size

Width, Clear	5' min.	A
Depth, Clear	3' min.	B
Height, Clear	8' min.	C
Finish Level above Sidewalk	12" min.	D
Depth of Recessed Entry	5' max.	E
Distance between Stoop and Sidewalk	3' min.	F

C. Miscellaneous

Stairs may be perpendicular or parallel to the building facade.

Ramps shall be parallel to facade or along the side of the building.

When ramps are included, Design Review is required.

Entry doors are covered or recessed to provide shelter from the elements.

Gates are not allowed.

All doors shall face the street.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					

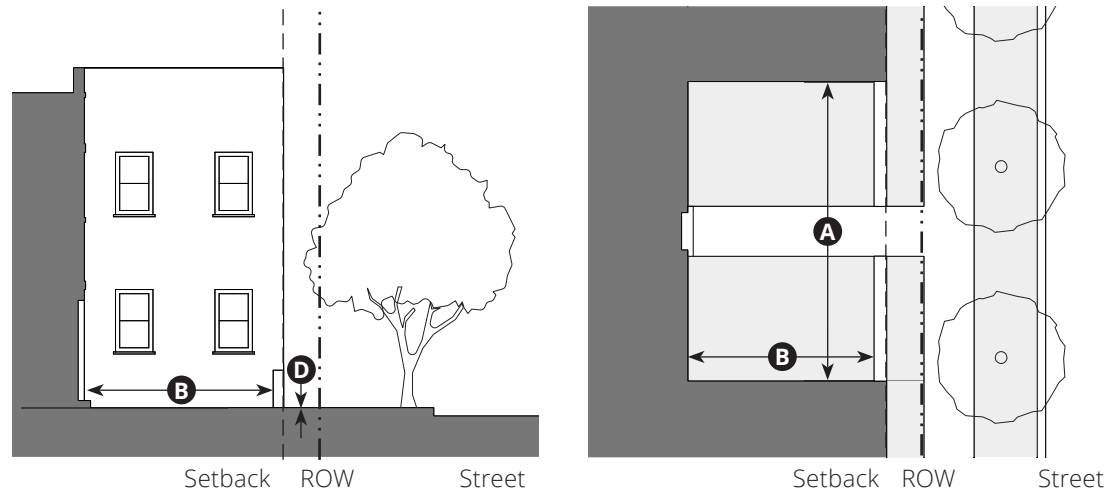
General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

3.3.070 Forecourt



Example of a Forecourt.



Key
 ---- ROW/ Lot Line - - - - - Setback Line

A. Description

The main facade of the building is at or near the front lot line and a portion is set back, extending the public realm into the lot for an entry court or shared garden space for housing, or as an additional shopping or restaurant seating area within retail and service areas.

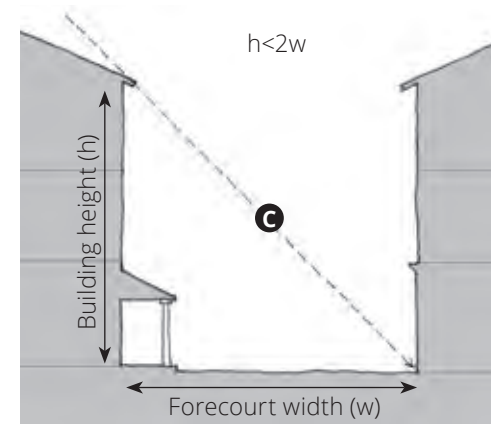
B. Size

Width, Clear	15' min.	A
Depth, Clear	15' min.	B
Ratio, Height to Width	2:1 max.	C
Height from Sidewalk	12" max. above Sidewalk	D

C. Miscellaneous

May be utilized to group several entries at a common elevation in compliance with accessibility standards.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					



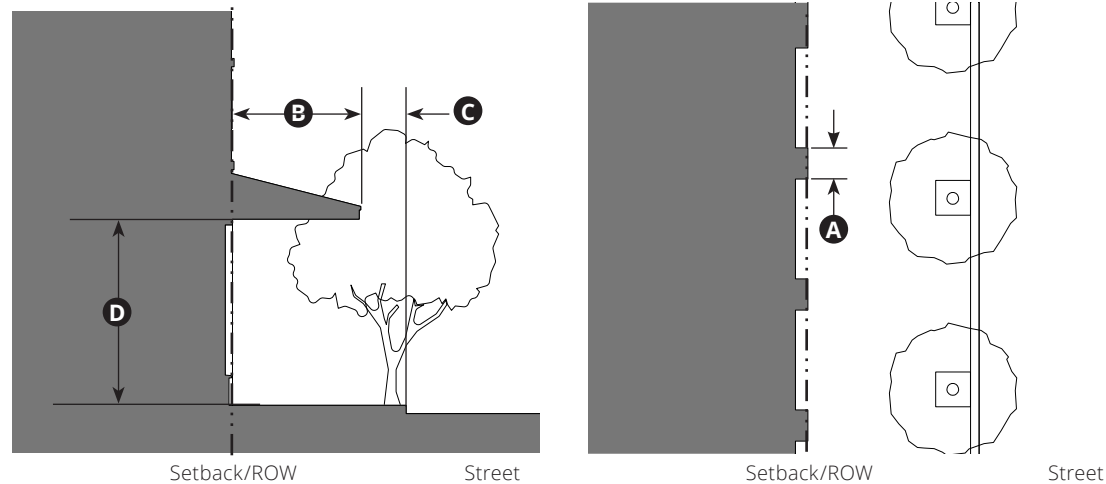
General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

3.3.080 Maker Shopfront



Example of a Maker Shopfront.



Key
 - - - - ROW/ Lot Line - - - - Setback Line

A. Description

The main facade of the building is at or near the front lot line with an at-grade or elevated entrance from the sidewalk. This type is only allowed on side streets from the adjacent main street and is intended for industrial artisan businesses to show their activity to people passing by on the sidewalk, as well as for retail sales of products made on-site. The Maker Shopfront may include a decorative roll-down or sliding door, including glazing and an awning that overlaps the sidewalk.

B. Size

Distance between Glazing	10' max.	A
Ground Floor Glazing between Sidewalk and Finished Ceiling Height	20% min.	

C. Awning

Depth	5' min.	B
Setback from Curb	2' min.	C
Height, Clear	8' min.	D

D. Miscellaneous

Rounded and hooped awning are not allowed.
 Decorative accordion-style doors/ windows or other operable windows that allow the space to open to the street require Director approval.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					

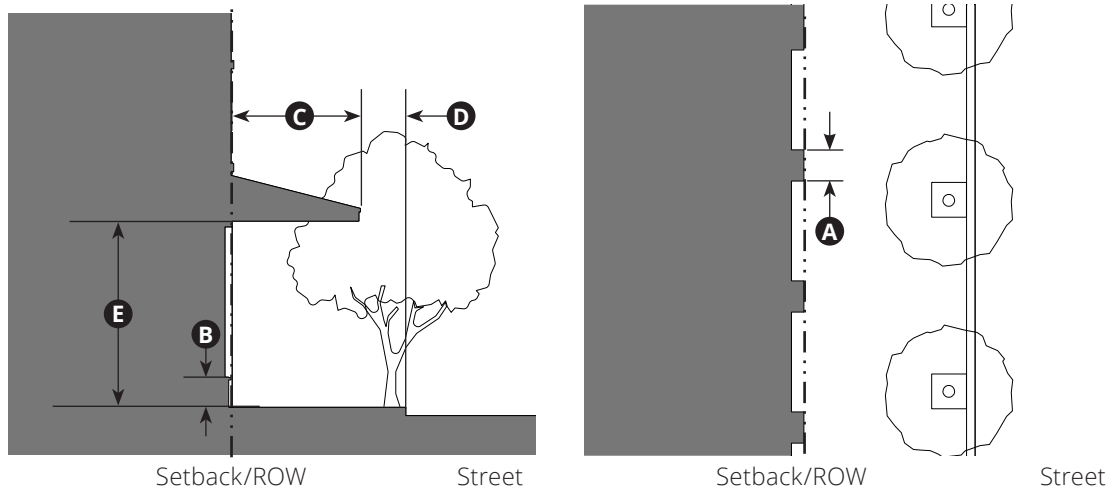
General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

3.3.090 Shopfront



Example of Shopfronts.



Key
 --- ROW/ Lot Line - - - - - Setback Line

A. Description

The main facade of the building is at or near the front lot line with at-grade entrance along the sidewalk. This type is intended for service, retail, or restaurant use and includes substantial glazing between the Shopfront base and the ground floor ceiling and may include an awning that overlaps the sidewalk.

B. Size

Distance between Glazing	3' max.	A
Ground Floor Glazing between Sidewalk and Finished Ceiling Height	75% min.	
Depth of Recessed Entries	5' max.	
Shopfront Base	12" min.; 30" max.	B

C. Awning

Depth	5' min.	C
Setback from Curb	2' min.	D
Height, Clear	8' min.	E

D. Miscellaneous

Mirrored or other reflective finishes, opaque, or other finishes that diminish views into views and out of the ground floor are not allowed.

Residential types of windows are not allowed.

Rounded and hooped awning are not allowed.

Decorative accordion-style doors/ windows or other operable windows that allow the space to open to the street require Director approval.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					

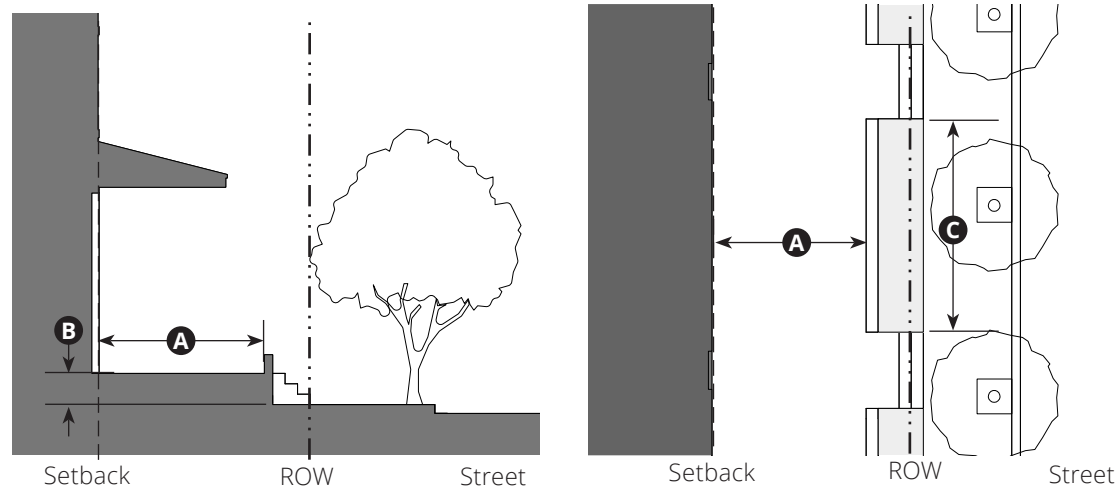
General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

3.3.100 Terrace



Example of a Terrace with low-wall seating.



Key
 - - - - ROW/ Lot Line - - - - Setback Line

A. Description

The main facade is at or near the front lot line with steps leading to an elevated area providing public circulation along the facade. This type is used to provide outdoor areas along the sidewalk for housing or to accommodate an existing or intended grade change for retail, service or office uses.

B. Size

Depth of Terrace	8' min. Residential	A
	12' min. Non-residential	
Finish Level above Sidewalk	36" max.	B
Distance between Stairs	25' max.	C

C. Miscellaneous

Where the zone requires the Shopfront Type, these standards are to be used with those for the Shopfront Frontage Type.

Where the frontage type requires the ground floor to be flush with the sidewalk, the terrace shall be considered to be the sidewalk.

Low walls used as seating are allowed.

May be utilized to group several entries at a common elevation in compliance with the zones' ground floor finish level standards.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					

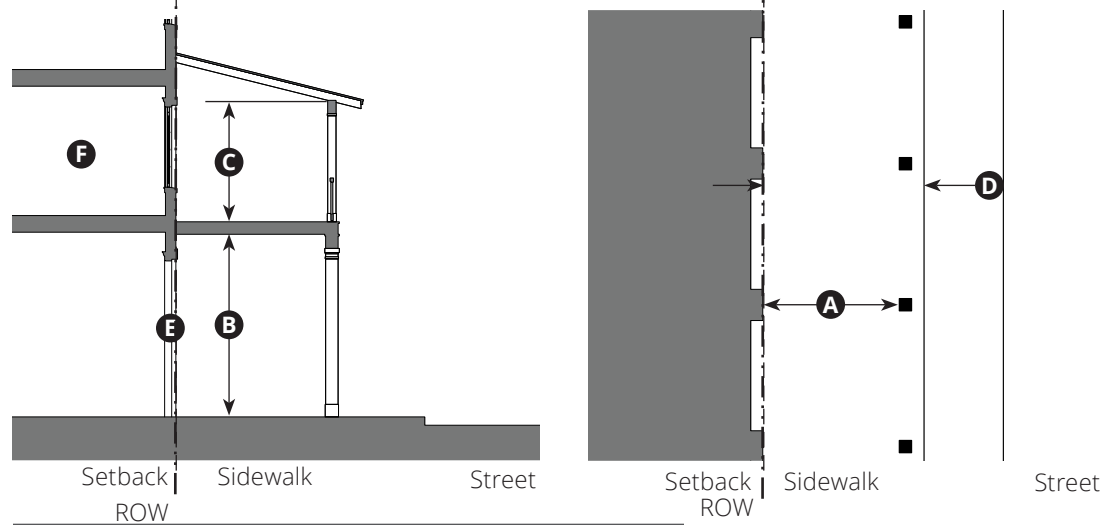
General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

3.3.110 Gallery



Example of a Gallery.



Key
 - - - - ROW/Lot Line - - - - Setback Line

A. Description

The main facade of the building is setback at or near the front lot line and an at-grade covered structure, typically articulated with colonnade or arches, covers an area in the right-of-way. This type may be one or multiple stories. When used in non-residential settings, the Shopfront Type is included; when used in residential settings, Stoops, Dooryards, and Forecourts are included.

B. Size

Depth, Clear	8' min.	A
Ground Floor Height, Clear	12' min.	B
Roof Deck Height, Clear	9' min.	C
Setback from Curb	18" min.	D

C. Miscellaneous

- Galleries shall also follow the standards for the Shopfront Frontage Type. **E**
- Habitable space **F**

Roof Deck above Gallery may be covered.

Galleries shall have a consistent depth.

Galleries are allowed to project over the sidewalk in the public right-of-way.

T4N	T4N-O	T4MS	T4MS-O	T5N	T5N-O
T5MS					

General Note: Photos on this page are illustrative, not regulatory.

Key **T#** Allowed **T#** Allowed in Open Sub-Zone **T#** Not Allowed

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Definitions

ARTICLE

4



Division 4.1 Definitions

4.1.010 Definitions

This Section provides definitions for specialized terms and phrases used in this Article. All other applicable definitions in SRMC Chapter 14.03 (Definitions) of Title 14 (Zoning) apply.

Additional information regarding the methods and measurements for Facade Zone and Highest Top Plate is provided in DTFBC Section 4.1.030 (Measurement Methods).

A Definitions

Architectural Features. Exterior building elements intended to provide ornamentation to the building, including, but not limited to: eaves, cornices, bay windows, window and door surrounds, light fixtures, canopies, and balconies.

B Definitions

Bay Window. A window that projects from the building facade that begins on the ground floor and can extend to upper floors, or begin on an upper floor.

Block Face. The aggregate of all building facades on one side of a block. The block face provides the context for establishing architectural harmony.

Block-Form, Building. A building that is individually as large as a block or individual buildings collectively

arranged along a street to form a continuous facade as long as most or all of a block.

Block Length. The horizontal distance measured from one end of the block to the other end along the same street.

Block Perimeter. The aggregate of all sides of a block measured along the adjacent streets.

Buildable Area. The horizontal area in which a building can be constructed.

Building Entrance. A point of pedestrian ingress and egress to the front of a building along the sidewalk of the street immediately adjacent to the building.

Building Envelope. The three-dimensional shape resulting from applying all required building setbacks, maximum building height, upper floor stepbacks, and facade/ massing articulation requirements.

Building Depth. The horizontal distance from one end of the building to the other end measured perpendicular to the front lot line, including wings.

Building Facade. The exterior wall of a building adjacent to a street, or civic space, the front or side along a private street.

Building Form. The overall shape and dimensions of a building.

Building Frontage. The ground floor facade(s) parallel to and along each adjoining street and/or open space.

Building Width. The horizontal distance from one end of the building to the other end measured parallel to the front lot line, including wings.

C Definitions

Carshare Parking Space. A parking space required to be dedicated for current or future use by a carshare service through a deed restriction, condition of approval, or license agreement. Such deed restriction, condition of approval, or license agreement shall grant priority use to any carshare service that can make use of the space, although such spaces may be occupied by other vehicles so long as no carshare organization can make use of the dedicated carshare space(s).

Carshare Service. A service that provides a network of motor vehicles available to rent by members by reservation on an hourly basis, or in smaller intervals.

Ceiling Height, Ground Floor. The height from finished floor to finished ceiling of primary rooms on the ground floor, not including secondary rooms such as bathrooms, closets, utility rooms, and storage spaces.

Chamfered Corner. An external wall of a building joining two perpendicular exterior walls, typically at a symmetrical, 45 degree angle creating a beveled edge to the building rather than a 90 degree corner.

Civic. A term defining not-for-profit organizations that are dedicated to arts, culture, education, religious activities, recreation, government, transit, and public parking facilities.

Civic Building. A structure operated by governmental or not-for-profit organizations and limited to civic and related uses.

Common Space (Common Area). A portion of a development held in common and/or single ownership and not reserved for the exclusive use or benefit of an individual tenant or owner and is available for use by all persons who reside or work in the building or on the design site.

Community Garden (Use Type). Land used by multiple users for the cultivation, of fruits, vegetables, plants, flowers, or herbs.

Corner Element. A physical distinction in a building at the corner of two streets or a street and civic space. The physical distinction is from the ground floor through a majority of the facade or through the top of the facade.

Corner Entry. A pedestrian entrance located on the corner of a building or lot.

Co-working Space. A facilitated environment which may include shared facilities but not limited to conference rooms, reception services, phones, and other business amenities. Work spaces are used by a recognized membership who share the site in order to interact and collaborate with each other as part of a community and to reduce duplicated costs by sharing facilities. The uses shall have externally observable attributes similar to uses allowed in the zone in which that they are located. Equipment is limited to those which do not generate noise or pollutants in excess of what is customary within a typical office environment. Such space located in a research & development building may use equipment consistent with research & development uses. Co-working space may be interchangeably referred to as "incubator space."

Courtyard. An unroofed area that is completely or partially enclosed by walls or buildings on at least two sides and often shared by multiple residential units or non-residential suites.

D Definitions

Depth, Ground-Floor Space. The distance from the street-facing facade to the rear interior wall of the ground-floor space available to allowed uses.

Depth-to-Height Ratio. The relationship of the depth of a space measured perpendicular to a building divided by the height of the adjacent wall(s).

Disposition, Formal. Composed in a formal arrangement, in a regular, classical, and typically symmetrical manner.

Disposition, Informal. Composed in an informal character with a mix of formal and natural characteristics.

Distance Between Entries. The horizontal distance measured parallel to the facade between entrances to a building or buildings.

Dooryard. See DTFBC Section 3.3.050 (Dooryard).

Double-Loaded, Building. A building containing dwellings and/or commercial suites with common hallways for access to the dwellings and/or suites.

Dormer. A window set vertically in a structure projecting through a sloping roof.

E Definitions

Elevated Ground Floor. A ground floor located above the grade plane of the adjacent sidewalk.

Encroachment. Any architectural feature, structure, or structural element, that breaks the plane of a vertical or horizontal regulatory limit extending into a setback, or above a height limit.

Entry. An opening, such as a door, passage, or gate, that allows access to a building or lot.

Entry, Primary. The opening that allows access to a building directly from the sidewalk along the front facade.

Entry, Service. An entrance located toward or at the rear of the building intended for the delivery of goods and removal of refuse.

F Definitions

Facade. See Building Facade.

Facade Zone. The area between the minimum and maximum setback lines along the front of a lot and along the side street of a corner lot. This area is where a minimum horizontal length of the building facade is required to be placed. The maximum horizontal length of the building facade is limited by the required side setbacks. The horizontal length of the facade not required to be in the facade zone may be placed anywhere within the allowed setbacks of the zone.

Finish Level, Ground Floor. The height difference between the finished floor on the ground floor and the adjacent sidewalk. In the case of a terrace frontage, the floor finish level is the height of the walk above the adjacent street. Regulations for ground floor finish level for ground floor residential uses do not apply to ground floor lobbies and common areas in buildings.

Flex Space. A room or group of internally connected rooms designed to accommodate future commercial uses, while initially accommodating less intense short-term uses, such as residential or live/work, until the commercial demand has been established.

Floorplate. The square footage area measurement of either the gross or the rentable floor area of a floor in a building.

Floorplate, Non-residential. The square footage area measurement of a floorplate dedicated to non-residential uses.

Floorplate, Residential. The square footage area measurement of a floorplate dedicated to residential uses.

Footprint, Building. The outline of the area of ground covered by the foundations of a building or structure.

Freestanding Wall. A wall that is separate from a building and supported by independent means.

Front. See Lot Line, Front.

Frontage, Private. The area between the building facade and the back of the sidewalk abutting a street or civic space.

Frontage, Public. The area between the on-street parking and the back of the sidewalk.

Frontage Type. Physical element(s) configured to connect the building facade to the back of the sidewalk abutting a street or civic open space.

G Definitions

Gable. A vertical wall in the shape of a triangle formed between the cornice or eave and the ridge of the roof.

Glazing. Openings in a building in which glass is installed.

Gross Floor Area. The total floor area inside the building envelope, including the external walls, but not including the roof.

H Definitions

Height, Number of Stories. The number of stories in a structure allowed above adjacent finished grade. See "Story."

Height, Overall. The vertical distance between adjacent finished grade and the highest part of the structure directly above.

Highest Top Plate. The vertical distance between adjacent finished grade and the highest top plate/eave of the building.

Historic Resource. A building or collection of buildings that was identified in the 2019/2020 San Rafael Downtown historic survey as being previously landmarked or potentially eligible for landmarking as an individual resource, a contributing resource, or a historic district.

House-Form, Building. A building that is the size of a small-to-large house and detached from other buildings, typically ranging from 20 feet to as large as 80 feet overall, including wings.

I Definitions

Improvement. The product of any modification to a site, structure or building.

J Definitions

No specialized terms beginning with the letter J are defined at this time.

K Definitions

No specialized terms beginning with the letter K are defined at this time.

L Definitions

Landing. A level area at the top or bottom of a staircase or between one flight of stairs and another.

Lot Line, Front. The perimeter boundary of a lot along the narrow side of the lot as compared to the lot's depth. The front is typically narrower than the side and provides the physical location for the address of the lot. Exceptions to this approach are historic lots that can have front lot lines that exceed the length of the side lot lines.

M Definitions

Main Body. The primary massing of the main building on a lot.

Main Building. The building that contains the principal use of the lot on which it is situated.

Main Facade. The front facade of a building.

Major. Having a greater size, scope, effect, characteristic or quality relative to the other corresponding sizes, scopes, effects, characteristics or qualities; or being the greater of two or more.

Massing. The overall shape or arrangement of the bulk or volume of a building.

Minor. Having a lesser size, scope, effect, characteristic or quality relative to the average size, scope, effect, characteristic or qualities; or being the lesser of two or more.

N Definitions

No specialized terms beginning with the letter N are defined at this time.

O Definitions

Office Amenity Space. Non-employee, non-traffic-generating uses that are not easily convertible to employee-generating uses such as exterior covered walkways, lobby atrium, large cafeteria and employee lounge areas, employee fitness areas, and laboratories.

Oriel Window (Syn Upper Floor Bay Window). A window that projects from the building facade and may extend for multiple stories.

Overdoors. A canopy or other small covering above an exterior door.

Overhead Doors. Doors constructed in horizontally hinged sections that are equipped with hardware that rolls the sections into an overhead position, clear of the opening.

P Definitions

Parapet. A wall along the edge of a roof or the portion of a wall that extends above the roof line.

Parking Driveway Width. The horizontal measurement of an access driveway to a parking area, measured perpendicular to the direction of travel.

Pedestrian-oriented Business. Business located in a context that encourages people to walk instead of drive by allowing customers to park once and complete multiple transactions and visits on foot.

Planting Strip. A landscaped or grassy area located between the street and the adjacent sidewalk.

Podium. A continuous base or pedestal under a building often occupied by parking.

Podium Top. A flat, elevated and open area above a podium that can be used as common area or simply as the base for upper floors.

Porch Engaged. See DTFBC Section 3.3.040 (Porch Engaged).

Porch Projecting. See DTFBC Section 3.3.030 (Porch Projecting).

Production, Artisanal. Food and/or products made by hand.

Public Use. A use undertaken by a political subdivision, its agents or assigns.

Q Definitions

No specialized terms beginning with the letter Y are defined at this time.

R Definitions

Rear. Opposite of front.

Rear Loaded (Rear Access). Lots that provide vehicular access from the rear of the lot.

Recessed Entry. A building entrance that is recessed from the facade of the building.

Regulating Plan. The map that identifies the zoning and standards for each lot in the Plan boundaries.

S Definitions

Semi-Public Use. A use owned or operated by a non-profit organization, private institution or foundation.

Service Entries. Building access for service activities.

Setback, Building. The required clear distance between a lot line and the building.

Setback, Parking. The required clear distance between a lot line and parking.

Shared Parking. Any parking spaces assigned to more than one user, where different persons utilizing the spaces are unlikely to need the spaces at the same time of day.

Shopfront Base. A very low wall, that does not include glass, between the display window(s) of a shopfront and the adjacent sidewalk.

Sidewalk. A paved area along a street intended exclusively for pedestrian use and often installed between the street and a lot.

Single-Loaded, Building. A building containing dwellings and/or commercial suites without common hallways for access to the dwellings and/or suites.

Stealth Design. The effect of integrating an element such as a cellular antenna into a building that results in the element being unobtrusive and not easily perceived from adjacent public sidewalks and civic space.

Stoop. See DTFBC Section 3.3.060 (Stoop).

Story. The portion of a building included between the surface of any floor and the surface of the next floor above it, or if there is no floor above, the space between the floor and the ceiling above. If the finished floor level directly above a basement or cellar is more than six feet above grade for more than 50 percent of the total perimeter, such basement or cellar shall be considered a story.

- **Story, Ground.** The lowest story or the ground story of any building, the floor of which is not more than 12 inches below the average contact ground level at the exterior walls of the building.
- **Story, Mezzanine.** A story which covers one-third or less of the area of the story directly underneath it. A mezzanine story shall be deemed a full story when it covers more than one-third of the area of the story directly underneath said mezzanine story.

Street, Front. Street located along the front lot line.

Street, Side. Street located along a lot line that is not the front or the rear lot line.

Street Frontage, Principal. The property line of a lot parallel to and along the public right-of-way which it borders and which is identified by an officially assigned street address.

Street Tree. A tree of any species or size planted in open spaces, parkways, sidewalk areas, easements, and streets.

T Definitions

Tandem Parking. A parking space deep enough to allow two cars to park, one behind the other.

Transit Stop. A location where buses stop to load and unload passengers. A transit stop sometimes includes a shelter or a dedicated platform along the sidewalk.

U Definitions

Understory. The smaller trees and shrubs below the canopy of large trees.

Upper Floor. A floor in a building containing habitable space that is located above the ground floor.

Urban Agriculture (Crop Production). Areas in some form of small-scale cultivation such as row crops, orchards, or greenhouses that support nearby or on-site food businesses, including cafés and restaurants.

V Definitions

No specialized terms beginning with the letter V are defined at this time.

W Definitions

Walkability. The condition when an area pedestrian-oriented in nature, where bicycling and walking are viable daily options because services, retail and food uses are within a short walking distance of most dwellings.

Wall Plane. A vertical surface defined by the facade(s) of a building(s).

Width-to-Height Ratio. The ratio of the horizontal size of a space measured perpendicularly to the vertical height of a building.

Wing. A structure physically attached to, and smaller in footprint and height to, the Main Body of a building.

X Definitions

No specialized terms beginning with the letter X are defined at this time.

Y Definitions

No specialized terms beginning with the letter Y are defined at this time.

Z Definitions

No specialized terms beginning with the letter Z are defined at this time.

4.1.020 Use Types

This Section provides definitions for specialized terms and phrases used in this Article. All other applicable definitions in SRMC Chapter 14.03 (Definitions) of Title 14 (Zoning) apply.

A Use Definitions

Artisan/ Craft Production. A facility accommodating manufacturing processes involving less intense levels of fabrication and/or production such as the assembly, fabrication, and conversion of already processed raw materials into products, where the operational characteristics of the manufacturing processes and the materials used are unlikely to cause significant impacts on surrounding land uses or the community. Examples of light manufacturing/assembly uses include:

- **Artisan/Craft Product Manufacturing.** An establishment that manufactures and/or assembles small products primarily by hand, including jewelry, pottery and other ceramics, as well as small glass and metal art and craft products, where any retail sales are incidental to the manufacturing activity.
- **Clothing and Fabric Product Manufacturing.** An establishment that assembles clothing, draperies, and/or other products by cutting and sewing purchased textile fabrics, and related materials including leather, rubberized fabrics, plastics and furs. Does not include custom tailors and dressmakers not operating as a factory and not located on the site of a clothing store - see DTFBC 4.1.020 Use Types "Personal Service". Does not include the production of textile fabrics and related materials.
- **Handcraft Industries, Small-Scale Manufacturing.** Establishments manufacturing and/or assembling small products primarily by hand, including jewelry, pottery and other ceramics, as well as small glass and

metal art and craft products, and taxidermists. Also includes manufacturing establishments producing small products not classified in another major manufacturing group, including brooms and brushes; buttons, costume novelties; jewelry; musical instruments; pens, pencils, and other office and artists' materials; sporting and athletic goods; toys; etc.

B Use Definitions

Bar/ Cocktail Lounge/ Nightclub. Brew pub and live entertainment and/or dancing without food and service. For live entertainment see SRMC Section 14.03.030 Definitions "Live Entertainment".

Bed and Breakfast Inn. See SRMC Section 14.03.030 Definitions "Bed and Breakfast Inn".

Building Materials and Supplies. Equipment rental business, hardware store, and paint store.

Business Sales and Services. Establishments providing direct services to consumers. Examples of these uses include employment agencies, insurance agent offices, real estate offices, travel agencies, utility company offices, elected official satellite offices, blueprint and photocopy shop, computer service, locksmith shop, office furniture sales and rental shop, printing shop, etc. This use does not include "Financial Service and Institution" which is separately defined.

C Use Definitions

Community Garden. See SRMC Section 14.03.030 Definitions "Community Garden".

D Use Definitions

Day Care Center. As defined by California Health and Safety Code Section 1596.76, any child or adult day care facility other than a family day care home, and includes

infant centers, preschools, extended day care facilities, and school age child care centers.

Drive-Thru Service. Facilities where food or other products may be purchased, or services may be obtained by motorists without leaving their vehicles. Examples of drive-through sales facilities include fast-food restaurants, drive-through coffee, pharmacies, bank teller windows and ATMs, dry cleaners, etc., but do not include gas station or other vehicle services.

E Use Definitions

Emergency Shelter for the Homeless, Temporary. A facility that provides temporary housing on a first-come, first-served basis where clients must leave in the morning and have no guaranteed bed for the next night OR provide beds for a specific period of time, regardless of whether or not clients leave the building. Facilities that provide temporary shelter during extremely cold weather (such as churches) are also included.

F Use Definitions

Financial Service and Institution. Financial institutions, including, but not limited to: banks and trust companies, credit agencies, holding (but not primarily operating) companies, lending and thrift institutions, other investment companies, securities/commodity contract brokers and dealers, security and commodity exchanges, and vehicle finance (equity) leasing agencies. Does not include check-cashing stores.

Fitness/ Recreation Facility, Less than 2,500 sf. Small-scale facilities, typically accommodating no more than two groups of students at a time, in no more than two instructional spaces. Examples of these facilities include: Individual and group instruction and training in the arts; production rehearsal; photography, and the processing of photographs produced only by users of the

studio facilities; martial arts training studios; gymnastics instruction, and aerobics and gymnastics studios with no other fitness facilities or equipment. Also includes production studios for individual musicians, painters, sculptors, photographers, and other artists.

Food Service Establishment. See SRMC Section 14.03.030 Definitions "Food Service Establishment" and "Food Service Establishment, High Volume".

Food Service Establishments with Alcohol Sales. The retail sale of beer, wine, and/or spirits for on-site or off-site consumption, either as part of another retail use or as a primary business activity.

Fueling Station. A fuel dispensing facility exclusively serving the business occupying the subject property and not involving either wholesale or retail sales of motor vehicles fuels to other individuals or businesses. A fueling station can include mini-market, and minor repair, such as tune-ups, brakes, batteries, tires, and mufflers.

G Use Definitions

No specialized terms beginning with the letter G are defined at this time

H Use Definitions

Hospital/ Major Medical Facility. See SRMC Section 14.03.030 Definitions "Hospital". It can include also extended care facility, treatment and convalescent and children's treatment facility.

Hotel. See SRMC Section 14.03.030 Definitions "Hotel".

I Use Definitions

No specialized terms beginning with the letter I are defined at this time.

J Use Definitions

No specialized terms beginning with the letter J are defined at this time.

K Use Definitions

No specialized terms beginning with the letter K are defined at this time.

L Use Definitions

Live/Work. An integrated dwelling and working space, occupied and utilized by a single household in a structure that has been designed or structurally modified to accommodate joint residential occupancy and work activity, and which includes: Complete kitchen space and sanitary facilities in compliance with the Building Code; and Working space reserved for and regularly used by one or more occupants of the unit.

M Use Definitions

Medical Service. A facility, other than a hospital, where medical, dental, mental health, surgical, and/or other personal health care services are provided on an outpatient basis. A medical office use would provide consultation, diagnosis, therapeutic, preventative or corrective treatment services by doctors, dentists, medical and dental laboratories, chiropractors, counselors, physical therapists, respiratory therapists, acupuncturists and psychiatrists, and similar practitioners of medical and healing arts for humans licensed for such practice by the state of California. Medical service uses typically require use of specialized medical equipment and medical training to evaluate, diagnose and administer treatments, medication or therapies which require a prescription (including administering oxygen or performing dialysis, and sleep diagnostics facilities); increased support staff needs; multiple patient treatment rooms; and patient waiting areas. Counseling services

and other services provided by nonmedical professionals may also be included under DTFBC 4.1.020 Use Types "Professional, Administrative."

N Use Definitions

No specialized terms beginning with the letter N are defined at this time.

O Use Definitions

Office, General. See SRMC Section 14.03.030 Definitions "Office General". It can include Professional and Administrative offices.

- **Professional, Administrative.** Office-type facilities occupied by businesses that provide professional services, or are engaged in the production of intellectual property. Examples of these uses include: Accounting, auditing and bookkeeping services, advertising agencies, attorneys, business associations, chambers of commerce, commercial art and design services, construction contractors (office facilities only), counseling services, court reporting services, design services including architecture, engineering, landscape architecture, urban planning, detective agencies and similar services, doctors, educational, scientific and research organizations, financial management and investment counseling, literary and talent agencies, management and public relations services, media post-production services, news services, photographers and photography studios, political campaign headquarters, psychologists, secretarial, stenographic, word processing, and temporary clerical employee services, security and commodity brokers, writers' and artists' offices.

Q Use Definitions

No specialized terms beginning with the letter Q are defined at this time.

P Use Definitions

Public Park, Playground, and Recreational Facility.

For Recreational Facility see SRMC Section 14.03.030 Definitions "Recreation Facility".

Parking Facility, Commercial or Municipal. See SRMC Section 14.03.030 Definitions "Parking Facility" or "Parking Area".

Personal Service. Establishments that provide non-medical services to individuals as a primary use. Examples of these uses include: barber and beauty shops, clothing rental, dry cleaning pick-up stores with no on-site processing, home electronics and small appliance repair, laundromats (self-service laundries), locksmiths, massage (licensed, therapeutic, non-sexual), bodywork office or establishment, nail salons, pet grooming with no boarding, shoe repair shops, tailors, tanning salons. These uses may also include accessory retail sales of products related to the services provided.

Public and Utility Facility. Public and utility facility such as library, museum, and other cultural facility, corporation maintenance or storage yard, pump station, utility substation storm drainage pond, water tank, utility distribution facility, etc.

R Use Definitions

Repairs, Major. General repair, rebuilding or reconditioning of engines, motor vehicles or trailers; collision service including body or frame, straightening or repair, overall painting or paint shop.

Repairs, Minor. Incidental minor repairs to include replacement of parts and service to passenger cars, such as tune-ups, brakes, batteries, tires, mufflers and upholstery, but not including any operation defined as "automobile repair, major, or any other operation similar thereto.

Residential Care Facility, Other. A facility providing care and treatment in a protective living environment for persons residing voluntarily, by court placement, or under protective control of federal, State, county, or City governance including, without limitation, correctional and post-correctional facilities, juvenile detention facilities, temporary detention facilities, chronically ill tenants, domestic violence victims, outpatient clients, and developmentally disabled. It can include:

- **Nursing Home.** A health care institution other than a hospital or sheltered care home which provides inpatient or resident beds and is licensed by the State Department of Health Services as a skilled nursing facility for two or more unrelated persons. A nursing home provides services to those who need continuous care but do not require hospital care or direct daily care from a physician.
- **Sheltered Care Home.** An institutional living facility for nine –16 unrelated persons providing living facilities, sleeping rooms, and meals. The number listed does not include the operator, members of the operator's family, or persons employed by the operator as staff, except that the total number of persons living in a Sheltered Care Home shall not exceed 18. Also includes temporary or permanent residential housing and service facilities for chronically ill tenants, domestic violence victims, and outpatient clients. It includes State licensed facilities.
- **Group Home.** A residential facility for unrelated persons providing living facilities, sleeping rooms, and meals. This category does not include a home for the developmentally disabled or other institutional uses such as protective living or sheltered care facilities.

Residential Care Facility, Other Small (less than 7).

A facility less than seven providing custodial care and treatment in a protective living environment for persons

residing voluntarily or by court placement including, without limitation, correctional and post-correctional facilities, juvenile detention facilities, and temporary detention facilities.

Residential Care Facility, Other Large (more than 7).

Residential living facilities for seven or more occupants where residents are under protective control of federal, State, county, or City governance. It includes halfway houses, work release programs, and other similar programs. Also includes residential housing and service facilities for chronically ill tenants, domestic violence victims, outpatient clients, and developmentally disabled.

Retail General. Stores and shops intended to serve the City as destination retail, rather than convenience shopping. Examples of these stores and lines of merchandise include:

- Antique store, apparel store, appliance store (and ancillary repair), music and photographic supply store, auction, bicycle shop, department store, discount store, drug store and pharmacy, electronics sales (televisions, radios, computers, etc.), florist shop, furniture store and upholstery shop (and ancillary repair), plant nursery and garden supply, shoe store, sporting goods store, stamp and coin shop, swimming pool supply.
- Art galleries, retail, art supplies, including framing services; books, magazines, and newspapers, cameras and photographic supplies, clothing, shoes, and accessories, collectibles (cards, coins, comics, stamps, etc.), drug stores and pharmacies, dry goods, fabrics and sewing supplies, hobby materials; home and office electronics, jewelry, luggage and leather goods, musical instruments, parts, accessories, small wares, specialty grocery store, specialty shops; sporting goods and equipment, stationery, toys and games; variety stores, videos, DVDs, records, and CDs, including rental stores.

S Use Definitions

School, Parochial, Private or Public. Includes the following facilities:

- **Elementary, Middle, Secondary.** A public or private academic educational institution, including elementary (kindergarten through 6th grade), middle and junior high schools (7th and 8th grades), secondary and high schools (9th through 12th grades), and facilities that provide any combination of those levels. May also include any of these schools that also provide room and board. Does not include pre-schools and child day care facilities (see DTFBC 4.1.020 Use Types “Day Care Center”). See also the definition of “Studio: Art, Dance, Martial Arts, Music, etc.” for smaller-scale facilities offering specialized instruction.

Specialized Education and Training. A school that provides education and/or training, including tutoring, or vocational training, in limited subjects. Examples of these schools include: Art school, ballet and other dance school, business, secretarial, and vocational school, computers and electronics school, drama school, driver education school, establishments providing courses by mail, language school, martial arts, music school, professional school (law, medicine, etc.), Seminaries/religious ministry training facility

T Use Definitions

No specialized terms beginning with the letter T are defined at this time.

U Use Definitions

No specialized terms beginning with the letter U are defined at this time.

V Use Definitions

No specialized terms beginning with the letter V are defined at this time.

W Use Definitions

Wireless Telecommunications Facilities. Public, commercial and private electromagnetic and photoelectric transmission, broadcast, repeater and receiving stations for radio, television, telegraph, telephone, data network, and wireless communications, including commercial earth stations for satellite-based communications. Includes antennas, commercial satellite dish antennas, and equipment buildings. Does not include telephone, telegraph and cable television transmission facilities utilizing hard-wired or direct cable connections

X Use Definitions

No specialized terms beginning with the letter X are defined at this time.

Y Use Definitions

No specialized terms beginning with the letter Y are defined at this time.

Z Use Definitions

No specialized terms beginning with the letter Z are defined at this time.

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4.1.030 Measurement Methods

Building within Facade Zone

Applicability. The facade zone standards apply to new buildings and additions along the front and side street of a design site.

Methodology. The required amount is expressed in the zone standards as a percentage. The percentage is calculated as follows through an example for the front facade zone. The same approach is to be applied to the side street, using the minimum front and rear building setbacks.

1. Identify width of design site (e.g., 50') and apply required front and side building setbacks (e.g., 5' and 5').
2. Subtract the horizontal length between each side setback and the adjacent side design site line from the total width of the design site. The result is net buildable width of the design site (e.g., 40').
3. Multiply the required minimum percentage in the zone standards (e.g., 50%) by the net buildable width of the design site (e.g., 50').
4. The result is the minimum length, in feet, of building facade that shall be placed in or abutting the facade zone (e.g., 20').
5. See DTFBC Figure 4.1.030.B (Applying the Required amount to the Facade Zone) for examples that are consistent with the intent of this standard.

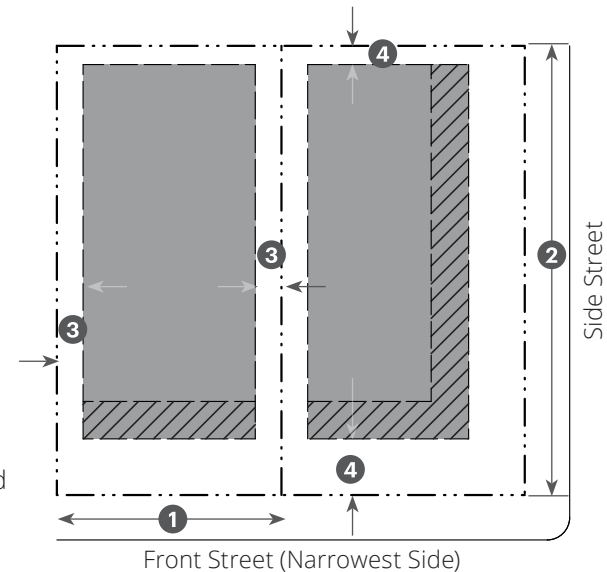


Figure 4.1.030.A Determining the Required Amount Subject to the Facade Zone

Table 4.1.020.A Example Calculation

50'	Design Site Width
- 5'	Side Setback
- 5'	Side Setback
= 40'	Net Buildable Width
40'	Net Buildable Width
x Zone Standard	(e.g., 50%)
= 20'	Required In or Abutting the Facade Zone

Key

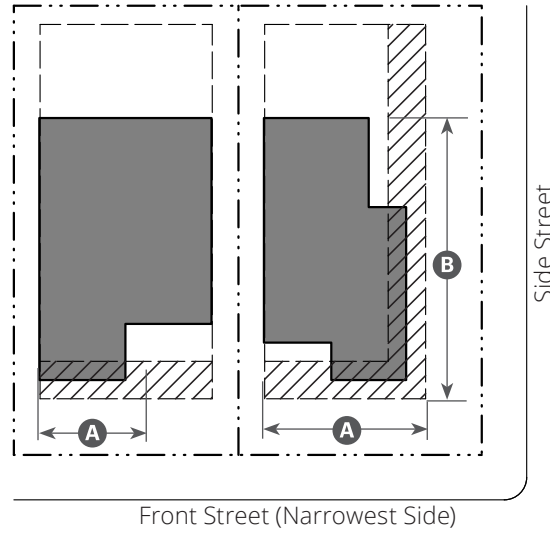
- ① Width of Design Site
- ② Depth of Design Site
- ③ Setback to be subtracted from Design Site Width
- ④ Setback to be subtracted from Design Site Depth

Figure 4.1.030.B Applying the Required amount to the Facade Zone

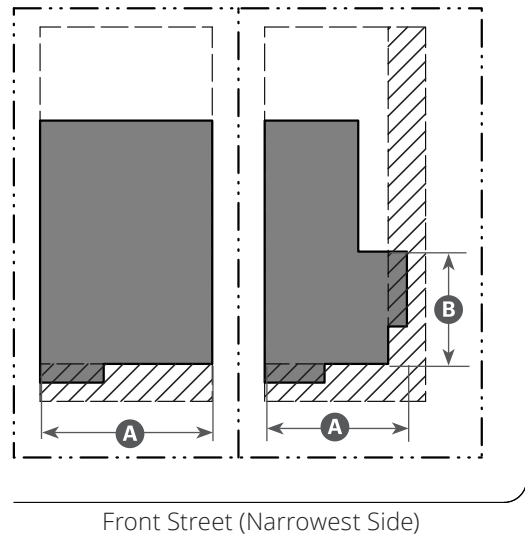
Abutting Facade Zone



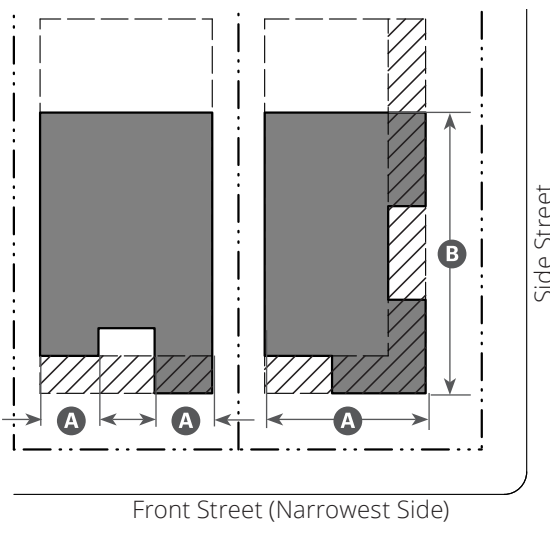
Within Facade Zone



Abutting and Within Facade Zone



At Front of Facade Zone



Key

/// Facade Zone

■ Buildable Area

Front Street

Side Street

Ⓐ 50% min.

Ⓑ 50% min.

Highest Top Plate

A Height, Overall. The vertical distance between adjacent finished grade and the highest part of the structure directly above.

B Top of Parapet

C Roof Structure

D Highest Top Plate. The vertical distance between adjacent finished grade and the highest top plate/eave of the building.

- For buildings with a pitched roof the measurement is to the highest eave.
- For buildings with a parapet and flat roof, the measurement is to the highest top plate.
- For buildings with a parapet and flat roof, the top plate of the parapet is not considered the highest top plate of the building.

E Highest Eave Measurement. The measurement is to bottom of the eave assembly.

F Eave. The edge of the roof that overhangs the face of the wall. The bottom of the eave can range from the exposed rafters to a finished horizontal surface.

G Dormer

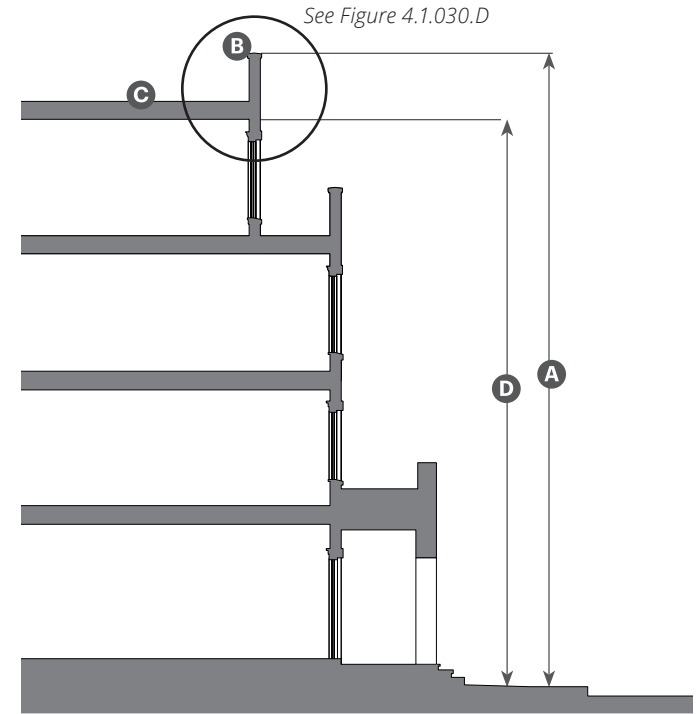


Figure 4.1.030.C Highest Top Plate for parapet and flat roof.

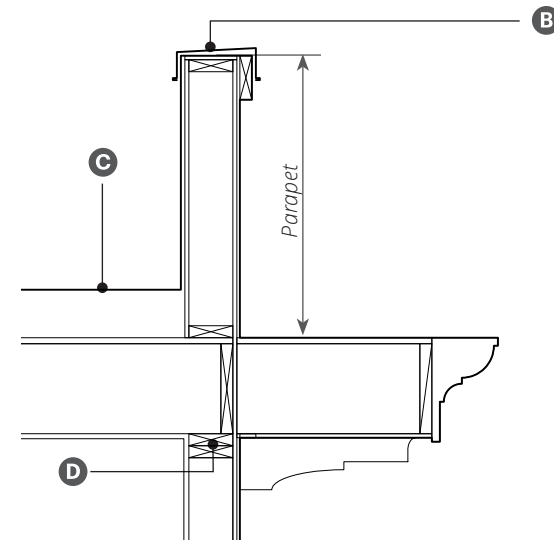


Figure 4.1.030.D Section detail of Highest Top Plate for parapet and flat roof.

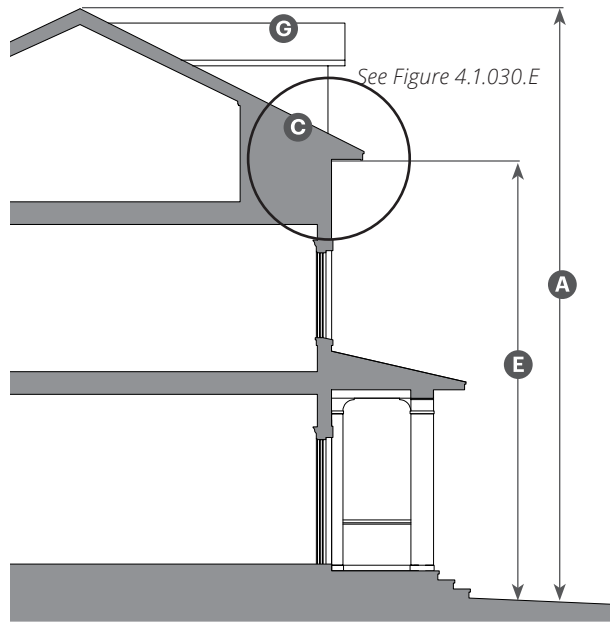


Figure 4.1.030.E Highest Top Plate for pitched roof.

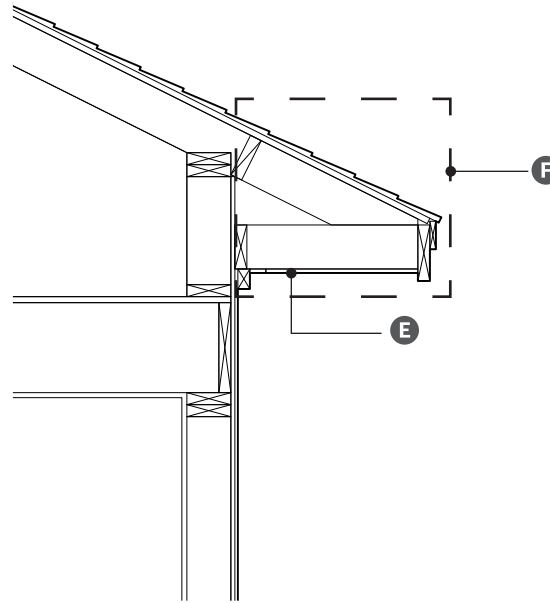


Figure 4.1.030.E Section detail of Highest Top Plate for pitched roof.

Height Measurement on Sloped Sites

Applicability. All sites where average slope is greater than 6 percent or as determined by the Director.


Building height and height bonus is measured vertically from the existing grade to the highest top plate at any given point.

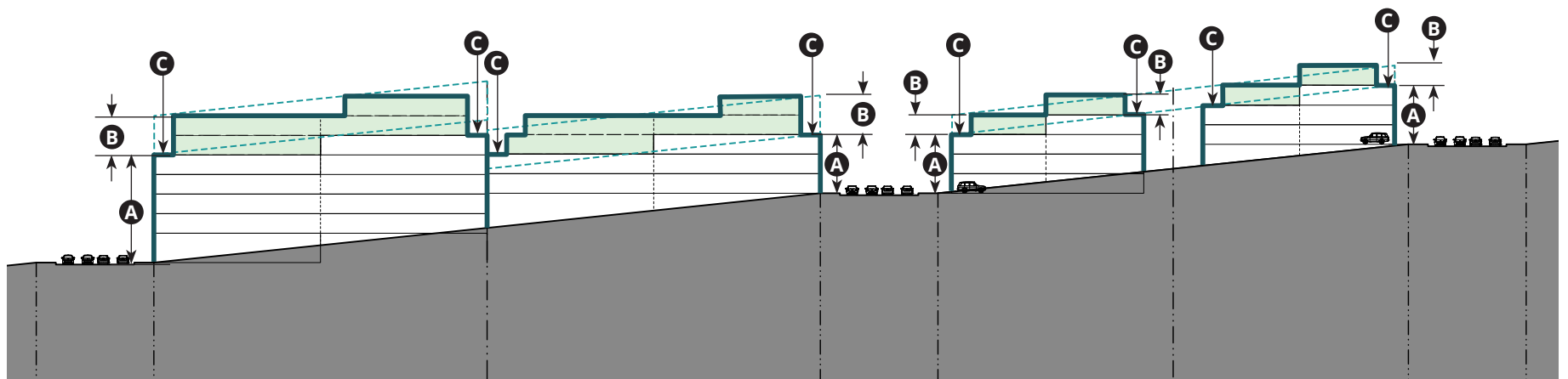
On sites with topography, additional height may be considered under SRMC Section 14.24.020.E

- A Stepback Height** as specified in the Zone Standards
- B Allowed Height Bonus** as specified in the Zone Standards
- C Upper Story Setback** as specified in the Zone Standards

 Height measurement parallel to slope

 Building profile

 **Bonus floors.** Additional height obtained through the height bonus



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EXHIBIT 10

DENSITY BONUS CALCULATIONS - BASE PROJECT:

- a. The base density has been calculated using the objective controls set for in Ch. 9 Downtown Precise Plan (specifically Section 2.3.050 T5 Neighborhood (TSN 40/60 and TSN 50/70)). Per the Base Project Floor Area Analysis shown below, the base Project includes 100 residential dwelling units.
- b. The Base Project includes 15 Very Low Income Units (15%), and 15 Moderate Income Units (15%)
- c. The project density bonus request is 100% (100 bonus units) for a total of 200 Units.
- d. The parking ratio for the "base project" is shown in a separate table below.

BASE PROJECT - ZONING INFORMATION

ZONING TSN 50/70
 LOT AREA (GROSS) 25,550 SQUARE FEET

TSN 50/70

	BASE ZONING	PROPOSED	COMPLIANCE
HEIGHT	50'	49'11"	COMPLIES
SETBACK - FRONT	0' MIN; 15' MAX	0'	COMPLIES
SETBACK - SIDE STREET	0' MIN; 15' MAX	0'	COMPLIES
SETBACK - SIDE	0' MIN; 15' MAX	VARIES AT EASEMENT	COMPLIES
SETBACK - REAR	0' MIN; 15' MAX	0'	COMPLIES
STEPBACKS - FRONT	0' MIN; 15' MAX	NONE W/ MANSARD ROOF	COMPLIES
STEPBACKS - SIDE STREET	0' MIN; 15' MAX	NONE W/ MANSARD ROOF	COMPLIES
STEPBACKS - REAR	0' MIN; 15' MAX	NONE W/ MANSARD ROOF	COMPLIES

BASE PROJECT - FLOOR AREA TABLE

	FLOOR AREA (SQUARE FEET)				GROSS AREA
	RESIDENTIAL	COMMERCIAL	MISC NON-RES	PARKING	
LEVEL 5	21,490				21,490
LEVEL 4	22,695				22,695
LEVEL 3	22,695				22,695
LEVEL 2	22,695				22,695
LEVEL 1 (GROUND)	13,725	5,000	900	2,000	21,625
BASEMENT 1				22,200	23,575
BASEMENT 2				11,100	11,100
TOTAL	103,300	5,000	2,275	35,300	145,875

+ 100% Bonus Residential Floor Area = 206,600 SF

BASE PROJECT - UNIT MIX SUMMARY

OVERALL	DWELLING UNITS				TOTAL
	ST	1B	2B		
LEVEL 5	8	9	6		23
LEVEL 4	8	8	6		22
LEVEL 3	8	8	6		22
LEVEL 2	8	8	6		22
LEVEL 1	4	7	0		11
TOTAL	36	40	24		100

UNIT DISTRIBUTION	ST	1B	2B	TOTAL
LEVEL 50%AMI UNITS	6	6	3	15
LEVEL 120%AMI UNITS	5	6	4	15
LEVEL MARKET-RATE UNITS	25	28	17	70

*Legend: ST: Studio 1B: 1 Bedroom Unit 2B: 2 Bedrooms Unit 50%AMI UNITS: Very Low Income Units 120%AMI UNITS: Moderate Income Units

BASE PROJECT - VEHICULAR PARKING

The project is not located within the Downtown Parking District therefore per Downtown Precise Plan 2.3.050.H residential parking requirements are:

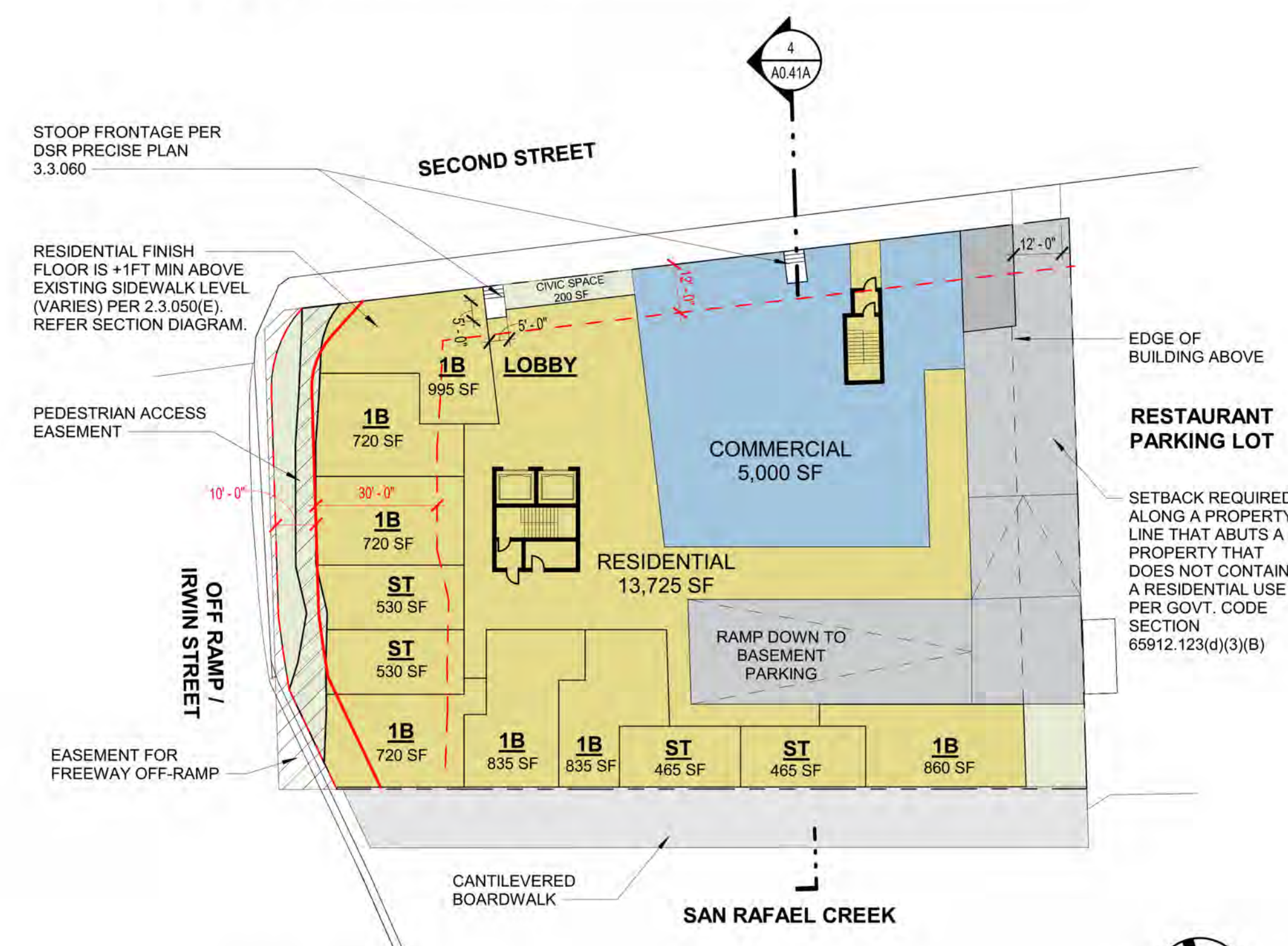
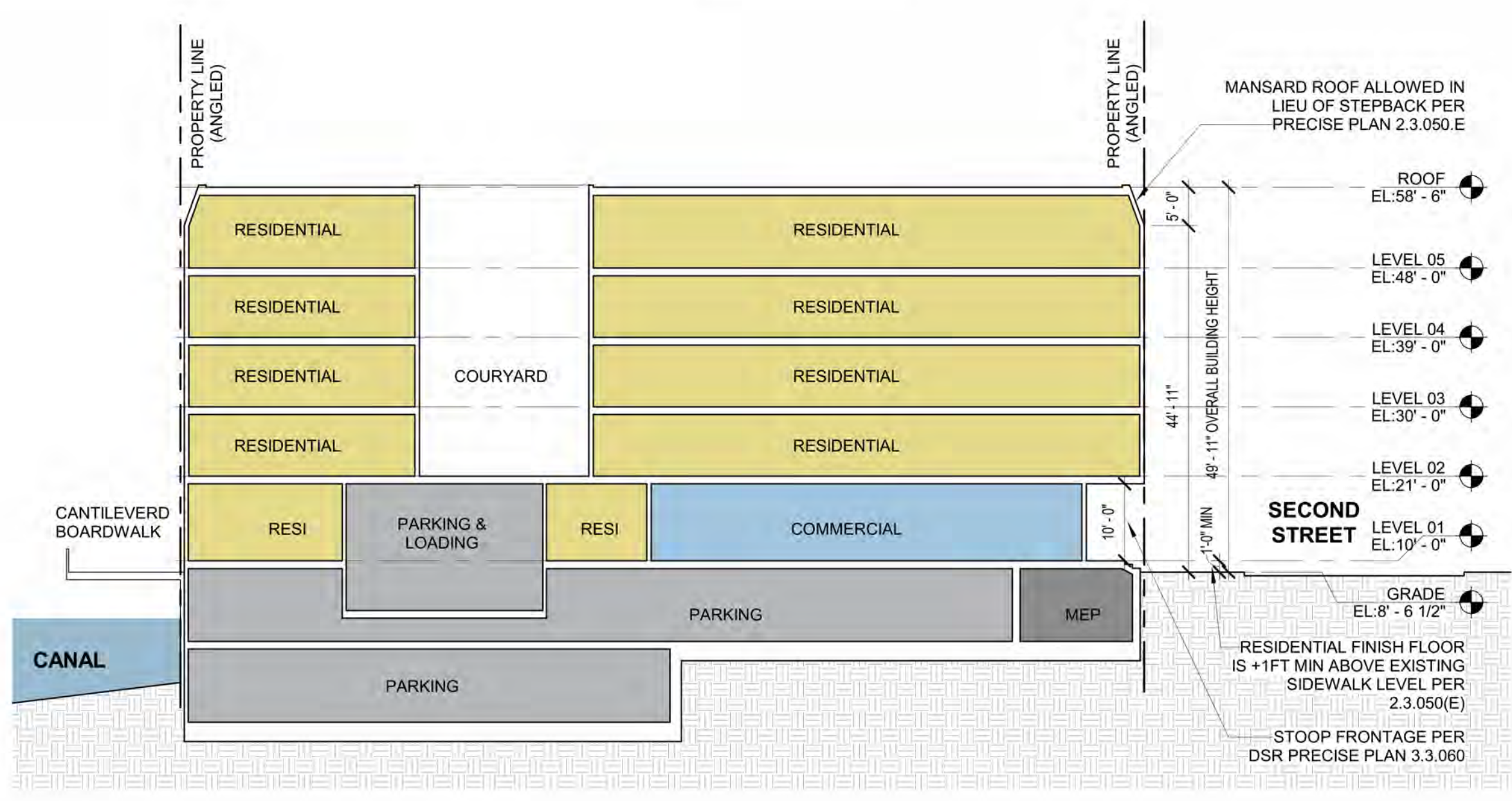
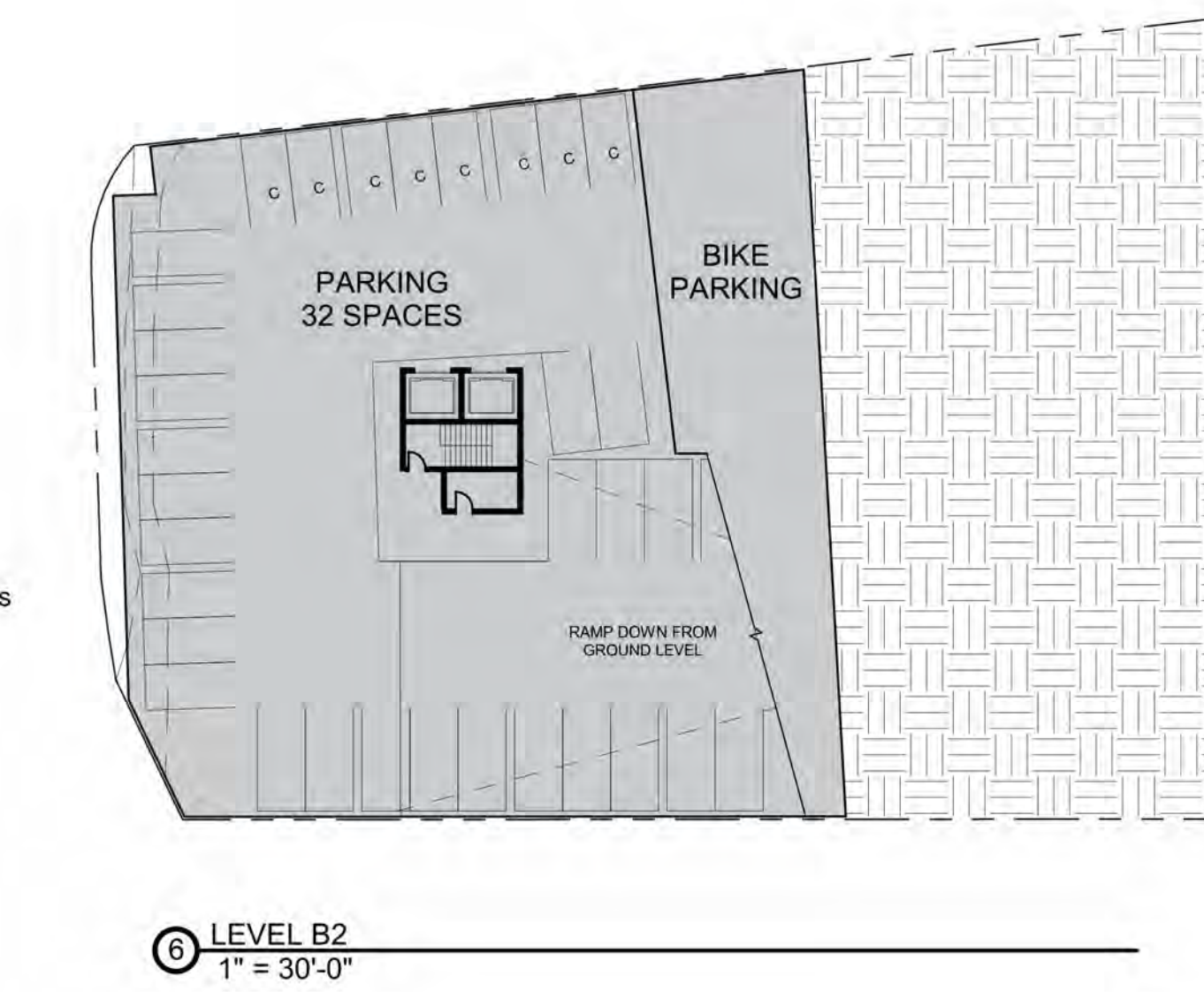
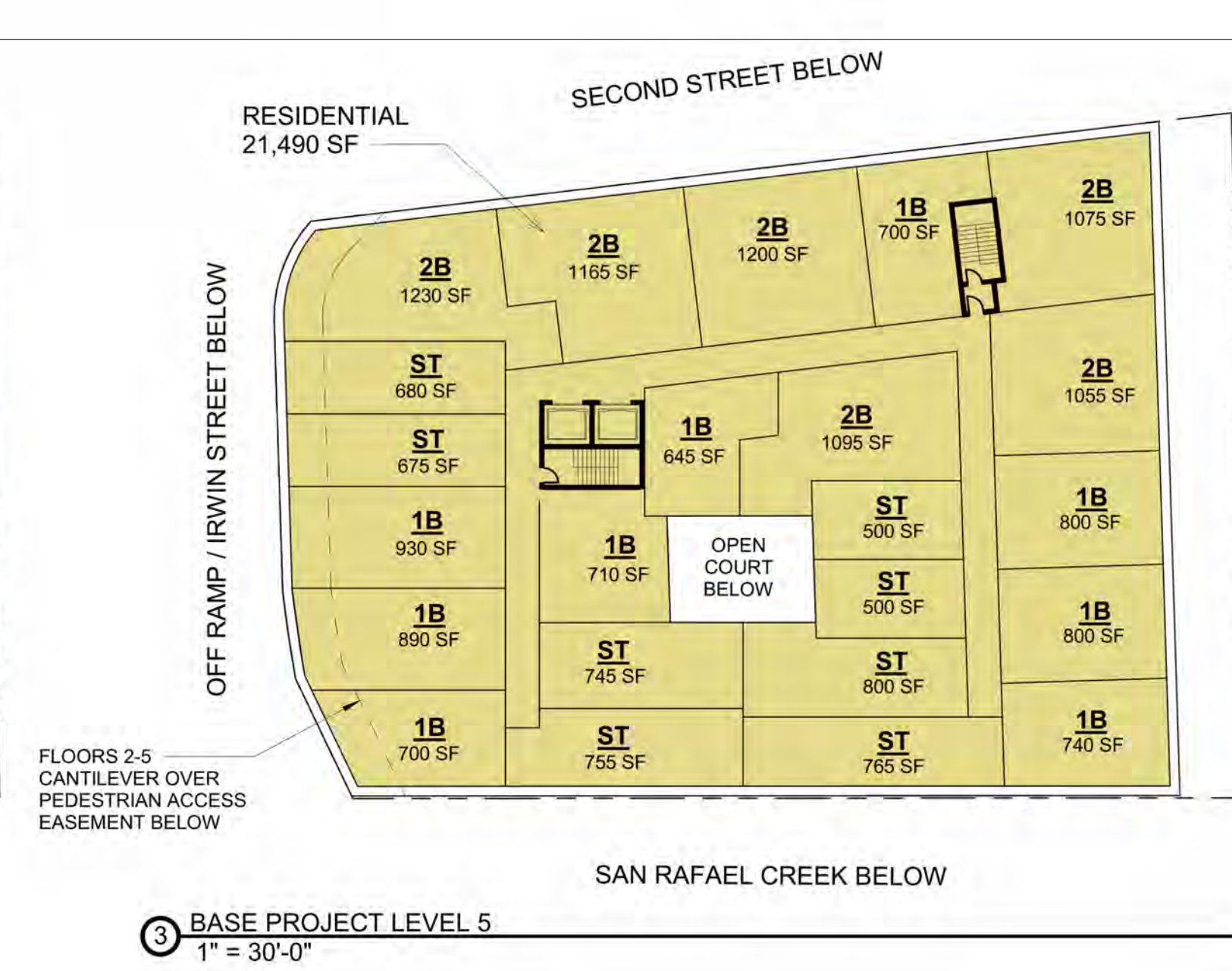
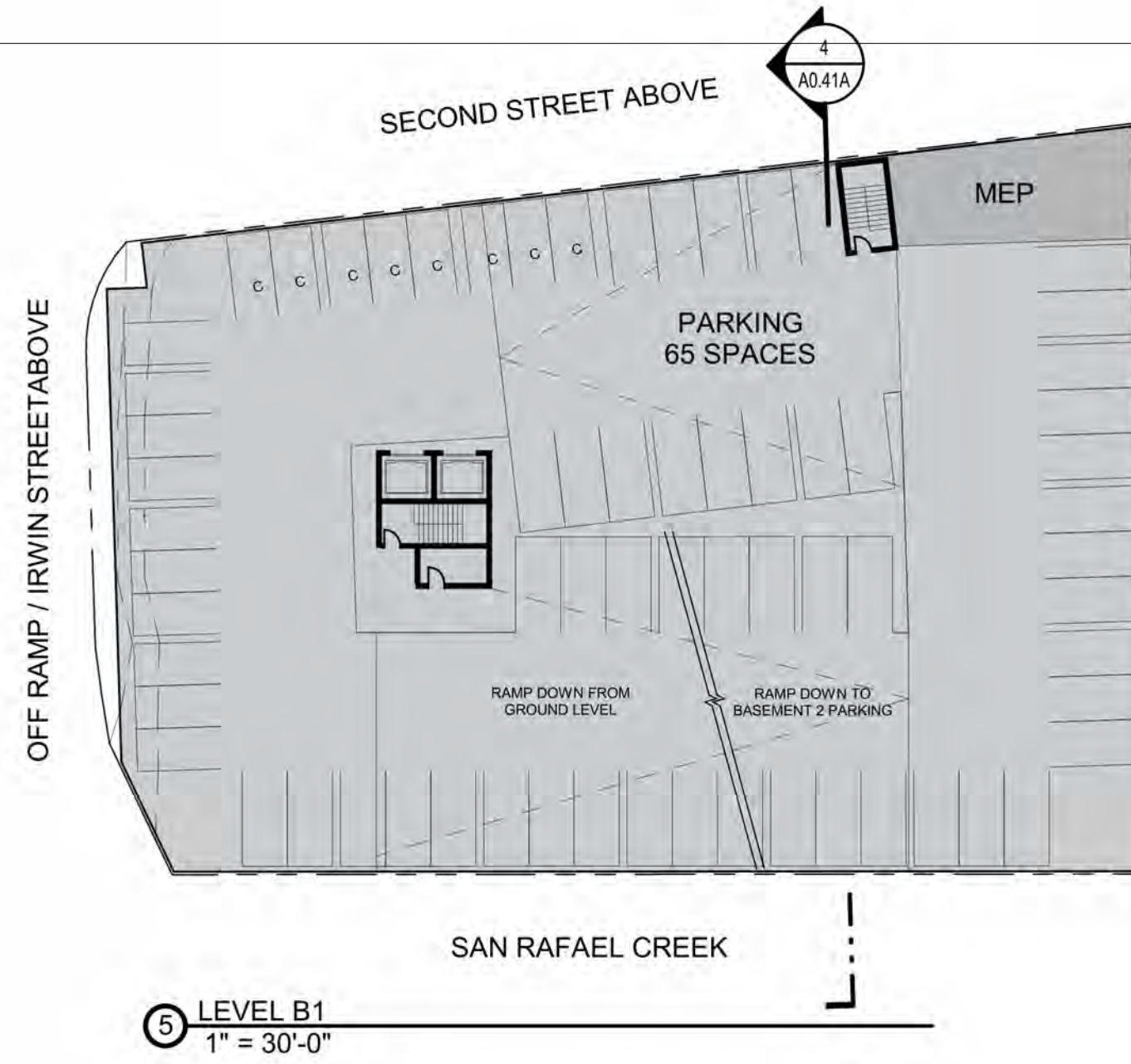
	UNITS	RATIO	TOTAL
CAR SHARE SPACE			2
STUDIO	36	0.75	27
1 BEDROOM UNITS	40	0.75	30
2 BEDROOM UNITS	24	1	24
COMMERCIAL	5,000 SF	2.75 PER 1000 SF OVER 2,500 SF	14
TOTAL REQUIRED			97
PROVIDED			97

* Two car share spaces included in Base Project per 3.1.040.A

BASE PROJECT - BIKE PARKING

2.3.050.H Bicycle Spaces for residential use requirements are:

	UNITS	RATIO	TOTAL
STUDIO	36	1	36
1 BEDROOM UNITS	40	1	40
2 BEDROOM UNITS	24	2	48
TOTAL REQUIRED			124
PROVIDED			124



NO.	DATE	DESCRIPTION
2	04.10.2025	PLANNING PERMIT SUBMITTAL

SCB Architecture, Planning, Interior and Urban Design

700 IRWIN STREET

SAN RAFAEL, CA 94901

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STATE DENSITY BONUS - BASE PROJECT

Drawn By: Author
 Checked By: Checker
 Project Number: 2024022

Sheet Number: **A0.41A**

EXHIBIT 11

Monday, November 10, 2025 at 11:47:44 AM Pacific Standard Time

Subject: Re: 700 Irwin Street Base Project clarifications
Date: Thursday, October 2, 2025 at 12:21:58 PM Pacific Daylight Time
From: Christina Ratcliffe
To: Andrew Junius, Chloe Angelis
CC: Andrea Visveshwara, Doherty, Nira F., MacLean, Connor T., Micah Hinkle

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender.

Andrew,

Thank you for the submittal. The theoretical base plan is consistent with City development standards.

Best,
Christina

Christina Ratcliffe, AICP (she/her)
Planning Consultant
christina.ratcliffe@cityofsanrafael.org
Cell: 510-666-7097

From: Andrew Junius <ajunius@reubenlaw.com>
Sent: Wednesday, October 1, 2025 4:59 PM
To: Christina Ratcliffe <Christina.Ratcliffe@cityofsanrafael.org>; Chloe Angelis <cangelis@reubenlaw.com>
Cc: Andrea Visveshwara <Andrea.Visveshwara@cityofsanrafael.org>; Doherty, Nira F. <ndoherty@bwslaw.com>; MacLean, Connor T. <cmaclean@bwslaw.com>; Micah Hinkle <Micah.Hinkle@cityofsanrafael.org>
Subject: RE: 700 Irwin Street Base Project clarifications

Christina – please see attached. The notes you have requested, along with the parking plan, have been added to the base project sheet.

Please confirm this bas plan is consistent with applicable City development standards.

Thank you.

Andrew Junius
Managing Partner

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O: (415) 567-9000
D: (415) 291-7022
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